



CITY OF ATASCADERO PLANNING COMMISSION AGENDA

**REGULAR MEETING
Tuesday, November 21, 2017
6:00 P.M.**

**Historic City Hall Council Chambers
6500 Palma Avenue, 4th Floor
Atascadero, California 93422**

CALL TO ORDER

Pledge of Allegiance

Roll Call: Chairperson Duane Anderson
Vice Chairperson Jerel Seay
Commissioner Ryan Betz
Commissioner Mark Dariz
Commissioner Josh Donovan
Commissioner Jan Wolff
Commissioner Tom Zirk

APPROVAL OF AGENDA

PUBLIC COMMENT

*(This portion of the meeting is reserved for persons wishing to address the Commission on any matter not on this agenda and over which the Commission has jurisdiction. Speakers are limited to **three minutes**. Please state your name for the record before making your presentation. The Commission may take action to direct the staff to place a matter of business on a future agenda.)*

CONSENT CALENDAR

(All items on the consent calendar are considered to be routine and non-controversial by City staff and will be approved by one motion if no member of the Commission or public wishes to comment or ask questions.)

1. APPROVAL OF DRAFT MINUTES OF PLANNING COMMISSION MEETING ON NOVEMBER 7, 2017



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PLANNING COMMISSION BUSINESS**COMMUNITY DEVELOPMENT STAFF REPORTS**

NONE

PUBLIC HEARINGS**DISCLOSURE OF EX PARTE COMMUNICATIONS:**

Prior to a project hearing Planning Commission Members must disclose any communications they have had on any quasi-judicial agenda items. This includes, but is not limited to, Tentative Subdivision Maps, Parcel Maps, Variances, Conditional Use Permits, and Planned Development Permits. This does not disqualify the Planning Commission Member from participating and voting on the matter, but gives the public and applicant an opportunity to comment on the ex parte communication.

(For each of the following items, the public will be given an opportunity to speak. After a staff report, the Chair will open the public hearing and invite the applicant or applicant's representative to make any comments. Members of the public will be invited to provide testimony to the Commission following the applicant. Speakers should state their name for the record and can address the Commission for three minutes. After all public comments have been received, the public hearing will be closed, and the Commission will discuss the item and take appropriate action(s).)

2. PLN 2017-1649 FOR 6500 EL CAMINO REAL (LA PLAZA)
(CONTINUED FROM NOVEMBER 7, 2017)

Property Owner:	Mike Zappas, 8189 San Dimas Lane, Atascadero, CA 93422
Representative:	Pat Blote, RRM Design Group, 3765 S. Higuera, SLO, CA 93401
Project Title:	PLN 2017-1649 – La Plaza Development
Project Location:	6300-6500 El Camino Real, Atascadero, CA 93422, APN 030-193-001,003,017,016, 031,030,033 (San Luis Obispo County)
Project Description:	<p>The applicant proposes a three-story vertical mixed use development project that includes 48 future residential and commercial airspace condominiums within the Downtown Commercial (DC) zone with vacating and closure of portions of right-of-way along Atascadero Avenue for the construction of a public pedestrian mall. Proposed project includes:</p> <ul style="list-style-type: none"> • 18,000 square feet (sf) of commercial-type uses on the first floor; • 9,700 sf of office space on 2nd and 3rd floors of a separate building; • Eight (8) air space commercial condominium spaces for future subdivision; • 40 air space condominium residential condominium units on 2nd and 3rd floors; • 11,761 square foot (sf) pedestrian plaza located on Atascadero Mall. <p>General Plan Designation: D Zoning District: DC</p>
Proposed Environmental Determination:	A Notice of Intent to Adopt Mitigated Negative Declaration is being circulated from 10/18/17 to 11/7/17 and is available at www.atascadero.org .
City Staff:	Alfredo R. Castillo AICP, Assoc. Planner, acastillo@atascadero.org , Phone: 470-3436
Staff Recommendation:	Staff recommends the Planning Commission adopt PC Resolution 2017-A, 2017-B, 2017-C, and 2017-D approving the project with conditions.



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3. PLN 2017-1641 FOR 1827 AND 1843 EL CAMINO REAL (ANNEX SUBDIVISION)

Property Owner:	MP Annex, LLC, 284 Higuera Street, San Luis Obispo, CA 93401
Representative:	Whelen Consulting, PO Box 5021, San Luis Obispo CA 93403
Project Title:	PLN 2017-1641 – Annex Subdivision
Project Location:	1827 and 1843 El Camino Real, Atascadero, CA 93422, APN 049-102-020 & 049-102-032 (San Luis Obispo County)
Project Description:	Proposed subdivision of a portion of two (2) legal lots into six (6) consistent with the Del Rio Road Commercial Area Specific Plan (DRCASP) overlay zone. General Plan Designation: GC Zoning District: CR
Proposed Environmental Determination:	The project was considered in Final EIR No. 201000541034, certified by the City Council on June 29, 2012, which adequately addressed the effects of the proposed project.
City Staff:	Alfredo R. Castillo AICP, Assoc. Planner, acastillo@atascadero.org, Phone: 470-3436
Staff Recommendation:	Staff recommends the Planning Commission adopt PC Resolution 2017-A and 2017-B approving the project with conditions.

COMMISSIONER COMMENTS AND REPORTS**DIRECTOR'S REPORT****ADJOURNMENT**

The next regular meeting will be on December 5, 2017, at 6:00 p.m. at City Hall Council Chambers, 6500 Palma Avenue, Atascadero.

Please note: Should anyone challenge in court any proposed development entitlement listed on this Agenda, that person may be limited to raising those issues addressed at the public hearing described in this notice or in written correspondence delivered to the Planning Commission at, or prior to, this public hearing.



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*City of Atascadero***WELCOME TO THE ATASCADERO PLANNING COMMISSION MEETING**

The Planning Commission meets in regular session on the first and third Tuesday of each month at 6:00 p.m. at City Hall, Council Chambers, 6500 Palma Avenue, Atascadero. Matters are considered by the Commission in the order of the printed Agenda.

Copies of the staff reports or other documentation relating to each item of business referred to on the Agenda are on file in the office of the Community Development Department and are available for public inspection during City Hall business hours at the Front Counter of City Hall, 6500 Palma Avenue, Atascadero, and on our website, www.atascadero.org. All documents submitted by the public during Commission meetings that are either read into the record or referred to in their statement will be noted in the minutes and available for review in the Community Development Department. Commission meetings are audio recorded, and may be reviewed by the public. Copies of meeting recordings are available for a fee. Contact the City Clerk for more information (470-3400).

In compliance with the Americans with Disabilities Act, **if you need special assistance to participate in a City meeting or other services offered by this City**, please contact the City Manager's Office or the City Clerk's Office, both at (805) 470-3400. Notification at least 48 hours prior to the meeting or time when services are needed will assist the City staff in assuring that reasonable arrangements can be made to provide accessibility to the meeting or service.

TO SPEAK ON SUBJECTS NOT LISTED ON THE AGENDA

Under Agenda item, "PUBLIC COMMENT", the Chairperson will call for anyone from the audience having business with the Commission to approach the lectern and be recognized.

1. Give your name for the record (not required)
2. State the nature of your business.
3. All comments are limited to 3 minutes.
4. All comments should be made to the Chairperson and Commission.
5. No person shall be permitted to make slanderous, profane or negative personal remarks concerning any other individual, absent or present.

This is when items not on the Agenda may be brought to the Commission's attention. A maximum of 30 minutes will be allowed for Public Comment Portion (unless changed by the Commission).

TO SPEAK ON AGENDA ITEMS (from Title 2, Chapter 1 of the Atascadero Municipal Code)

Members of the audience may speak on any item on the agenda. The Chairperson will identify the subject, staff will give their report, and the Commission will ask questions of staff. The Chairperson will announce when the public comment period is open and will request anyone interested to address the Commission regarding the matter being considered to step up to the lectern. If you wish to speak for, against or comment in any way:

1. You must approach the lectern and be recognized by the Chairperson.
2. Give your name (not required).
3. Make your statement.
4. All comments should be made to the Chairperson and Commission.
5. No person shall be permitted to make slanderous, profane or negative personal remarks concerning any other individual, absent or present.
6. All comments limited to 3 minutes.

If you wish to use a computer presentation to support your comments, you must notify the Community Development Department at 470-3402 at least 24 hours prior to the meeting. Digital presentations brought to the meeting should be on a USB drive or CD. You are required to submit to the Recording Secretary a printed copy of your presentation for the record. Please check in with the Recording Secretary before the meeting begins to announce your presence and turn in the printed copy.

The Chairperson will announce when the public comment period is closed, and thereafter, no further public comments will be heard by the Commission.



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CITY OF ATASCADERO PLANNING COMMISSION

DRAFT MINUTES

**Regular Meeting – Tuesday, November 7, 2017 – 6:00 P.M.
City Hall Council Chambers
6500 Palma Avenue, Atascadero, California**

CALL TO ORDER - 6:01 p.m.

Chairperson Anderson called the meeting to order at 6:01 p.m. and Commissioner Seay led the Pledge of Allegiance.

ROLL CALL

Present: Commissioners Betz, Dariz, Donovan, Wolff, Zirk, Vice Chairperson Seay, and Chairperson Anderson

Absent: None

Others Present: Recording Secretary, Annette Manier

Staff Present: Community Development Director, Phil Dunsmore
Assistant Planner, Katie Banister

APPROVAL OF AGENDA

MOTION: By Commissioner Dariz and seconded by Commissioner Wolff to approve the Agenda.

Motion passed 7:0 by a roll-call vote.

PUBLIC COMMENT

None

Chairperson Anderson closed the Public Comment period.

CONSENT CALENDAR

- 1. APPROVAL OF DRAFT MINUTES OF PLANNING COMMISSION MEETING ON OCTOBER 17, 2017**

MOTION: By Vice Chairperson Seay and seconded by Commissioner Betz to approve the Consent Calendar.

Motion passed 7:0 by a roll-call vote.

PLANNING COMMISSION BUSINESS

None

COMMUNITY DEVELOPMENT STAFF REPORTS

None

Community Development Director Dunsmore introduced preschool project, and stated that while staff is waiting on additional materials for the presentation, he would cover some items under the Director’s Report as follows:

DIRECTOR’S REPORT

Community Development Director Dunsmore stated that staff is continuing to follow-up on the Carl’s Jr. sign. The City is in discussion with the property owner and is awaiting compliance. Community Development Director Dunsmore announced that the La Plaza item has been continued to a date certain (November 21, 2017), because the project description has changed from apartments to condominiums.

PUBLIC HEARINGS

DISCLOSURE OF EX PARTE COMMUNICATIONS:

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(For each of the following items, the public will be given an opportunity to speak. After a staff report, the Chair will open the public hearing and invite the applicant or applicant’s representative to make any comments. Members of the public will be invited to provide testimony to the Commission following the applicant. Speakers should state their name for the record and can address the Commission for three minutes. After all public comments have been received, the public hearing will be closed, and the Commission will discuss the item and take appropriate action(s).)

2. PLN 2017-1661 FOR 3215 EL CAMINO REAL (AUTUMNLY PRESCHOOL)

Property Owner:	Shelley Hanlon, 3215 El Camino Real, Atascadero, CA 93422
Applicant:	Same as above
Project Title:	PLN 2017-1661 / CUP 2017-0320
Project Location:	3215 El Camino Real, Atascadero, CA 93422 APN 049-163-017 (San Luis Obispo County)
Project Description:	The proposed project is a daycare center / preschool with a capacity of 35 children. The business will include outdoor play areas and a parking area. General Plan Designation: RE Zoning District: RS

Proposed Environmental Determination:	Categorically exempt from CEQA as a Class 3 Exemption for the construction or conversion of small structures (CEQA Guidelines Section 15303).
City Staff:	Katie Banister, Assistant Planner, kbanister@atascadero.org, Phone: 470-3480
Staff Recommendation:	Staff recommends the Planning Commission adopt PC Resolution 2017-A approving a Conditional Use Permit allowing a child care center use and development plan for the property, based on findings and subject to conditions.

EX PARTE COMMUNICATIONS

Commissioner Zirk announced that he met with the applicant and visited the site.

Assistant Planner Banister gave the staff report, and she and Community Development Director Dunsmore answered questions from the Commission. Assistant Planner Banister stated that the applicant would have to apply for a building permit to change the occupancy of the building, and she shared an alternative site plan (Exhibit A).

Assistant Planner Banister recommended two corrections as follows:

- Correct the General Plan designation to SE
- Correct Conditions 25 and 26 to read “If City Standard frontage improvements are not required by Condition 24 above,”

PUBLIC COMMENT

The following members of the public spoke during public comment: Shelley Hanlon, Russ Coulton, Lea Coulton, Christe Brunz, and Lacey Newton. Shelley Hanlon answered questions from the Commission and stated that the front playground could be eliminated (per State standards) to address the traffic flow issue.

An email was received in regards to this project from Dave and Christy Trupe and was distributed to the Commission (Exhibit B).

Chairperson Anderson closed the Public Comment period.

Staff answered questions in regards to parking, lights, noise and traffic.

Commissioners discussed adding conditions such as hours of operation being from 7 a.m. to 6 p.m., adding a fence alongside the driveway, installing a gate so exiting can occur during an emergency for sheltering at the church next door, and the potential to decrease the number of children to be less than 35. Commissioners were in agreement (by straw vote) that a day care is an appropriate site location for this land use if all of these circulation and access items can be resolved. Commissioners agreed that Alternatives (Item 2 on Page 15 of the staff report) would apply to this project, which generally states that more information is needed before a decision can be made.

MOTION:

By Commissioner Zirk and seconded by Commissioner Wolff to continue the item to a date uncertain and address the following topics: 1) Applicant to work with a civil engineer and return with a new site plan that addresses traffic flow with a circular entry and exit, and addressing the neighbor’s concerns, 2) Adequate parking on the slope where pick-up and drop-off will occur based on staff’s recommendations, 3) Widening of the driveway while taking into consideration the location of the current gas meters, 4) Address signage/speed with the assistance of the Public Works Department to aid with traffic issues on entering and exiting El Camino Real and adding a “Cautionary- Children are Present” sign if possible.

Motion passed 7:0 by a roll-call vote.

3. PLN 2017-1650, TITLE 9 PLANNING AND ZONING TEXT AMENDMENTS

Applicant:	City of Atascadero, 6500 Palma Ave., Atascadero, CA 93422
Project Title:	PLN 2017-1650
Project Location:	Citywide, Atascadero, CA 93422 (San Luis Obispo County)
Project Description:	The action consists of proposed Zoning Ordinance Text Amendments to correct inconsistencies and typographical errors, and to update zoning definitions for consistency with the General Plan.
Proposed Environmental Determination:	Exempt by the general rule that CEQA applies only to projects which have the potential for causing a significant effect on the environment. The proposed text amendment will not have any significant adverse environmental impacts.
City Staff:	Katie Banister, Assistant Planner, kbanister@atascadero.org, Phone: 470-3480
Staff Recommendation:	Staff recommends the Planning Commission adopt PC Resolution 2017-A recommending the City Council introduce an ordinance for first reading only, to approve PLN 2017-1650, based on findings.

Community Development Director Dunsmore introduced the topic.

EX PARTE COMMUNICATIONS

None

Assistant Planner Banister presented the staff report to the Commission. Assistant Planner Banister and Community Development Director Dunsmore answered questions from the Commission.

Commissioner Seay suggested the following items be corrected: On Page 33, Item 2. Land use definitions, under the bullet point that says “Allow parks and playgrounds”, change the Section from Section 9-3.330 to 9-3.230 and list this on Page 41, not Page 46. On Page 127, Section (4) change “build- ings” to “buildings.” Throughout the document, eliminate the word “see” in front of the word Section and just list the Section number for consistency. Commissioner Donovan questioned the difference between “Eating and Drinking places,” “Micro-brewery/ brewpub” and “tasting rooms” and asked why each is defined separately. Staff explained the differences between each of the land uses.

PUBLIC COMMENT

None

Chairperson Anderson closed the Public Comment period.

MOTION:

By Commissioner Donovan and seconded by Commissioner Betz to adopt PC Resolution 2017-A approving PLN 2017-1650 recommending the City Council introduce an ordinance for first reading, by title only, to approve PLN 2017-1650, based on findings with the following changes: 1) On Page 33, Under 2. Land Use Definitions, under the bullet point that says “Allow parks and playgrounds, change the Section to 9-3.230, and list this on Page 41, 2) On Page 127, correct the spelling of the word buildings, and 3) eliminate the word “see” in front of “Sections” throughout the document.

Motion passed 7:0 by a roll-call vote.

**4. PLN 2015-1649 FOR 6500 EL CAMINO REAL (Z3 LLC)
(TO BE CONTINUED TO NOV. 21, 2017)**

Property Owner:	Mike Zappas, 8189 San Dimas Lane, Atascadero, CA 93422
Representative:	Pat Blote, RRM Design Group, 3765 S. Higuera, SLO, CA 93401
Project Title:	PLN 2017-1649 – La Plaza Redevelopment
Project Location:	6500 El Camino Real, Atascadero, CA 93422, APN 030-193-001,003,017,016, 031,030,033 (San Luis Obispo County)
Project Description:	The applicant proposes a mixed-use development project on previously developed properties within Downtown Atascadero. The proposed project includes: <ul style="list-style-type: none"> • 18,000 square feet (sf) of commercial-type uses on the first floor; • 9,700 sf of office space on 2nd and 3rd floors of a separate building;

	<ul style="list-style-type: none"> • 40 air space condominium residential units on 2nd and 3rd floors; • 11,761 sf pedestrian plaza located on Atascadero Mall. <p>General Plan Designation: D Zoning District: DC</p>
Proposed Environmental Determination:	A Notice of Intent to Adopt Mitigated Negative Declaration is being circulated from 10/18/17 to 11/7/17 and is available at www.atascadero.org .
City Staff:	Alfredo R. Castillo AICP, Assoc. Planner, acastillo@atascadero.org , Phone: 470-3436
Staff Recommendation:	To Be Continued To November 21, 2017

COMMISSIONER COMMENTS AND REPORTS

None

DIRECTOR'S REPORT

Community Development Director stated that the next meeting is scheduled for November 21, 2017, and there will be a quorum for that meeting. On that agenda will be La Plaza and a subdivision of 6 lots for the Annex.

ADJOURNMENT – 8:21 p.m.

The next Regular meeting of the Planning Commission is scheduled for **November 21, 2017**, at **6:00 p.m.** at City Hall, Council Chambers, 6500 Palma Avenue, Atascadero.

MINUTES PREPARED BY:

Annette Manier, Recording Secretary
Administrative Assistant

The following exhibits are available in the Community Development Department:

- Exhibit A – Alternative site plan
- Exhibit B - Email from Dave and Christy Trupe



Atascadero Planning Commission

Staff Report – Community Development Department

PLN 2017-1649 La Plaza Downtown Redevelopment

RECOMMENDATION(S):

DRC and Staff Recommends the Planning Commission adopt:

1. Draft PC Resolution 2017-A, certifying Environmental Document Number 2017-0018;
2. Draft PC Resolution 2017-B, approving a master plan of development (CUP) for PLN 2017-1649; and
3. Draft PC Resolution 2017-C, approval of tentative parcel Map AT 17-0088 for condominium purposes for PLN 2017-1649.

Planning Commission Recommends to the City Council:

4. Draft PC Resolution 2017-D, recommending approval to the City Council vacating a 10-foot road and landscape easement adjacent to the back of sidewalk along El Camino Real, vacating right-of-way and partial closure of Atascadero Avenue to accommodate a public pedestrian plaza.

Project Info In-Brief:

PROJECT ADDRESS:	6500, 6490, 6452, 6450, 6420, 6320, 6360, 6390, 6322, 6300 El Camino Real	Atascadero, CA	APN	030-193-001,003,017,016,031,030,033
PROJECT PLANNER	Alfredo R. Castillo, AICP Associate Planner	470-3436	acastillo@atascadero.org	
APPLICANT	RRM Design Group, 3465 S. Higuera Street, San Luis Obispo, CA 93401			
PROPERTY OWNER	Z3 LLC, 8189 San Dimas Lane, Atascadero, CA 93401			
GENERAL PLAN DESIGNATION:	ZONING DISTRICT:	SITE AREA	EXISTING USE	PROPOSED USE
Downtown (D)	Downtown Commercial (DC)	1.83 net acres	Retail, Food and Beverage, and vacant land.	Multi-Family Residential / Office / General Retail
ENVIRONMENTAL DETERMINATION				
<input type="checkbox"/> Environmental Impact Report SCH: <input checked="" type="checkbox"/> Mitigated Negative Declaration – EDN 2017-0018 <input type="checkbox"/> Categorical Exemption CEQA – Guidelines Section 153 _____ <input type="checkbox"/> Statutory Exemption §§ 21000, et seq & _____ <input type="checkbox"/> No Project – Ministerial Project				

REPORT-IN-BRIEF:

The applicant proposes a mixed-use development project on a previously developed site within Downtown Atascadero with two new 3-story buildings proposed. Proposed project includes:

- One three story building with 9,000 square feet (sf) of commercial uses on the first floor; and
- 9,070 sf of office space on 2nd and 3rd floors;
- One three story building with 9,000 square feet of commercial space on the first floor and 40 air space residential condominium units on 2nd and 3rd floors; and
- 11,761 sf pedestrian plaza located on Atascadero Mall.

The proposed project includes 84 off-street parking spaces between the building and Hwy 101, and proposed right-of-way abandonment of portions of El Camino Real, Atascadero Avenue, and partial vehicular closure of Atascadero Avenue (Atascadero Mall) to allow for construction of a plaza. Proposed height of all occupied structures is 45-feet, with an additional 20-feet for un-occupied architectural features. Maximum height of structures within the proposed development is 65-feet. An on-site freestanding pylon sign proposed to be a total height of 70-feet is proposed between the building and Highway 101.

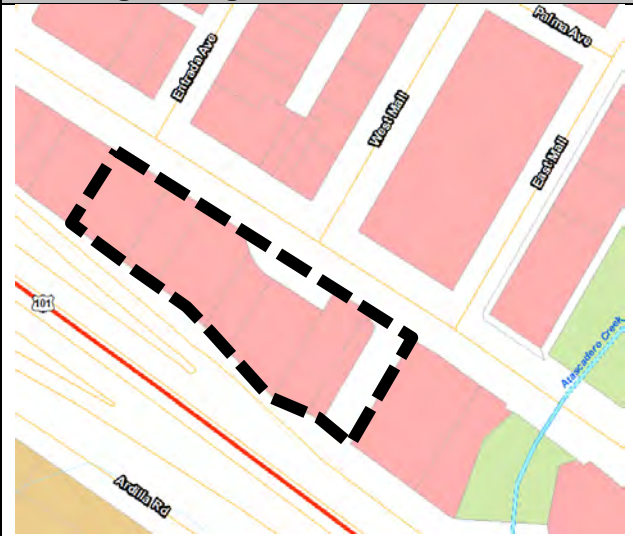



The applicant is requesting the following:

- A density bonus to allow 40 residential units where 37 would normally be allowed for “high quality architectural design” consistent with General Plan Policy 2.1.2
- A height waiver for building heights greater than 18-feet per Atascadero Municipal Code section 9-4.113(b);
- A height waiver for construction of a 70-foot tall freeway oriented pylon sign;
- A lot merger of 12 parcels into four (4) parcels with parcel 1 and 2 to be subdivided further for future condominium purposes;
- Abandonment of public access easements / right-of-way along El Camino Real and Atascadero Avenue; and
- Reduction of vehicular access and partial vehicular closure of Atascadero Avenue (Atascadero Mall) to construct a public pedestrian plaza.

DISCUSSION:

Existing Surrounding Uses / Parcel Configurations:

Existing Zoning		Existing Aerial / Surrounding	
			
North:	South:	East:	West:
Downtown Commercial (DC)	Downtown Commercial (DC)	Right-of-Way / El Camino Real	Freeway (US 101 Right of Way)

Background:

In March 2009, a fire destroyed two commercial buildings within the project site. Since that time, the property has remained underutilized. A coffee shop, beauty parlor and cell tower still remain on the site. In 2010, a redevelopment proposal was brought forth by the former property owner. The proposal, known as “La Plaza Theaters”, consisted of a



39,680± square foot (sf) building that would contain a 10 screen movie theater, retail stores and a sit-down restaurant. This project went before the City Council and was continued for additional analysis. The former property owner formally withdrew their application for development of this property in January 2011.

In August 2013, City Council approved a commercial mixed-use project that envisioned 26,500 sf retail/restaurant space on first floor uses, and office uses on 2nd floor. The approval included an approval to abandon a portion of El Camino Real and Atascadero Avenue (Atascadero Mall) for the proposed development.

Since the approval, the previous property owner marketed the property for purchase and development; however, probate and other tax issues delayed the sale. Mr. Zappas, the current project applicant, purchased the property in 2016, along with three other properties between the site and Traffic Way. He has since worked with RRM design group and City Staff on a proposed re-design of the project that included additional height, along with a revised land use plan that includes mixed-use (residential units above commercial) consistent with the General Plan vision for the Downtown.

The DRC preliminarily reviewed the proposed project in March 2017 and provided the applicant and their design team direction. The applicant returned to the DRC in July 2017, and the DRC voted 4-0 to forward the project to the Planning Commission for approval.

Summary:

The project area is approximately 1.83 net acres with a gentle slope of less than 5 percent. The site has been previously developed since the 1920's including the development of what is known as the "Golden Way Block" which included development geared towards travelers along El Camino Real, then known as the State Highway. Additional development on the proposed project site included the "Atascadero Hotel". This hotel was destroyed by a fire in the 1960's and was replaced with a retail development. A substantial amount of the retail development was also destroyed in a fire in 2009.

Today, a drive-up coffee retail site, a beauty supply personal service / retail, a vacant building (the former Jack-in-the-Box), and an unmanned wireless telecommunications facility remain.

The applicant proposes construction of two (2) new vertical mixed-use buildings. Building A, the largest proposed buildings, would include retail trade and sales, and/or tourism and dining type uses on the 1st floor, and 40 air space residential condominium units on a proposed 2nd and 3rd floor. Multi-Family residential units are an allowed use in the Downtown Commercial (DC) zoning district on 2nd floors and above, with a maximum density of 20 units per acre. The applicant requests additional units through the City's General Plan Policy, which allows for a density bonus of up to 15% for high quality architectural design. Building B proposes retail or restaurant type uses on the 1st floor, but office uses on the 2nd and 3rd floor.



Total building height of “occupied” areas for both buildings is 45-feet. Total building height, as proposed, is 65-feet, which includes architectural features. The applicant proposes a freestanding, freeway oriented pylon sign, with a total height of 70-feet. Both the pylon sign and the proposed height of the buildings require a building height exception, through conditional use permit approval (master plan of development).

The applicant proposes improvements along Atascadero Avenue that include a new pedestrian plaza linking the existing underground pedestrian crossing to the Sunken Gardens. This requires a partial closure of Atascadero Avenue (Atascadero Mall) and reduces the existing vehicular travel-way from 134-feet, with on-street diagonal parking and median, to 20-feet in order to provide access from El Camino Real to the existing auto repair business at 6700 El Camino Real. A summary of the proposed development includes the following:

- Demolition of all existing structures, with the exception of wireless telecommunications facility;
- 40 air space residential condominium units on 2nd and 3rd floor space;
- 18,000 sf of retail space on 1st floor;
- 9,700 sf of office on 2nd and 3rd floor space;
- Total building height of 65-feet;
- 61 parking surface parking spaces;
- 23 garage parking spaces;
- 70-foot freestanding freeway oriented pylon sign;
- 11,760 sf pedestrian plaza within existing right-of-way of Atascadero Mall;
- Reduction of vehicular travel-way and parking on Atascadero Mall from 134-feet to 20-feet; and
- Closure of one (1) existing driveway and new entry way from West Mall.

Analysis:

Architectural Design

The proposed project is an Italianate inspired architectural design that compliments the existing Rotunda Building (Historic City Hall) and the Printery building, which were constructed during the City’s Colony era. The building is proposed to be three stories in height with a maximum height of 45-feet for occupied spaces and 65-feet for unoccupied spaces.

First floor uses include downtown commercial uses such as retail, personal services, restaurant, and tourist type uses (no tenants have been announced at this time). 2nd and 3rd floor uses in Building A include residential condominium units, while Building B proposes office uses. Building A is the largest building in terms of footprint size. The building includes three (3) major pop-out elements and major entry points, and two (2) features that evoke the corner tower at the Historic Carlton Hotel, as well as a clock tower that mimics the clock tower that previously existed at Atascadero High School. These tower elements provide a visual perspective from both El Camino Real and



Highway 101 that will work to identify our downtown. The proposed building includes architectural features such as large windows and articulation that tie the building together while complementing other downtown buildings.

Ground level architectural features in Building A include the use of Italianate columns, and use of a colonnade. Storefronts include large windows visible from the street and additional articulation at the ground level, consistent with the City's adopted Downtown Revitalization Plan Design Guidelines. Second and third story details include balconies with complimentary detailing, and pop-outs to break up massing and provide scale to the building.

Ground level features in Building B include an entry feature that evokes the main entry of Historic City Hall with the shape of the primary circular window and columns. The location of the primary entry of the building has a direct line of sight with the Sunken Gardens Park. While smaller than Building A, Building B ground floor includes differing architectural features at the base of the building and includes awnings to differentiate from Building A, but ties in with surrounding buildings located along El Camino Real and Entrada. Building B also includes one (1) tower element which provides a visual cue for motorist traveling northbound on El Camino Real. Building B also includes pedestrian scale elements such as lighting and additional pop-outs on upper stories that provide massing and scale.

Both buildings contain varying rooflines that include rafter tails and corbels, as well as use of similar color and materials. These materials include the use of mission style roof elements, brick elements with similar color hues as Historic City Hall, the Printery, and Atascadero Middle School. Other elements include concrete panels, porcelain tile, and dark earth tone colors that complement the materials utilized throughout the project.

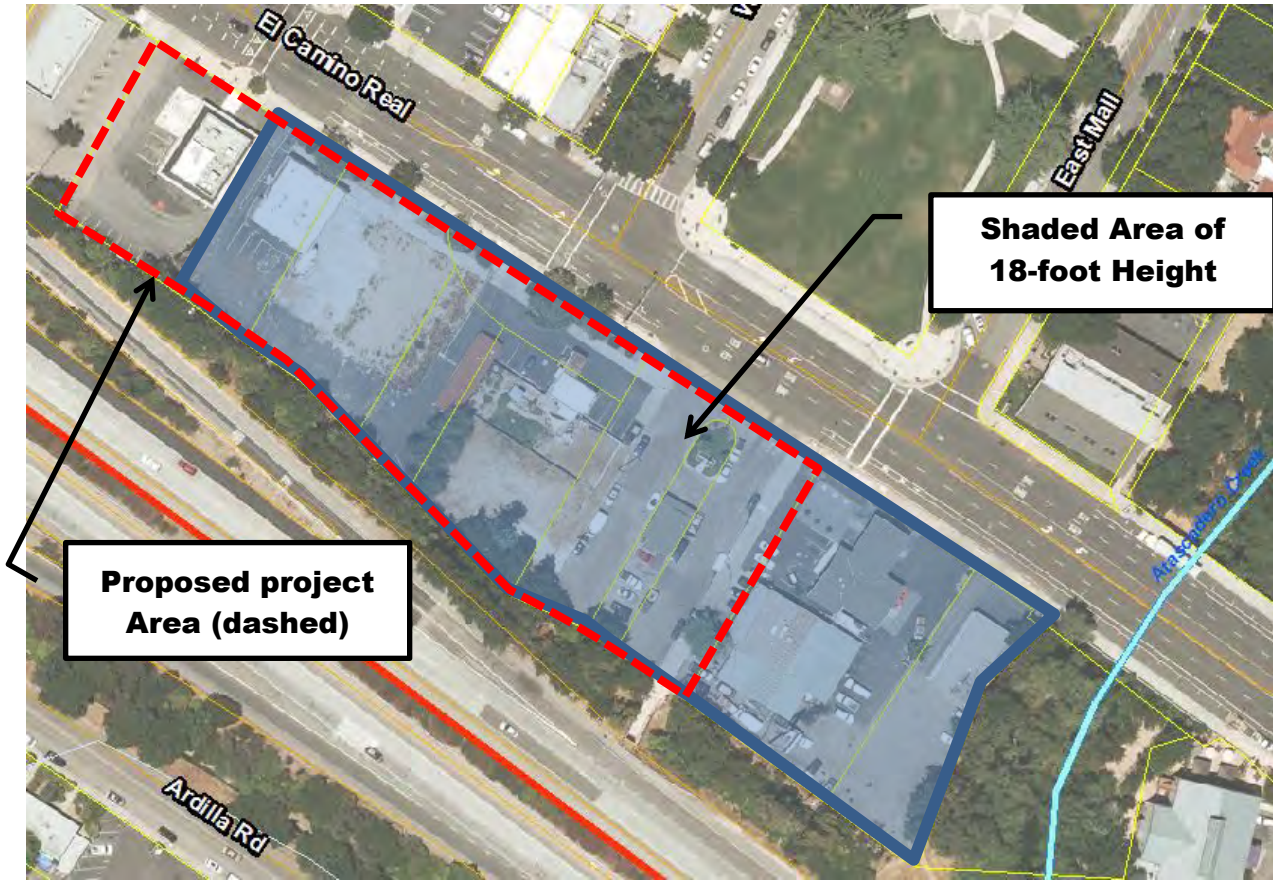
Height Exception

The proposed project is located within the Downtown Zone district, which should be the location in the City that provides for the largest and most prominent buildings similar to City Hall, the Carlton and other buildings that formerly existed in the downtown. However, current zoning restricts the height of buildings to 18 feet between the park and the highway. This requirement was adopted as part of the Redevelopment Plan in 2000. The purpose of the restriction was to provide a visual window between the freeway and the historic City Hall. However, due to the presence of vegetation within the freeway right-of-way, there is limited visibility of City Hall from the freeway. In addition, the height restriction significantly reduces the development potential and architectural desirability of buildings on the west side of El Camino Real.

The applicant is requesting a height exception consistent with Section 9-4.113 of the Zoning Ordinance to allow a building height of 45-feet and to allow unoccupied building features (towers) to extend up to 65 feet.



Area of 18-Foot Height Limit



The adopted redevelopment plan and the City’s Municipal Code allow for height exceptions based on findings that the proposed height will not be a detriment to the enjoyment and use of surrounding properties and that the height will not exceed the lifesaving capacity of the Atascadero Fire and Emergency Services Department (see attached in Resolution 2017-B).

Site Plan and Landscaping

The site is bounded by Atascadero Avenue (Atascadero Mall) to the south, El Camino Real to the east and Highway 101 to the west. The northern edge of the proposed project is the former Jack in the Box restaurant. The existing Patterson Reality Office (Former Founder’s Bank) would remain as is. The proposed project includes two project entries. The applicant will modify the existing entrance directly across from Entrada Avenue to the centerline of the roadways for better traffic and circulation flow. The primary entry jogs to the southeast for traffic safety purposes, as requested by the City Engineer. The entry way is 20-feet wide, consistent with the AMC. The following are proposed site plan and landscaping plan improvements:

- Passenger drop off zone as required by the AMC;



- 84 total parking spaces, including 61 surface spaces and 23 garage / podium style spaces reserved for residents;
- Areas for trash enclosures;
- Drought tolerant landscaping within the parking lot;
- Relocation of existing mid-block pedestrian crossing directly across from East Mall to West Mall;
- New Plaza area between Building A and the proposed project entry adjacent to West Mall;
- Extension of proposed plaza from Building B to the existing Atascadero Avenue (Atascadero Mall) ; and
- A new Plaza connecting the existing pedestrian undercrossing connecting Atascadero High School / residents west of Highway 101 to the Downtown. This plaza utilizes the existing right-of-way of Atascadero Mall and will close the majority of this area to automobile uses.

The proposed project meets all required setbacks, parking, and landscaping requirements of the AMC. A review of required setbacks, parking, etc. is included in Attachment 1. The proposed project is consistent with the City's Downtown Revitalization Vision Plan, as it sites new non-residential buildings directly adjacent to the street and proposed plaza. Additionally, the proposed project includes a parking component, with spots reserved for residents, in the rear of the proposed project adjacent to Highway 101.

Density Bonus

The applicant is requesting a Density Bonus to allow 3 additional residential units in exchange for high quality architecture. This would result in a total of 40 units where 37 units would normally be allowed within the Downtown Commercial Zone. General Plan Policy states the following:

General Plan Policy 2.1.2

Develop standards and procedures to allow the Planning Commission to grant up to a 15% density bonus for attached multi-family projects of exceptionally high design quality through the Conditional Use Permit process.

The City has approved three (3) density bonuses for high quality architecture:

- ❑ Las Lomas / Woodridge Specific Plan, including multi-family housing component
- ❑ Dove Creek
- ❑ Emerald Ridge Apartments

This density bonus is for exceptionally high quality design, and is not connected to affordable housing. This is a discretionary action that the Planning Commission may grant to the applicant, and is included as a General Plan policy as a tool to encourage high quality, innovative design. When Staff reviews such requests, it takes the following into consideration:



1. Utilizes high quality materials and finishes through all four sides of the building(s) located on-site;
2. The project uses reduction of massing and other techniques to reduce appearance of bulk and density;
3. Use of varied rooflines, pop-outs, architectural appendages and other elements that create visually appealing projects;
4. Use of architectural materials appropriate with the vernacular of the architectural theme; and
5. Significant contributions to the community such as a park, historic preservation, or road dedications.

The proposed project clearly meets the standards for high quality design and includes the following “high quality” design features:

- Use of high quality materials such as brick, panel accents, mission style roofing, and architectural features similar to Historic City Hall and the Printery on all four sides of the building;
- The proposed project utilizing massing and scale to reduce bulk and the apparent density of the project;
- The proposed project utilizes various roof line types, pop-outs, eve brackets, exposed rafter tails, and other architectural features that enhance the architecture and creates a visually appealing project within Downtown Atascadero;
- The proposed project incorporates materials consistent with Italianate architecture that is reflected in Historic City Hall, and the Printery; and
- The proposed project includes the construction of public spaces, including a public plaza within the Atascadero Mall right-of-way.

Staff has included specific findings for the high quality architectural bonus as a part of Draft PC Resolution 2017-B.

Signage

The applicant is seeking a height exception from the City’s sign ordinance to place a 70-foot tall pylon sign visible from the freeway. The AMC requires an Administrative Use Permit (AUP) approval for sign code exceptions. In this instance, the Planning Commission Use Permit will cover the need for the AUP requirement. The proposed pylon sign includes articulation on all four sides, and will incorporate the City of Atascadero Logo within the signage. The applicant is also proposing a monument sign located at the project entry with Entrada. Monument signage is allowed within the Downtown Commercial zone; however, it is limited for use to buildings that are set back 10-feet from the back of sidewalk. In this instance the Building B is located directly adjacent to the back of sidewalk, and Building A colonnade is directly adjacent, and is considered an integral part of the building.



11. Ground Monument Sign	Yes	All Zoning Districts Except Residential	60 of CN, CP, CR, CL, CT, CPK, DP, 1 70 of L, LS, P, OS 40 of DC, DO, AG	1 per 200 linear feet	0 ft	(a) Minimum base height shall be 1 ft. (b) Maximum base height shall be 4 ft. (c) Base does not count towards sign face area. (d) Base shall have decorative landscaping and shall be consistent with the architecture of the building(s) it serves. (e) Bases shall be located in a landscape planter. (f) Cabinet type signs shall be prohibited. (g) Monument signs in DO/DC require a building to be set back a min. of 10 ft from back of walk. (h) Bases shall be located in a landscape planter and set back 3 ft from the back of sidewalk in the DO/DC area.
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The applicant has not submitted a design scheme for the proposed monument signage. Additionally, the applicant is proposing “building name” signage, identified on the south elevation for Building B. Similar to the monument signage, no dimensions were given on the building name signage; however, the AMC provides standards for such signage.

5. Building Name Identification	Yes	All Zoning Districts except Ag & Residential	20 sq	1 per building	Based on building height	Signage used to identify the building and the owner(s).
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To provide for flexibility in signage, Staff is recommending the Planning Commission make the following findings for signage exceptions:

- Allow for a freeway oriented 75-foot maximum pylon sign, an increase of five additional feet to ensure flexibility and visibility from freeway;
- Allowance of a ground monument signage due to prominence of building and consistency with the Downtown Revitalization Vision Plan;
- Allow for a ground monument signage to be 60 square feet;
- Allow for the use of a multi-tenant free-standing sign in place of a ground monument signage with DRC approval; and
- Allow for Building Name Signage not to exceed 100 sq. ft. for each building, with the ability to have multiple building name signs.

Findings for exceptions to the sign ordinance are including in Draft PC Resolution 2017-B. Conditions have also been added to this resolution.

Proposed Plaza

The applicant is proposing abandonment of right-of-way directly adjacent to Atascadero Mall, a parcel that the applicant owns in fee (pays property taxes), and partial closure of the approximately 66 of the remaining 86-feet of right-of-way. With partial closure of vehicular traffic along Atascadero Avenue (Mall), a public pedestrian plaza is proposed to be constructed by the applicant.

The pedestrian plaza would connect the existing pedestrian undercrossing at the dead-end of Atascadero Avenue and US Highway 101. This would also ensure a safe pedestrian flow from the undercrossing to the relocated pedestrian crossing at El Camino Real and West Mall. Proposed improvements include passive recreational activities, container gardens, and a historical monument. The applicant is required to pay Parkland and Open Space acquisition impact fees for all residential units, and new office and retail space. Proposed plaza improvements would qualify for development impact fee credits for both these fees, in exchange for construction of these



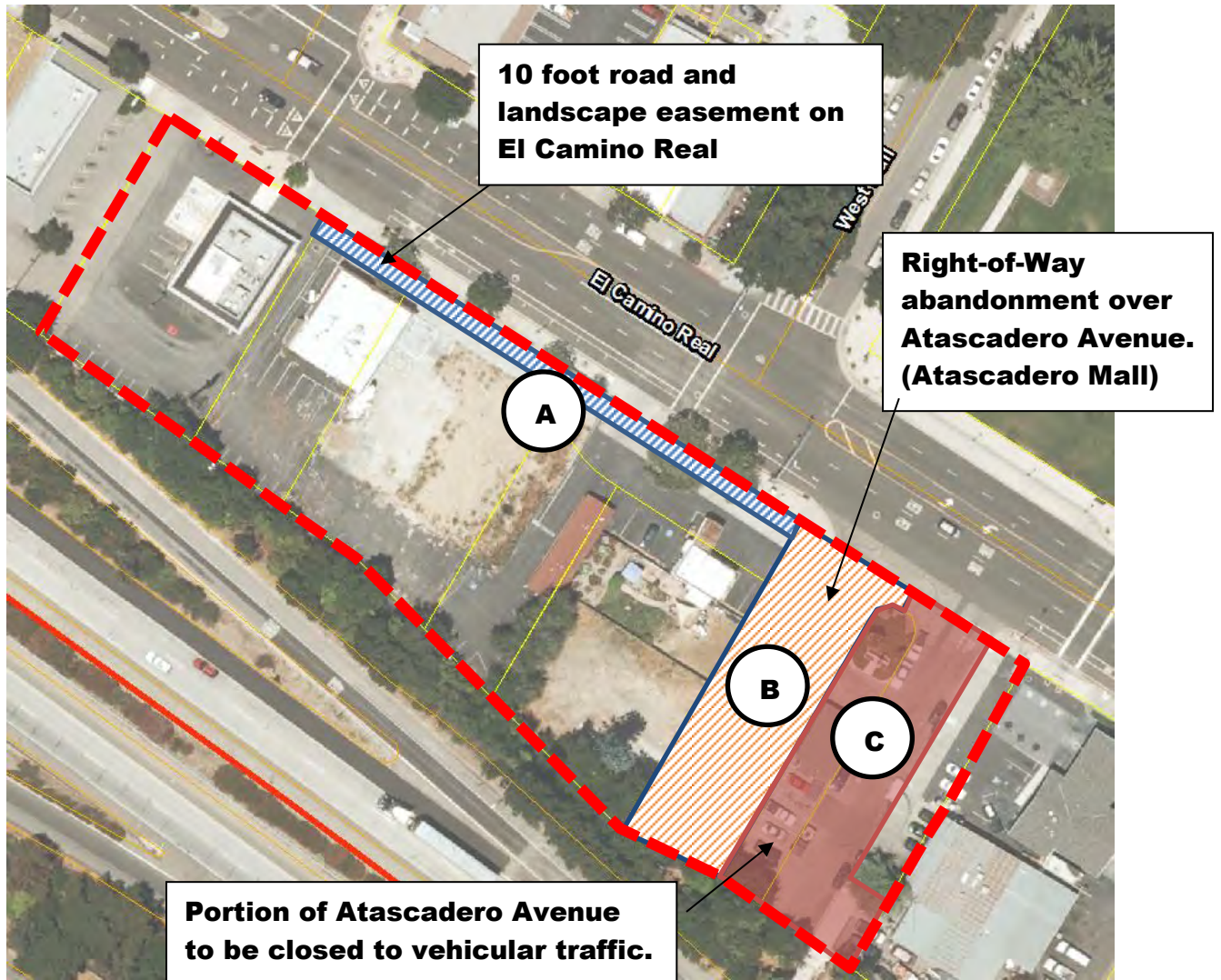
improvements. To ensure the proposed improvements are realized, it is recommended that the applicant enter into a developer's agreement prior to the issuance of permits. This condition is included in Draft PC Resolution 2017-B.

Ultimately, the proposed improvements are consistent with the City's Downtown Revitalization Plan. The plan envisioned a future plaza connecting the existing tunnel to the Downtown. Through this private / public partnership, the plan is being implemented with the proposed project.

Roadway Abandonment / Road Closure

As a part of the 2013 Hoff La Plaza Mixed-Use approval, the City Council approved a "summary vacation" however, a 2nd step to complete the right-of-way abandonment process was never completed. Therefore Staff is recommending that the Planning Commission recommend to the City Council to vacate the easement along El Camino Real and a portion of right-of-way adjacent to Atascadero Avenue through the General Vacation procedure, as specified in the Streets and Highways Code, and be completed through recordation of the Tentative Parcel Map.





The Applicant is requesting the following abandonment of right-of-way and public use easements, and partial vehicular closure of an existing street:

- A. Vacating a Public Use Easement along El Camino Real;
- B. Vacating a portion of right-of-way within Atascadero Avenue; and
- C. Partial vehicular closure of remaining right-of-way within Atascadero Avenue.

A. Vacating a Public Use Easement along El Camino Real

Along El Camino Real is a public easement currently used as a planting strip adjacent to the back of sidewalk. Proposed development of both buildings is located within this easement.



B. Vacating a portion of right-of-way within Atascadero Avenue

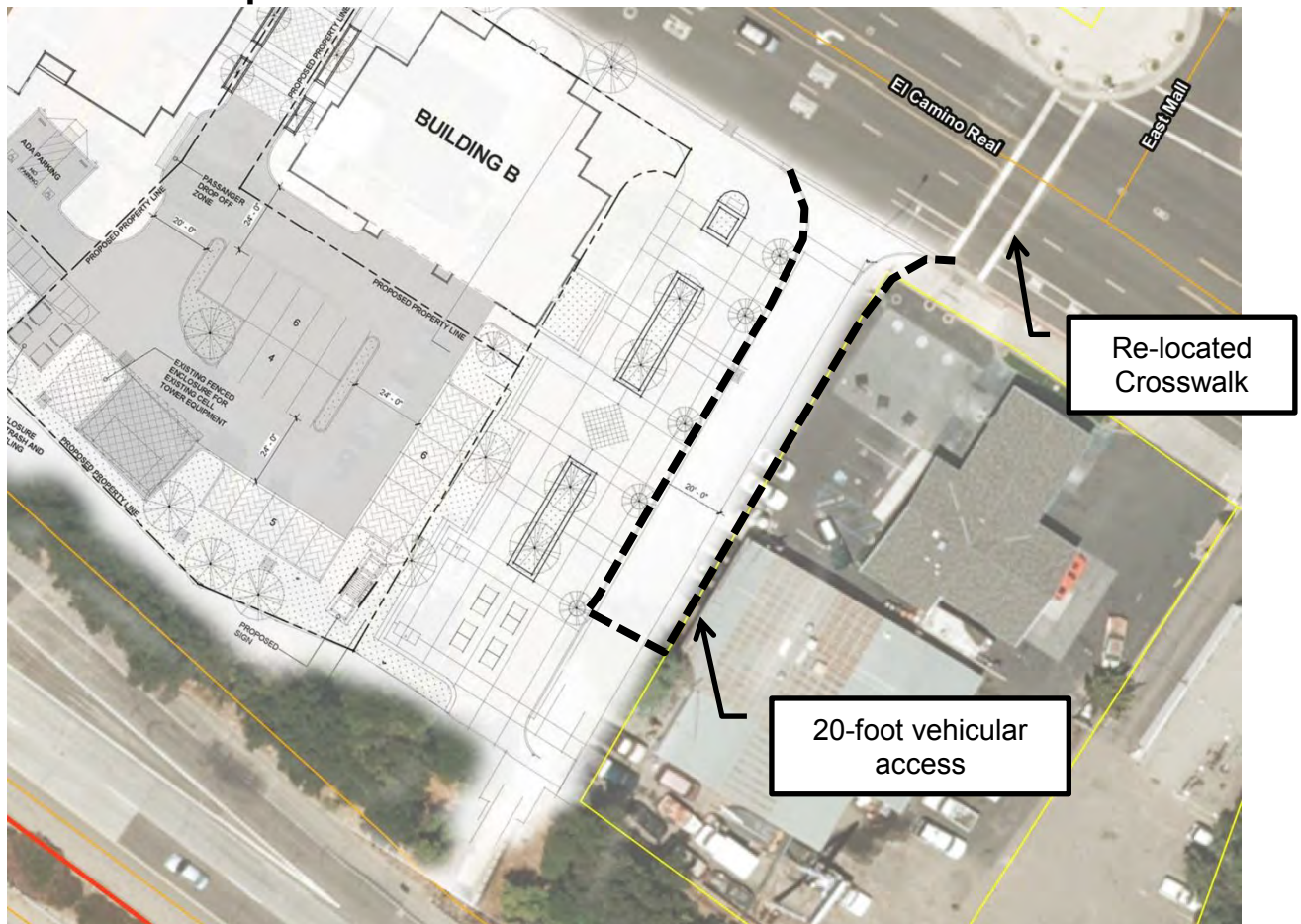
The subject portion of right-of-way along the north side of Atascadero Mall Avenue has curb, gutter and sidewalk, and a paved street. The street right-of-way is bisected by a City owned parcel where the former Main Street Office was located.

It is the staff's recommendation that the entire right-of-way (0.24 acres) to the north east of the center City owned parcel be abandoned to accommodate the proposed project. The proposed abandonment is acceptable since the City would keep the adjacent right-of-way directly to the south for a pedestrian plaza and reduced travel-way for vehicular access.

C. Partial vehicular closure of remaining right-of-way within Atascadero Avenue.

The City will maintain the right of way from the center City owned parcel to the eastern curb edge. A portion of this right-of-way will be developed as a public pedestrian plaza. A 20-foot wide portion will be retained for vehicles to maintain access to existing businesses, as shown on the exhibit below.

Proposed Atascadero Avenue Vehicular Closure Schematic



Abandonment and Vehicular Closure Process:

Right-of-way abandonments are governed by the following code sections from the California Streets and Highways Code and the Atascadero Municipal Code. In order for the proposed abandonment to be approved, the project must comply with all of the provisions and requirements set forth in each code section.

Requirements for vacating a road are found in the Streets and Highways Code, Section 8332:

8324 – California Streets and Highways Code

- (a) At the hearing, the legislative body shall hear the evidence offered by persons interested.
- (b) If the legislative body finds, from all the evidence submitted, that the street, highway, or public service easement described in the notice of hearing or petition is unnecessary for present or prospective public use, the legislative body may adopt a resolution vacating the street, highway, or public service easement. The resolution of vacation may provide that the vacation occurs only after conditions required by the legislative body have been satisfied and may instruct the clerk that the resolution of vacation not be recorded until the conditions have been satisfied.

Requirements for closing a road to vehicular traffic are found in the Streets and Highways Code, Section 1920:

1920 – California Streets and Highways Code:

When the governing body of a city by resolution or ordinance removes a street from public use, or closes it to vehicular or pedestrian traffic, such resolution or ordinance may set forth such minimum maintenance requirements, including the maintenance of drainage, for the street as the governing body determines are necessary for the public safety.

City Requirements

California Government Code Section 65402 requires that all abandonments be consistent with the legislative bodies General Plan, as follows:

“If a general plan or part thereof has been adopted...no real property shall be...vacated or abandoned...until the location, purpose and extent of such...street vacation or abandonment...has been submitted to and reported upon by the planning agency as to the conformity with said general plan or part thereof.”

El Camino Real is identified in the General Plan Circulation and Bikeway/Trail diagrams as an arterial road. Atascadero Avenue is identified in the General Plan Circulation and Bikeway/Trail diagrams as a local road. Only one business, La Rosa’s Automotive (a non-conforming land use) relies upon the Atascadero Avenue for access. The road closure would not eliminate the access to this business.



The Public Works Department has evaluated Atascadero Avenue and has determined that the proposed improvements are sufficient to support the surrounding area at build-out. Therefore, abandonment of the northern portion of the right-of-way would be consistent with the General Plan street classification and would maintain sufficient traffic capacity for future build-out.

The project has been conditioned to provide a public access and utility easement across the Atascadero Avenue road area prior to abandonment. This easement has been prepared and will be recorded prior to Council action. A condition has also been added to require any buildings or structures to be located outside the easement.

Staff has determined that when a right-of-way or portion thereof is abandoned, the zoning shall become the same as the adjacent zoning designation. The requested abandonment will be for the benefit of the adjacent parcel and will become part of the Downtown Commercial zoned parcel. No new parcels will be created as a result of this abandonment.

Staff has included a recommendation to the City Council for right-of-way abandonment as part of Draft PC Resolution 2017-D (Attachment 7).

Tentative Parcel Map

The applicant is proposing a tentative parcel map that combines the underlying seven (7) lots and proposes to combine them into four (4) parcels, with the ability for two (2) of the four (4) parcels to be further subdivided into air space condominiums through the final map process. A total of 40 air space residential condominium units and eight (8) air space commercial condominium spaces are proposed. The tentative map has been reviewed for consistency with the AMC, the California Subdivision Map Act, and the California Street and Highways Code because of the proposed vacancies of right-of-way. Staff is recommending approval of the proposed tentative parcel map, which includes findings in Draft PC Resolution 2017-C.

Conclusion:

The La Plaza Redevelopment Project represents a significant step forward in implementing the City's Downtown Revitalization Plan that was crafted over 10 years ago. While development proposals for this key piece of property have been previously proposed and approved, the applicant and property owner have made a significant commitment to Atascadero and is ready to proceed forward with development of the site. The proposed project is a catalyst for future development within the Downtown and includes a mix of uses that will contribute to a "24-hour" feel of downtown with residences, retail/services, and office uses. The proposed project meets objectives and implementation measures including the following:

- Implementing the General Plan, consistent with the City's planned land uses;
- Maximizes the property's potential in a manner consistent with the General Plan and the Downtown Revitalization Plan;



- Maximizes the economic viability of the vacant site;
- Provides necessary adequate infrastructure and public amenities;
- Includes potential job generating uses that create a multiplier effect for downtown business; and
- Ensures consistent and rational development of the site in accordance with established functional, environmental, and aesthetic standards.

The proposed project is an identified City Council priority project and its approval is included in its 2017-2018 Action Plan. City staff and the DRC recommend the Planning Commission approve Draft PC Resolutions A-C, and recommend approval to the City Council for proposed roadway abandonment in Draft PC Resolution 2017-D.

ENVIRONMENTAL DETERMINATION:

The City of Atascadero prepared an Initial Study to determine if the proposed project would have a significant adverse effect on the environment. The Initial Study found that there may be some impacts created by the project; however those impacts could be rendered to thresholds of less than significant levels through mitigation measures. Consequently, a Mitigated Negative Declaration was prepared for the Project. (See Attachment 4). Certification of Mitigated Negative Declaration No. 2017-0018 will satisfy the requirements of CEQA for the La Plaza Redevelopment Project.

FINDINGS:

To approve the Conditional Use Permit and Tentative Parcel Map, the Planning Commission must make the following findings. These findings and the facts to support these findings are included in the attached resolutions.

Conditional Use Permit (AMC Section 9-2.110(b).(3).(iv)

1. The proposed project or use is consistent with the General Plan;
2. The proposed project or use satisfies all applicable provisions of this title;
3. The establishment, and subsequent operation or conduct of the use will not, because of the circumstances and conditions applied in the particular case, be detrimental to the health, safety or welfare of the general public or persons residing or working in the neighborhood of the use, or be detrimental or injurious to property or improvements in the vicinity of the use;
4. That the proposed project or use will be consistent with the character of the immediate neighborhood and will not be contrary to its orderly development;
5. That the proposed use or project will not generate a volume of traffic beyond the safe capacity of all roads providing access to the project, either existing or to be improved in conjunction with the project, or beyond the normal traffic volume of the surrounding neighborhood that would result from full development in accordance with the land use element;



6. That the proposed project is in compliance with any pertinent city policy or criteria adopted by ordinance or resolution of the city council;

7. Additional findings deemed necessary including the following:

Height Waiver

- a. The project will not result in substantial detrimental effects on the enjoyment and use of adjoining properties.
- b. The modified height will not exceed the lifesaving equipment capabilities of the Fire Department.

Sign Ordinance Exception

- a. The sign is consistent with the purposes set forth in Section 9-15.002;
- b. The opportunity to combine signs for more than one (1) use on a single sign structure has been considered.
- c. For freeway-oriented signs, the sign area and height are the minimum needed to achieve adequate visibility along the freeway due to highway ramp locations and grade differences.
- d. Conformance with all other applicable codes and ordinances of the City, including, but not limited to, the Zoning Ordinance, General Plan and its several elements, and the appearance review guidelines.

Density Bonus Request Per General Plan Policy 2.1.2

- a. The proposed project utilizes high quality materials and finishes throughout all four sides of the building(s) located on-site.
- b. The proposed project utilizes reduction of massing and other techniques to reduce appearance of bulk, and density.
- c. The proposed project implements the use of varied rooflines, pop-outs, architectural appendages and other elements that create visually appealing projects.
- d. The proposed project utilizes architectural materials appropriate with the vernacular of the architectural theme.
- e. The proposed project incorporates significant contributions to the community such as a park, historic preservation, or road dedications.

Tentative Parcel Maps

1. The proposed subdivision, together with the provisions for its design and improvement, is consistent with the General Plan and the proposed Specific Plan (Government Code §§ 66473.5 and 66474(a) and (b));
2. The site is physically suitable for the type of development (Government Code § 66474(c));



3. The site is physically suitable for the proposed density of development (Government Code § 66474(d));
4. The design of the subdivision or the proposed improvements will not cause substantial environmental damage or substantially and avoidably injure fish or wildlife or their habitat. (Government Code § 66474(e));
5. The design of the subdivision or the type of improvements will not cause serious health problems. (Government Code § 66474(f));
6. The design of the subdivision will not conflict with easements for access through or use of property within the proposed subdivision. (Government Code § 66474(g));
7. The installation of public improvements are necessary prior to recordation of a Final Map in order to insure orderly development of the surrounding area (Government Code § 66411.1(b)(2)).

ALTERNATIVES:

1. The Planning Commission may include modifications to the project and/or conditions of approval for the project. Any proposed modifications including conditions of approval, should be clearly re-stated in any vote on any of the attached resolutions.
2. The Planning Commission may determine that more information is needed on some aspect of the project and may refer the item back to the applicant and staff to develop the additional information. The Commission should clearly state the type of information that is required. A motion, and approval of that motion, is required to continue the item to a future date.
3. The Planning Commission may deny the project. The Commission must specify what findings cannot be made, and provide a brief oral statement, based on the Staff Report, oral testimony, site visit, correspondence, or any other rationale introduced and deliberated by the Planning Commission.

ATTACHMENTS:

1. Project Review / Environmental
2. Mitigated Negative Declaration 2017-0018 / Comment Letters
3. Draft PC Resolution 2017-A
4. Draft PC Resolution 2017-B
5. Draft PC Resolution 2017-C
6. Draft PC Resolution 2017-D



**ATTACHMENT 1: PROJECT REVIEW / ENVIRONMENTAL REVIEW CHECKLIST
 PLN 2017-1649**

Basic Project Information			
Project Number:	PLN 2017-1649		
Planner:	Alfredo R. Castillo, AICP <i>ARC</i>		
Project Address:	6500, 6490, 6452, 6450, 6420, 6320, 6360, 6390, 6322, 6300 El Camino Real		
APN:	030-193-001,003,017,016, 031,030,033		
City: Atascadero	County: San Luis Obispo		
Site Area:	1.83 acres		
General Plan Designation:	Downtown (D)		
Zoning District:	Downtown Commercial (DC)		
Project Description:	Proposed mixed-use redevelopment project on previously developed parcels within the downtown corridor. Proposed project includes 18,000 square feet (ft) of retail commercial type uses, 9,700 sf of office space, and a total of 40 units. The proposed project includes 86 off-street parking spaces, and abandonment of a portion of Atascadero Avenue (Atascadero Mall). Proposed height of all occupied spaces is 45-feet, with an additional 20-feet for architectural features. Total height of structures within the proposed development is 65-feet. A total of 3-stories for building is proposed and the use of plazas and other open space areas.		
Existing & Surrounding Information			
Existing Uses:	Retail, Food and Beverage, and vacant / underutilized buildings.		
Use Classification:	Multi-Family Housing / General Retail / Offices	Allowed <input type="checkbox"/>	Conditional <input checked="" type="checkbox"/>
Surrounding Uses / Zoning District:	North:	Financial Services / Downtown Commercial (DC)	
	South:	Right-of-Way / Non-Conforming Uses / DC	
	East:	Right-of-Way / Park / DC	
	West:	Highway 101 / Right-of-Way	
Colony house(s) on property?	Yes <input type="checkbox"/>	No <input checked="" type="checkbox"/>	Notes:
Any existing structures 50 years or older?	Yes <input checked="" type="checkbox"/>	No <input type="checkbox"/>	Notes: CEQA will need to analyze structures of historical significance, including Malibu Brew coffee shop.



Does the site contain any jurisdictional waters? (blue line creeks, wetlands, etc.)	<input type="checkbox"/> Atascadero Creek <input type="checkbox"/> Graves Creek <input type="checkbox"/> Paloma Creek <input type="checkbox"/> Boulder Creek <input type="checkbox"/> Other _____ <input checked="" type="checkbox"/> N/A			
Zoning Ordinance / Municipal Code Standards:				
Does the proposed project exceed the maximum density allowed in the existing/proposed zoning district?	Yes <input checked="" type="checkbox"/>	No <input type="checkbox"/>	Calculate density: 22 du / acre. Applicant is currently requesting a density bonus.	
What is the total non-residential square foot (sf) and Floor Area Ratio? <input type="checkbox"/> N/A	Total Square Foot 27, 700 sf. (non-residential) FAR 1.13			
Does the proposed project meet setback standards? (AMC 9-4) <input type="checkbox"/> N/A	Yes <input checked="" type="checkbox"/>	No <input type="checkbox"/>	If no, explain:	
Does the proposed project meet maximum height standards? (AMC 9-4) <input type="checkbox"/> N/A	Yes <input type="checkbox"/>	No <input checked="" type="checkbox"/>	If no explain: A height waiver exception will need to be approved by the Planning Commission as it exceeds Downtown Commercial (DC) heights in the code.	
If the proposed project requires fencing, does it meet standards? (AMC 9-4) <input checked="" type="checkbox"/> N/A	Yes <input type="checkbox"/>	No <input type="checkbox"/>	If no, explain: No proposed fencing as a part of the development.	
If the proposed project requires landscaping, does it meet standards? (AMC 9-4 / AMC 8-10) <input type="checkbox"/> N/A	Yes <input checked="" type="checkbox"/>	No <input type="checkbox"/>	If no, explain:	
If the proposed project includes a parking requirement, does it meet standards? (AMC 9-4) <input type="checkbox"/> N/A	Yes <input checked="" type="checkbox"/>	No <input type="checkbox"/>	Parking Required: 72 spaces Parking Provided: 86 spaces	If no, explain:
If the proposed project includes lighting, does it meet standards? (AMC 9-4) <input type="checkbox"/> N/A	Yes <input checked="" type="checkbox"/>	No <input type="checkbox"/>	If no, explain:	



<p>Does the proposed project meet established standards for uses listed in AMC 9-6, if applicable?</p> <p><input checked="" type="checkbox"/> N/A</p>	<p>Yes <input type="checkbox"/></p>	<p>No <input type="checkbox"/></p>	<p>If no, explain:</p>
<p>Does the proposed project need any other exceptions to the City Zoning Ordinance?</p>	<p>Yes <input checked="" type="checkbox"/></p>	<p>No <input type="checkbox"/></p>	<p>If yes, explain: Height Waiver exception required; density bonus required in the form of high quality architecture – 15%</p>

Environmental Information			
<p>Is the proposed project under the screening criteria for Project Air Quality Analysis by SLOAPCD?</p>	<p>Yes <input type="checkbox"/></p>	<p>No <input checked="" type="checkbox"/></p>	<p>Notes: SLOAPCD does not have a “mixed-use category” so thresholds based on proposed mixed of uses. Contact SLO APCD for guidance.</p>
<p>Based on aerial photography of the site, will the project have an effect on any riparian or sensitive natural community identified in local or regional plans, policies, or regulations or by the California Department of Fish and Game or US Fish and Wildlife Service?</p>	<p>Yes <input type="checkbox"/></p>	<p>No <input checked="" type="checkbox"/></p>	<p>Notes:</p>
<p>Is the proposed project located on or near a known historical or cultural resource (Use GIS internal mapping)?</p>	<p>Yes <input type="checkbox"/></p>	<p>No <input checked="" type="checkbox"/></p>	<p>Notes:</p>
<p>Does the site contain any evidence of past landslides, unstable soils or serpentine rock?</p>	<p>Yes <input type="checkbox"/></p>	<p>No <input checked="" type="checkbox"/></p>	<p>Notes:</p>
<p>Does the proposed project include more than 50 cubic yards of grading?</p>	<p>Yes <input checked="" type="checkbox"/> (requires grading plan)</p>	<p>No <input type="checkbox"/></p>	<p>Notes: Grading and drainage plan has been submitted and will be reviewed by Public Works.</p>
<p>Does the proposed project including grading on slopes greater than 10 percent?</p>	<p>Yes <input type="checkbox"/></p>	<p>No <input type="checkbox"/></p>	<p>Notes:</p>



<p>Does the new project include more than 2,500 square feet of new or replacement impervious surface? (required for RWQCB Post Stormwater Construction Regulations)</p>	<p>Yes <input checked="" type="checkbox"/></p>	<p>No <input type="checkbox"/></p>	<p>Total amount of impervious surface 74,232 sf. Project will take credit for replacing surface. Previous site was 100% impervious.</p>
<p>Does the proposed project remove any native trees? (AMC 9-11)</p>	<p>Yes <input type="checkbox"/></p>	<p>No <input checked="" type="checkbox"/></p>	<p>Number of Trees proposed to be removed: _____ Total DBH proposed to be remove: _____</p>
<p>Is the project located on a site included on a list of hazardous materials sites compiled pursuant to Government Code Section 65962.5 (Cortese List)?</p>	<p>Yes <input type="checkbox"/></p>	<p>No <input checked="" type="checkbox"/></p>	<p>If Yes explain:</p>
Environmental Information			
<p>Does the proposed project alter the existing drainage pattern of the site or alter a designated waters of the US?</p>	<p>Yes <input type="checkbox"/></p>	<p>No <input checked="" type="checkbox"/></p>	<p>If Yes, explain:</p>
<p>Does the proposed project increase noise levels in excess of City Standards when the use is complete?</p>	<p>Yes <input checked="" type="checkbox"/></p>	<p>No <input type="checkbox"/></p>	<p>If Yes, explain: An acoustical noise study is required for proposed residential uses adjacent to Highway 101.</p>
<p>Does the proposed project increase temporary noise levels that cannot be mitigated by the City's existing Noise Ordinance?</p>	<p>Yes <input type="checkbox"/></p>	<p>No <input checked="" type="checkbox"/></p>	<p>If Yes, explain:</p>
<p>Does the proposed project require construction of new water and/wastewater treatment facilities?</p>	<p>Yes <input type="checkbox"/></p>	<p>No <input checked="" type="checkbox"/></p>	<p>If Yes, explain:</p>



Does the proposed project require the construction of new recreational facilities?	Yes <input type="checkbox"/>	No <input checked="" type="checkbox"/>	If Yes, explain: In-lieu fees are to be paid.	
Does the proposed project decrease the established traffic Level of Service below Level "C" as contained in the General Plan? (Use ITE Trip Generation for review)	Yes <input type="checkbox"/>	No <input checked="" type="checkbox"/>	Number of daily trips generated: 1,342 ADT PM Peak: 106 PM	If Yes, explain:
City Council Policy				
Is the project applicable to any of the following City Council policies?	Inclusionary Housing <input type="checkbox"/> Mixed-Use Processing <input checked="" type="checkbox"/> Park / Creek Reservation <input type="checkbox"/> Planned Development <input type="checkbox"/> Prime Commercial Sites <input type="checkbox"/>			



**ATTACHMENT 2:
MITIGATED NEGATIVE DECLARATION 2017-0018 AND ADDENDUM**

See following page



**ADDENDUM TO PROPOSED MITIGATED NEGATIVE
DECLARATION 2017-0018
PLN 2017-1649**



**City of Atascadero
Proposed Mitigated Negative Declaration – Errata
EDOC 2017-0018**

The following is an errata to the proposed mitigated negative declaration based on the following:

- Change in original project scope / project description;
- Comment letters received.

Change in project scope / project description

The project description has been modified since the publishing of the proposed mitigated negative declaration. The original scope included 40 residential apartment units. The applicant informed Staff on October 27, 2017, that they wanted to change the scope from apartment rental units to 40 residential condominium units that could be sold individually. The following is the revised project description to match the scope in redlines / strikeout:

Project Description – Errata

Proposed mixed-use redevelopment project on previously developed properties within Downtown Atascadero. Proposed project includes:

- 18,000 square feet (sf) of commercial type uses on the first floor
- 9,700 sf of office space on 2nd and 3rd floors of a separate building
- 40 **air space residential condominium** units on 2nd and 3rd floors
- 11,761 square foot (sf) pedestrian plaza located on Atascadero Mall

The proposed project includes 84 off-street parking spaces, and proposed road abandonment of a portion of El Camino Real, and partial vehicular closure of Atascadero Avenue (Atascadero Mall) to allow for construction of a plaza connecting Atascadero Avenue (west of Highway 101) to Downtown Atascadero through an active pedestrian link. Proposed height of all occupied structures is 45-feet, with an additional 20-feet for un-occupied architectural features. Total height of structures within the proposed development is 65-feet. An on-site freestanding pylon sign proposed to be a total height of 70-feet is proposed at the rear of the site between the building and Highway 101.

The proposed project is not located within a 100 year flood zone. There are no known wetlands or designated waters of the US on the proposed parcels for development, nor are there known mapped pre-historical or archeological resources known on-site. The site has been previously disturbed with development dating back to early 1900's, including the Atascadero Hotel, and other various forms of commercial development. The proposed project will include demolition of structures of existing structures, including a former gas station, a



former fast food restaurant and other small buildings that are currently utilized for commercial uses. An existing cell tower facility will remain on the site.

Effect on Circulated Proposed Mitigated Negative Declaration:

The proposed mitigated negative declaration analyzed 40 apartment units versus 40 air space residential condominium units. The change in project description requires a change in the analysis to the following sections:

- Transportation / Traffic.

The following are changes that reflect the change in project description to the proposed mitigated negative declaration:

16. Transportation / Traffic Proposed Project:

The proposed project includes a mix of residential and non-residential uses. City Staff completed an ITE trip generation model for the proposed project. The following ITE codes were utilized:

- ~~40 Low Rise Apartment Units (ITE 221)~~ **Residential Condo / Townhouse (ITE 230)**
- 0.30 acres City Park (ITE 411);
- 9,700 sf of General Office (ITE 710);
- 14,300 sf of Specialty Retail Center (ITE 826);
- 3,850 sf of Quality Restaurant (ITE 931).

ITE trip generation model rounds up square footage / acreage to nearest tenth. Based on this information, the proposed project would add ~~1,340~~ **1,309** daily trips, or an increase of 65% of the total daily trips from existing development. AM Peak Hour trips are ~~435~~ **134** trips generated and PM peak hour trips are ~~405~~ **103** trips total. The ITE trips model is based on a suburban land use model and assumes singular uses for each separate and does not take into account a mix of uses within a one building. Caltrans published studies in regards to urban infill and traffic generate. In 2009, Caltrans concluded that in many cases, urban infill development such as La Plaza trip generate rates are generally lower when compared to the ITE model (Caltrans: Trip Generation Rates for Urban Infill Land Uses in California).

Based on the amount of trips the proposed project will generate, the City Engineer has determined that the proposed project will not contribute to the derogation of LOS on El Camino Real to a LOS C or below, the threshold established by the City's General Plan. This includes both the PM and AM peak hour trips. This impact is considered less than significant.

Transportation / Traffic – Project Scope Determination

The changes from residential apartments to residential condominiums drop the anticipated number of trips by 2%. The proposed mitigated negative declaration examined a higher number; therefore, the mitigation measures, as proposed, remain



valid and the conclusions that transportation and traffic impacts are rendered to less than significant levels with implementation of proposed mitigation.

Comment Letters Received

Staff received two (2) comment letters from San Luis Obispo APCD, and Caltrans. Those comment letters are included as Attachment A. Based on both comment letters, the following revisions are shown in bold / underline for added mitigation and strikethrough format for deleted text.

Air Quality

AQ-8 **Should hydrocarbon contaminated soil be encountered during construction activities, the APCD must be notified as soon as possible and no later than 48 hours after affected material is discovered to determine if an APCD Permit will be required. In addition, the following measures shall be implemented immediately after contaminated soil is discovered:** During Construction

- **Covers on storage piles shall be maintained in place at all times in areas not actively involved in soil addition or removal;**
- **Contaminated soil shall be covered with at least six inches of packed uncontaminated soil or other TPH –non-permeable barrier such as plastic tarp. No headspace shall be allowed where vapors could accumulate;**
- **Covered piles shall be designed in such a way to eliminate erosion due to wind or water. No openings in the covers are permitted;**
- **The air quality impacts from the excavation and haul trips associated with removing the contaminated soil must be evaluated and mitigated if total emissions exceed the APCD’s construction phase thresholds;**
- **During soil excavation, odors shall not be evident to such a degree as to cause a public nuisance; and,**
- **Clean soil must be segregated from contaminated soil.**

The notification and permitting determination requirements shall be directed to the APCD Engineering & Compliance Division at (805) 781-5912.

Exhibit A – Comment Letters Received.



Exhibit A - Comment Letters Received

See following page



DEPARTMENT OF TRANSPORTATION

50 HIGUERA STREET
SAN LUIS OBISPO, CA 93401-5415
PHONE (805) 549-3101
FAX (805) 549-3329
TTY 711
<http://www.dot.ca.gov/dist05/>



*Making Conservation
a California Way of Life.*

October 27, 2017

SLO 101 45.86

Alfredo Castillo
Community Development Department
City of Atascadero
6500 Palma Avenue
Atascadero, CA 93422

COMMENTS TO LA PLAZA DOWNTOWN REDEVELOPMENT NOI/MND

Dear Mr. Castillo:

At this time Caltrans is unable to provide comment on the La Plaza Downtown Redevelopment Project. A traffic study for the project was not attached to the NOI/MND letter received by Caltrans on October 26, 2017 and was not readily available for review on the city of Atascadero's website. We have concerns about the potential traffic impacts this project may have on US 101 ramp operations. We look forward reviewing the traffic analysis and providing comments at a future time.

If you have any questions, or need further clarification on items discussed above, please contact me at (805) 549-3432 or Jenna.Schudson@dot.ca.gov.

Sincerely,

Jenna Schudson
Transportation Planner
Development Review Coordinator
District 5, LD-IGR South Branch



Air Pollution Control District
San Luis Obispo County



November 7, 2017

Alfredo Castillo, Planner, AICP
Atascadero Community Development Department
6500 Palma Ave.
Atascadero, CA 93422

SUBJECT: APCD Comments Regarding the Mitigated Negative Declaration for the La Plaza Downtown Redevelopment (Env. Doc. 2017-0018)

Dear Mr. Castillo:

Thank you for including the San Luis Obispo County Air Pollution Control District (APCD) in the environmental review process. We have completed our review of the proposed mixed-use re-development project on previously developed properties between 6300 and 6550 El Camino Real within downtown Atascadero and adjacent to the northbound US Highway 101 Traffic Way exit. The project would include:

- 18,000 square feet of commercial type uses on the first floor including a 3,749 square-foot sit down restaurant;
- 9,700 square feet of office space on 2nd and 3rd floors of a separate building;
- 40 units on 2nd and 3rd floors;
- 11,761 square feet pedestrian plaza located on Atascadero Mall.

The proposed project also includes 84 off-street parking spaces, and proposed road abandonment of a portion of El Camino Real, as well as partial vehicular closure of Atascadero Avenue (Atascadero Mall) to allow for construction of a plaza connecting Atascadero Avenue (west of Highway 101) to downtown Atascadero through an active pedestrian link. The project would include the demolition of existing structures including a former gas station, a former fast food restaurant, and other small buildings currently utilized for commercial use. *The following are APCD comments that are pertinent to this project.*

GENERAL COMMENTS

As a commenting agency in the California Environmental Quality Act (CEQA) review process for a project, the APCD assesses air pollution impacts from both the construction and operational phases of a project, with separate significant thresholds for each. **Please address the action items contained in this letter that are highlighted by bold and underlined text.**

In early October the APCD worked with you and identified five applicable standard construction phase mitigation measures for this project. Thank you for including these measures as AQ-1 to 5 in the Mitigated Negative Declaration (MND). In that early October work, the APCD also provided you with a screening air quality assessment of the proposed project's operational phase. We informed you that the screening results indicated that the operational phase of the project would be below the CEQA significance threshold for ozone precursors, but would be within 10% of the greenhouse gas (GHG) threshold. Therefore, the APCD recommended that the city accomplish a refined air quality evaluation using the CalEEMod land use emission model and account for the mitigation benefits from the project's site design, urban core proximity, and other applicable mitigation measures.

The MND includes a summary of these modeling results which demonstrate that the project's mitigation benefits will reduce the project's GHG impacts below the significance threshold. Beyond the site design and proximity measures that were already part of the project, the city also included a mitigation measure to incorporate four electrical vehicle charging stations (MND Measure GHG-1). The APCD supports this measure because it provides local support of the state goal to increase the number of zero emission vehicles in California. For the project's construction phase, the modeling also showed the need for architectural coating mitigation to be applied to the project and applicable mitigation measures were incorporated as AQ-6 and 7 into the MND. **The APCD supports the construction and operational phase mitigation measures included in the MND.**

Mixed Use Support

APCD staff traditionally commend the project proponents for projects that promote mixed used development. When people can walk to nearby stores, parks and work, traffic is reduced and the potential for mass transit use increases. This is consistent with several of the District's land use goals and policies in the Clean Air Plan. However, mixed use projects located adjacent to high-volume roadways can present a health risk to the residents who live there. Please see our additional comments on this risk concern below.

Infill within City Limits & Urban Reserve Lines

The APCD encourages balance of residential and commercial infill within the existing Urban Reserve Lines (URL), as this is consistent with the land use goals and policies of the APCD's Clean Air Plan. This project is an example of urban core infill which maximizes the ability of residents to live, work and shop locally, thus reducing the need to drive and minimizing vehicle exhaust emissions which account for over 50% of the county's air pollution including greenhouse gas emissions. Increasing urban core density can reduce trips and travel distances and encourage the use of alternative forms of transportation. However, again, please see our comments below regarding health risk concerns associated with this specific project.

CONSTRUCTION PHASE IMPACTS

Beyond the constructional phase comments that APCD previously provided, the following are additional APCD comments based on our review of the MND:

Hydrocarbon Contaminated Soil

Should hydrocarbon contaminated soil be encountered during construction activities, the APCD must be notified as soon as possible and no later than 48 hours after affected material

is discovered to determine if an APCD Permit will be required. In addition, the following measures shall be implemented immediately after contaminated soil is discovered:

- Covers on storage piles shall be maintained in place at all times in areas not actively involved in soil addition or removal;
- Contaminated soil shall be covered with at least six inches of packed uncontaminated soil or other TPH -non-permeable barrier such as plastic tarp. No headspace shall be allowed where vapors could accumulate;
- Covered piles shall be designed in such a way to eliminate erosion due to wind or water. No openings in the covers are permitted;
- The air quality impacts from the excavation and haul trips associated with removing the contaminated soil must be evaluated and mitigated if total emissions exceed the APCD's construction phase thresholds;
- During soil excavation, odors shall not be evident to such a degree as to cause a public nuisance; and,
- Clean soil must be segregated from contaminated soil.

The notification and permitting determination requirements shall be directed to the APCD Engineering & Compliance Division at 805-781-5912.

OPERATIONAL PHASE IMPACTS

Beyond the operational phase comments that APCD previously provided, the following are additional APCD comments based on our review of the MND:

Operational Permit Requirements

Based on the information provided, we are unsure of the types of equipment that may be present at the site. Operational sources may require APCD permits. The following list is provided as a guide to equipment and operations that may have permitting requirements, but should not be viewed as exclusive. For a more detailed listing, refer to the Technical Appendix, page 4-4, in the APCD's 2012 CEQA Handbook.

- Portable generators and equipment with engines that are 50 hp or greater;
- Electrical generation plants or the use of standby generator;
- Food and beverage preparation (primarily coffee roasters);
- Dry cleaning;
- Public utility facilities;
- Boilers;
- Internal combustion engines; and
- Cogeneration facilities.

Most facilities applying for an Authority to Construct or Permit to Operate with stationary diesel engines greater than 50 hp, should be prioritized or screened for facility wide health risk impacts. A diesel engine-only facility limited to 20 non-emergency operating hours per year or that has demonstrated to have overall diesel particulate emissions less than or equal to 2 lb/yr does not need to do additional health risk assessment. **To minimize potential delays, prior to the start of the project, please contact the APCD Engineering Division at 805-781-5912 for specific information regarding permitting requirements.**

November 7, 2017

Page 4 of 4

Health risk from a nearby high-volume roadway

While the ACPD supports mixed use, urban core infill developments like this project, development adjacent to a high-volume roadway can present a health risk to residents. The 2015 Caltrans US Highway 101 traffic volumes at this location exceed 60,000 average daily trips. Based on a review of information in the project referral, it appears that the project would result in the siting of future residents within approximately 100 to 280 feet from US Highway 101, which is considered a high-volume roadway. Diesel fueled trucks and cars travel on US Highway 101, so future residents living in the proposed residential units for this project would be exposed to diesel particulate matter (DPM), which has been classified by the state as a toxic air contaminant and a carcinogen.

Recognizing the public disclosure role of CEQA, the ACPD recommends that the lead agency estimate and disclose potential health risks from US Highway 101 on the future residents of this project for informational purposes. The APCD also recognizes that the lead agency has authority to require measures to protect public health and safety.

Should it be determined that mitigation of health impacts from this toxic emission source is necessary, the most effective way to do so is by locating the sensitive receptors (residential units) as far as feasible from the toxic source (US Highway 101), which will directly reduce cancer risk. Another option would be replacing residential units with nonresidential land uses. Please note that implementing strategies such as air filtration systems, sound walls, and vegetation barriers have not been proven to be as effective as moving a sensitive receptor farther from the toxic source.

Again, thank you for the opportunity to comment on this proposal. If you have any questions or comments, feel free to contact me at 805-781-5912.

Sincerely,



Andy Mutziger
Air Quality Specialist

AJM/ihs

cc: Z3 LLC
Dora Drexler, Enforcement Division, APCD
Gary Willey, Engineering Division, APCD

h:\plan\ceqa\project_review\4000\4000\4042-1\4042-1.docx

**ATTACHMENT 3: DRAFT PC RESOLUTION 2017-A – MITIGATED
NEGATIVE DECLARATION
PLN 2017-1649**

**DRAFT PC RESOLUTION 2017-A
CERTIFICATION OF MITIGATED NEGATIVE DECLARATION**

**A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF
ATASCADERO, CALIFORNIA, CERTIFYING
ENVIRONMENTAL DOCUMENT NUMBER 2017-0018**

**PLN 2017-1649
LA PLAZA DOWNTOWN REDEVELOPMENT
Z3 LLC**

WHEREAS, an application has been received from Z3 LLC, Owner / Applicant (8189 San Dimas Lane, Atascadero, CA 93422), to consider Planning Application PLN 2017-1649, for a project consisting of a master plan of development (CUP), tentative parcel map, roadway abandonment, and certification of a Mitigated Negative Declaration ("MND") on a 1.83 acre site located on 6300 through 6500 El Camino Real (APN's 030-193-001,003,017,016, 031,030,033); and,

WHEREAS, an Initial Study and Proposed Mitigated Negative Declaration, Environmental Document Number 2017-0018, were prepared for the project and made available for public review in accordance with the requirements of the California Environmental Quality Act (CEQA); and

WHEREAS, Section 21000, et. seq., of the Public Resources Code and Section 15000, et. seq., of Title 14 of the California Code of Regulations (the "CEQA Guidelines"), which govern the preparation, content, and processing of Negative Declarations, have been fully implemented in the preparation of the Mitigated Negative Declaration; and

WHEREAS, pursuant to California State Law and the Atascadero Municipal Code, public hearing notices were mailed to all property owners within an area exceeding a three hundred foot radius of the subject property and a public hearing was published for a minimum of 10 days prior to the first public hearing; and

WHEREAS, a duly noticed public hearing was held by the Atascadero Planning Commission on November, 21, 2017, to consider the Mitigated Negative Declaration to which all interested persons were given the opportunity to be heard and has recommended certification of the Mitigated Negative Declaration; and



WHEREAS, the Planning Commission of the City of Atascadero has reviewed and considered the information contained in the Mitigated Negative Declaration for the PLN 2015-1556, including all associated applications; and,

NOW, THEREFORE, BE IT RESOLVED, the Planning Commission of the City of Atascadero, hereby resolves to certify Proposed Mitigated Negative Declaration, Environmental Document Number 2017-0018 based on the following Findings:

SECTION 1. Findings for Certification of Mitigated Negative Declaration. The Planning Commission finds as follows:

1. The Proposed Mitigated Negative Declaration has been completed in compliance with CEQA.

Fact. City Staff has prepared a proposed Mitigated Negative Declaration consistent with sections 15070, 15071, 15072, 15073 of the CEQA Guidelines, as the initial study completed by City staff showed that revisions, known as mitigation measures, have been agreed to by the applicant that would mitigate potential significant environmental effects to a point where clearly no significant effects would occur.

2. The Proposed Mitigated Negative Declaration was presented to the Planning Commission, and the information contained therein was considered by the Planning Commission, prior to recommending action on the project for which it was prepared.

Fact. City Staff delivered a Staff report with the proposed project and Mitigated Negative Declaration under its timeframes for proper review and vetting by Planning Commissioners.

3. The project does not have the potential to degrade the environment when mitigation measures are incorporated into the project.

Fact. The proposed project contains mitigation measures that clarify City Policies, General Plan policies, and other pertinent development-related measures to be completed by the project applicant that mitigated potential significant environmental effects.

4. The project will not achieve short-term to the disadvantage of long-term environmental goals.

Fact. The proposed project is consistent with the City's underlying zoning ordinance, as well as guiding policy document, the City's General Plan. The proposed project promotes orderly development with high quality architecture and environmental design.

5. The project does not have impacts which are individually limited, but cumulatively considerable.



Fact. The Mitigated Negative Declaration considered existing plus project, as well as cumulative plus project impacts in its analysis. The proposed project did not produce any potential impacts that were cumulatively considerable.

6. The project will not cause substantial adverse effects on human beings either directly or indirectly.

Fact. The proposed project will not cause substantial adverse effects on human beings, either directly or indirectly, as the proposed project includes the construction and development of 18,000 square feet (sf) of commercial type uses, 9,070 sf of office uses, 40 air space residential condominium units, and a 11,761 sf pedestrian plaza as a private redevelopment project within Downtown Atascadero.

EXHIBIT A: Environmental Document Number 2017-0018, Mitigated Negative Declaration Mitigation Measures

BE IT FURTHER RESOLVED, that a copy of this Resolution be delivered forthwith by the Planning Commission Secretary to the City Council of the City of Atascadero.

On motion by Commissioner _____ and seconded by Commissioner ____ the foregoing resolution is hereby adopted in its entirety by the following roll call vote:

AYES:	0
NOES:	0
ABSTAIN:	0
ABSENT:	0
ADOPTED:	

CITY OF ATASCADERO, CA

Duane Anderson
Planning Commission Chairperson

Attest:

Phil Dunsmore
Planning Commission Secretary



**EXHIBIT A: ENVIRONMENTAL DOCUMENT NUMBER 2017-0009,
 MITIGATED NEGATIVE DECLARATION
 MITIGATION MEASURES**

**MITIGATION SUMMARY TABLE
 La Plaza Downtown Redevelopment
 PLN 2017-1649**

Per Public Resources Code § 21081.6, the following measures also constitutes the mitigation monitoring and/or reporting program that will reduce potentially significant impacts to less than significant levels. The measures will become conditions of approval (COAs) should the project be approved. The City of Atascadero, as the Lead Agency, or other responsible agencies, as specified, are responsible to verify compliance with these COAs.

MITIGATION MEASURE	TIMING
Aesthetics	
AES-1 At the time of building permit submittal of the proposed project, applicant must submit a photometric plan showing locations of proposed on-sight lighting. Prior to final occupancy, City Staff and the applicant shall meet on-site and review lights at dusk condition to ensure off-site light spillage and glare are limited.	Prior to Building Permit Issuance
AES-2 Any luminaire pole height shall not exceed 14-feet in height to minimize off-site light spillage for consistency with the Atascadero Municipal Code.	Prior to Building Permit Issuance
AES-3 Limit intensity to up to 3.0 foot candles at ingress/egress, and otherwise 0.6 foot candle minimum to 1.0 maximum in parking areas and/or for street lighting, bollards, etc to be reviewed at the time of building final.	Prior to Building Permit Final
Air Quality	
AQ-1 Demolition activities can have potential negative air quality impacts, including issues surrounding proper handling, abatement, and disposal of asbestos containing material (ACM). Asbestos containing materials could be encountered during the demolition or remodeling of existing structures or the disturbance, demolition, or relocation of above or below ground utility pipes/pipelines (e.g., transit pipes or insulation on pipes). If this project will include any of these activities, then it may be subject to various regulatory jurisdictions, including the requirements stipulated in the National Emission Standard for Hazardous Air Pollutants. These requirements include, but are not limited to: 1) written notification, within at	Prior to Building Permit Issuance



MITIGATION MEASURE

TIMING

least 10 business days of activities commencing, to the APCD, 2) asbestos survey conducted by a Certified Asbestos Consultant, and, 3) applicable removal and disposal requirements of identified ACM. Please contact the APCD Engineering & Compliance Division at (805) 781-5912 for further information or go to slocleanair.org/rules-regulations/asbestos.php for further information. To obtain a Notification of Demolition and Renovation form go to the "Other Forms" section of slocleanair.org/library/download-forms.php.

AQ-2 Based on the information provided, San Luis Obispo APCD is unsure of the types of equipment that may be present during the project's construction phase. Portable equipment, 50 horsepower (hp) or greater, used during construction activities may require California statewide portable equipment registration (issued by the California Air Resources Board) or an APCD permit. During Construction

The following list is provided as a guide to equipment and operations that may have permitting requirements, but should not be viewed as exclusive. For a more detailed listing, refer to the Technical Appendices, page 4-4, in the APCD's 2012 CEQA Handbook.

- Power screens, conveyors, diesel engines, and/or crushers;
- Portable generators and equipment with engines that are 50 hp or greater;
- Electrical generation plants or the use of standby generator;

Internal combustion engines;

- Rock and pavement crushing;
- Unconfined abrasive blasting operations;
- Tub grinders;
- Trommel screens; and,
- Portable plants (e.g. aggregate plant, asphalt batch plant, concrete batch plant, etc).

To minimize potential delays, prior to the start of the project, please contact the APCD Engineering & Compliance Division at (805) 781-5912 for specific information regarding permitting requirements.

AQ-3 Effective February 25, 2000, the APCD prohibited developmental burning of vegetative material within San Luis Obispo County. If you have any questions regarding these requirements, contact the APCD Engineering & Compliance Division at (805) 781-5912. During Construction



MITIGATION MEASURE

TIMING

AQ-4

Construction activities can generate fugitive dust, which could be a nuisance to local residents and businesses in close proximity to the proposed construction site. Projects with grading areas that are greater than 4-acres or are within 1,000 feet of any sensitive receptor shall implement the following mitigation measures to manage fugitive dust emissions such that they do not exceed the APCD's 20% opacity limit (APCD Rule 401) or prompt nuisance violations (APCD Rule 402):

During
Construction

- a. Reduce the amount of the disturbed area where possible;
- b. Use of water trucks or sprinkler systems in sufficient quantities to prevent airborne dust from leaving the site and from exceeding the APCD's limit of 20% opacity for greater than 3 minutes in any 60 minute period. Increased watering frequency would be required whenever wind speeds exceed 15 mph. Reclaimed (non-potable) water should be used whenever possible. **Please note that since water use is a concern due to drought conditions, the contractor or builder shall consider the use of an APCD-approved dust suppressant where feasible to reduce the amount of water used for dust control.** For a list of suppressants, see Section 4.3 of the CEQA Air Quality Handbook
- c. All dirt stock pile areas should be sprayed daily and covered with tarps or other dust barriers as needed;
- d. Permanent dust control measures identified in the approved project revegetation and landscape plans should be implemented as soon as possible, following completion of any soil disturbing activities;
- e. Exposed ground areas that are planned to be reworked at dates greater than one month after initial grading should be sown with a fast germinating, non-invasive, grass seed and watered until vegetation is established;
- f. All disturbed soil areas not subject to revegetation should be stabilized using approved chemical soil binders, jute netting, or other methods approved in advance by the APCD;
- g. All roadways, driveways, sidewalks, etc. to be paved should be completed as soon as possible. In addition, building pads should be laid as soon as possible after grading unless seeding or soil binders are used;
- h. Vehicle speed for all construction vehicles shall not exceed 15 mph on any unpaved surface at the construction site;



MITIGATION MEASURE

TIMING

- i. All trucks hauling dirt, sand, soil, or other loose materials are to be covered or should maintain at least two feet of freeboard (minimum vertical distance between top of load and top of trailer) in accordance with CVC Section 23114;
- j. “Track-Out” is defined as sand or soil that adheres to and/or agglomerates on the exterior surfaces of motor vehicles and/or equipment (including tires) that may then fall onto any highway or street as described in California Vehicle Code Section 23113 and California Water Code 13304. To prevent ‘track out’, designate access points and require all employees, subcontractors, and others to use them. Install and operate a ‘track-out prevention device’ where vehicles enter and exit unpaved roads onto paved streets. The ‘track-out prevention device’ can be any device or combination of devices that are effective at preventing track out, located at the point of intersection of an unpaved area and a paved road. Rumble strips or steel plate devices need periodic cleaning to be effective. If paved roadways accumulate tracked out soils, the track-out prevention device may need to be modified;
- k. Sweep streets at the end of each day if visible soil material is carried onto adjacent paved roads. Water sweepers shall be used with reclaimed water should be used where feasible. Roads shall be pre-wetted prior to sweeping when feasible;
- l. All PM₁₀ mitigation measures required should be shown on grading and building plans; and,
- m. The contractor or builder shall designate a person or persons to monitor the fugitive dust emissions and enhance the implementation of the measures as necessary to minimize dust complaints, reduce visible emissions below the APCD’s limit of 20% opacity for greater than 3 minutes in any 60 minute period. Their duties shall include holidays and weekend periods when work may not be in progress. The name and telephone number of such persons shall be provided to the APCD Engineering & Compliance Division prior to the start of any grading, earthwork or demolition.

AQ-5	This project is in close proximity to nearby sensitive receptors. Projects that will have diesel powered construction activity in close proximity to any sensitive receptor shall implement the following mitigation measures to ensure that public health benefits are realized by reducing toxic risk from diesel emissions:	During Construction
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MITIGATION MEASURE

TIMING

To help reduce sensitive receptor emissions impact of diesel vehicles and equipment used to construct the project, the applicant shall implement the following idling control techniques:

1. California Diesel Idling Regulations
 - a. ***On-road diesel vehicles*** shall comply with Section 2485 of Title 13 of the California Code of Regulations. This regulation limits idling from diesel-fueled commercial motor vehicles with gross vehicular weight ratings of more than 10,000 pounds and licensed for operation on highways. It applies to California and non-California based vehicles. In general, the regulation specifies that drivers of said vehicles:
 1. Shall not idle the vehicle's primary diesel engine for greater than 5 minutes at any location, except as noted in Subsection (d) of the regulation; and,
 2. Shall not operate a diesel-fueled auxiliary power system (APS) to power a heater, air conditioner, or any ancillary equipment on that vehicle during sleeping or resting in a sleeper berth for greater than 5.0 minutes at any location when within 1,000 feet of a restricted area, except as noted in Subsection (d) of the regulation.
 - b. ***Off-road diesel equipment*** shall comply with the 5-minute idling restriction identified in Section 2449(d) (2) of the California Air Resources Board's In-Use Off-Road Diesel regulation.
 - c. Signs must be posted in the designated queuing areas and job sites to remind drivers and operators of the state's 5-minute idling limit.
 - d. The specific requirements and exceptions in the regulations can be reviewed at the following web sites: www.arb.ca.gov/msprog/truck-idling/2485.pdf and www.arb.ca.gov/regact/2007/ordiesl07/froal.pdf.

AND/OR

2. Diesel Idling Restrictions Near Sensitive Receptors

In addition to the state required diesel idling requirements, the project applicant shall comply with these more restrictive requirements to minimize impacts to nearby sensitive receptors:



MITIGATION MEASURE

TIMING

- a. Staging and queuing areas shall not be located within 1,000 feet of sensitive receptors;
- b. Diesel idling within 1,000 feet of sensitive receptors shall not be permitted;
- c. Use of alternative fueled equipment is recommended; and
- d. Signs that specify the no idling areas must be posted and enforced at the site.

AQ-6 Exterior architectural coating shall be limited to Volatile Organic Compound (VOC) of 50 grams per liter maximum to the extent feasible. During Construction

AQ-7 Interior architectural coating shall be limited to Volatile Organic Compound (VOC) of 0 grams per liter maximum to the extent feasible within residential and non-residential spaces. During Construction

Green House Gas Emissions

GHG-1 The proposed project shall include a minimum of four (4) electric vehicle (EV) chargers within the proposed parking lot. Vehicle chargers shall be counted as parking spaces for the purposes of parking calculations. Prior to Building Permit Final

Cultural Resources

CR-1 Recordation and documentation shall be produced according to the California Register of Historic Resources prior to issuance of demolition permits for both 6452 and 6490 El Camino Real. Documentation shall include the following: Prior to Demo Permit Issuance

- Completion of State of California Form DPR 523B;
- Large scale color photographs of exterior and interior with scale and caption;
- Large scale site plan and floor plan of existing building(s) with legend and callouts of location photographs taken;
- Copies of selected historic photographs;
- Written description of construction and use of the structures;
- Digital copies of all materials submitted in PDF format.

CR-2 In the event that human remains are discovered on the property, all work on the project shall stop and the Atascadero Police Department and the County Coroner shall be contacted immediately. The Atascadero Community Development Director shall be notified. If the human remains are identified as being native American, the California Native American Heritage Commission (NAHC) shall be contacted at (916) 653-4082 within 24 hours. A representative from both During Construction



MITIGATION MEASURE

TIMING

the Chumash Tribe and the Salinan Tribe shall be notified during the excavation of any remains.

Noise

- | | | |
|------|--|-----------------------------------|
| NO-1 | Soffit vents, eave vents, dormer vents and other wall and roof penetrations shall be on the walls and roofs facing away from the noise source wherever possible. | Prior to Building Permit Issuance |
| NO-2 | The walls of habitable spaces and office space on second and third stories nearest the noise source shall have wall construction with an S.T.C. (Sound Transmission Class) rating of 30 or greater. For instance, stucco exterior or equivalent on 2" x 4" stud walls with minimum R-13 batt insulation and two layers of ½" gypsum board on the interior will provide an S.T.C. rating of 30 or greater along these walls. | Prior to Building Permit Issuance |
| NO-3 | Common acoustic leaks, such as electrical outlets, pipes, vents, ducts, flues, and other breaks in the integrity of the wall, ceiling or roof construction on the side of the dwellings nearest transportation noise sources shall receive special attention during construction. All construction openings and joints on the walls on the noise facing side of the site shall be insulated, sealed and caulked with a resilient, non-hardening, acoustical caulking material. All such openings and joints shall be airtight to maintain sound isolation. | Prior to Building Permit Issuance |
| NO-4 | To meet the interior LDN 45 dBA requirements, windows for habitable spaces on the second and third floors of affected units facing the noise source shall be of double-glazed construction with one light of laminated glass, and installed in accordance with the recommendations of the manufacturer. The windows shall have full gaskets, with an S.T.C. rating of 30 or better, as determined in testing by an accredited acoustical laboratory. | Prior to Building Permit Issuance |

Transportation & Traffic

- | | | |
|------|---|---|
| TP-1 | The applicant or developer must relocate the existing pedestrian crossing at the midblock crossing at the intersection of East Mall and El Camino Real to a location to the north subject to the approval of the Public Works Director. The preferred location would be at a signalized intersection. The applicant shall re-stripe and provide conduit for a new pedestrian crossing signal adjacent to the project site. The proposed improvement may be eligible for TIF credits based on the adopted Capital Improvement Plans at the time of building permit issuance. | Prior to release final sign off of off-site improvements. |
| TP-2 | The applicant shall work with City Staff and the Regional Transit Authority to relocate the adjacent southbound RTA stop that currently exists near the intersection of Entrada and El Camino Real. This includes relocation or re-construction of | Prior to release final sign off of off-site improvements. |



MITIGATION MEASURE

TIMING

a bus shelter, as deemed appropriate by the City Engineer.

Utilities Services

- | | | |
|------|---|--|
| UT-1 | The proposed project must pay all applicable sewer connection fees at the time of building permit issuance. A fee credit may be applicable for all existing fixtures and connections listed in demolition permits for buildings removed. | Prior to Building Permit Issuance |
| UT-2 | The applicant shall be responsible for providing fair share impact fees for wastewater in effect at the time of building permit issuance. | Prior to Building Permit Issuance |
| UT-3 | The developer must include trash and recycle receptacles near public entries and a minimum of (3) trash and recycle receptacles within the proposed plaza to be dispersed throughout the length of the plaza. Receptacles design shall be approved by Staff and included on landscaping plans at the time of building permit submittal. | Prior to release final sign off of on-site improvements. |



OCT 18 2017



CITY OF ATASCADERO

COMMUNITY DEVELOPMENT DEPARTMENT

TOMMY GONG, COUNTY CLERK

 DEPUTY CLERK

**Notice of Intent to Adopt
 Mitigated Negative Declaration**

PLN NO.	2017-1649	Environmental Document No.	2017-0018
PROJECT TITLE	La Plaza Downtown Redevelopment		
APPLICANT NAME & PHONE NUMBER	Z3 LLC (805) 674-3414	Email	plblote@rrmdesign.com
MAILING ADDRESS:	8190 San Dimas Ln	Atascadero, CA	93422
STAFF CONTACT:	Alfredo Castillo	(805) 470-3436	acastillo@atascadero.org
PROJECT ADDRESS:	6500 El Camino Real	Atascadero, CA 93422	APN: 030-193-003

PROJECT DESCRIPTION:

Proposed mixed-use redevelopment project on previously developed properties within Downtown Atascadero. Proposed project includes:

- 18,000 square feet (ft) of commercial type uses on the first floor;
- 9,700 sf of office space on 2nd and 3rd floors of a separate building;
- 40 units on 2nd and 3rd floors;
- 11,761 square foot (sf) pedestrian plaza located on Atascadero Mall.

The proposed project includes 84 off-street parking spaces, and proposed road abandonment of a portion of El Camino Real, and partial vehicular closure of Atascadero Avenue (Atascadero Mall) to allow for construction of a plaza connecting Atascadero Avenue (west of Highway 101) to Downtown Atascadero through an active pedestrian link. Proposed height of all occupied structures is 45-feet, with an additional 20-feet for un-occupied architectural features. Maximum height of structures within the proposed development is 65-feet. An on-site freestanding pylon sign proposed to be a total height of 70-feet is proposed between the building and Highway 101.

The proposed project is not located within a 100 year flood zone. There are no known wetlands or designated waters of the US on the proposed parcels for development, nor are there known mapped pre-historical or archeological resources known on-site. The site has been previously disturbed with development dating back to early 1900's, including the Atascadero Hotel, retail development, etc. The proposed project will include demolition of structures of existing structures including a former gas station, a former fast food restaurant and other small buildings that are currently utilized for commercial uses. An existing cell tower facility will remain on the site.

LEAD AGENCY: City of Atascadero
 Community Development Department
 6500 Palma Avenue
 Atascadero, CA 93422

DOCUMENT AVAILABLE ONLINE: <http://www.atascadero.org/environmentaldocs>

STATE CLEARING HOUSE REVIEW: Yes NO

REVIEW PERIOD BEGINS: 10/18/2017 **REVIEW PERIOD ENDS:** 11/07/2017

PUBLIC HEARING REQUIRED: No Yes November 7, 2017 at 6PM, City Council Chambers, 6500 Palma Avenue, Atascadero, CA 93422

PUBLIC NOTICE: The City of Atascadero is releasing a draft Initial Study and Mitigated Negative declaration at the above *project address* for review and comment to all effected agencies, organizations, and interested parties. Reviewers should focus on the content and accuracy of the report and the potential impacts upon the environment. The notice for this project is in compliance with the California Environmental Quality Act (CEQA). Persons responding to this notice are urged to submit their comments in writing. Written comments should be delivered the City (lead agency) no later than 5pm on the date listed as "review period ends". Submittal of written comments via email is also accepted and should be directed to the Staff contact at the above email address. This document may be viewed by visiting the Community Development Department, listed under the lead agency address, or accessed via the City's website.



CITY OF ATASCADERO

COMMUNITY DEVELOPMENT DEPARTMENT

Initial Study Summary – Environmental Checklist

PLN NO. 2017-1649 **Environmental Document No.** 2017-0018

PROJECT TITLE: La Plaza Downtown Redevelopment

Environmental Factors Potentially Affected: The proposed project could have a "Potentially Significant Impact" for at least one of the environmental factors checked below. Please refer to the attached pages for discussion on mitigation measures or project revisions to either reduce these impacts to less than significant levels or require further analysis.		
<input checked="" type="checkbox"/> Aesthetics	<input checked="" type="checkbox"/> Geology and Soils	<input type="checkbox"/> Recreation
<input type="checkbox"/> Agricultural Resources	<input checked="" type="checkbox"/> Hazards / Hazardous Materials	<input checked="" type="checkbox"/> Transportation
<input checked="" type="checkbox"/> Air Quality	<input checked="" type="checkbox"/> Noise	<input checked="" type="checkbox"/> Wastewater
<input checked="" type="checkbox"/> Biological Resources	<input checked="" type="checkbox"/> Population / Housing	<input checked="" type="checkbox"/> Water / Hydrology
<input checked="" type="checkbox"/> Cultural Resources	<input checked="" type="checkbox"/> Public Services / Utilities	<input checked="" type="checkbox"/> Land Use

DETERMINATION: (To be completed by the Lead Agency)

On the basis of this initial evaluation, the Community Development Director finds that:

- The proposed project COULD NOT have a significant effect on the environment, and a NEGATIVE DECLARATION will be prepared.
- Although the proposed project could have a significant effect on the environment, there will not be a significant effect in this case because revisions in the project have been made by or agreed to by the project proponent. A MITIGATED NEGATIVE DECLARATION will be prepared.
- The proposed project MAY have a significant effect on the environment, and an ENVIRONMENTAL IMPACT REPORT is required.
- the proposed project MAY have a "potentially significant impact" or "potentially significant unless mitigated" impact on the environment, but at least one effect 1) has been adequately analyzed in an earlier document pursuant to applicable legal standards, and 2) has been addressed by mitigation measures based on the earlier analysis as described on attached sheets. An ENVIRONMENTAL IMPACT REPORT is required, but it must analyze only the effects that remain to be addressed.
- Although the proposed project could have a significant effect on the environment, because all potentially significant effects (a) have been analyzed adequately in an earlier EIR or NEGATIVE DECLARATION pursuant to applicable standards, and (b) have been avoided or mitigated pursuant to that earlier EIR or NEGATIVE DECLARATION, including revisions or mitigation measures that are imposed upon the proposed project, nothing further is required.

Alfredo Castillo, AICP

Prepared by (print)

Signature

10/18/17

Date

Phil Dunsmore, AICP

Reviewed by (Print)

Signature

10-18-17

Date

PROJECT ENVIRONMENTAL ANALYSIS

The City of Atascadero's environmental review process incorporates all of the requirements for completing the Initial Study as required by the California Environmental Quality Act (CEQA) and the CEQA Guidelines. The Initial Study includes Staff's on-site inspection of the project site and surrounding and a detailed review of the information on file for the proposed project. In addition, available background information is reviewed for each project. Relevant information regarding soil types and characteristics, geological information, significant vegetation and/or wildlife resources, water availability, wastewater disposal service, existing land uses and surrounding land use categories and other information relevant to the environmental review process are evaluated for each project. Exhibit A includes the references used, as well as the agencies or groups that were contacted as a part of this initial study. The City of Atascadero uses the checklist to summarize the results of the research accomplished during the initial environmental review of the project.

Persons, agencies, or organizations interested in obtaining more information regarding the environmental review process for a project should contact the Community Development Department, 6500 Palma Avenue, Atascadero, CA 93422 or call (805) 461-5000.

A. PROPOSED PROJECT

Description: Proposed mixed-use redevelopment project on previously developed properties within Downtown Atascadero. Proposed project include:

- 18,000 square feet (ft) of commercial type uses on the first floor;
- 9,700 sf of office space on 2nd and 3rd floors of a separate building;
- 40 units on 2nd and 3rd floors;
- 11,761 square foot (sf) pedestrian plaza located on Atascadero Mall.

The proposed project includes 84 off-street parking spaces, and proposed road abandonment of a portion of El Camino Real, and partial vehicular closure of Atascadero Avenue (Atascadero Mall) to allow for construction of a plaza connecting Atascadero Avenue (west of Highway 101) to Downtown Atascadero through an active pedestrian link Proposed height of all occupied structures is 45-feet, with an additional 20-feet for un-occupied architectural features. Total height of structures within the proposed development is 65-feet. An on-site freestanding pylon sign proposed to be a total height of 70-feet is proposed at the rear of the site between the building and Highway 101.

The proposed project is not located within a 100 year flood zone. There are no known wetlands or designated waters of the US on the proposed parcels for development, nor are there known mapped pre-historical or archeological resources known on-site. The site has been previously disturbed with development dating back to early 1900's, including the Atascadero Hotel, and other various forms of commercial development. The proposed project will include demolition of structures of existing structures including a former gas station, a former fast food restaurant and other small buildings that



are currently utilized for commercial uses. An existing cell tower facility will remain on the site.

Assessor parcel number(s): 030-193-001, 003, 016, 017,030, 031, 033
Latitude: 33.113597 **Longitude:** -117.270879

Other public agencies whose approval is required: Atascadero Mutual Water Company (AMWC), Regional Water Quality Control board (RWQCB), SLO Air Pollution Control (APCD).

B. EXISTING SETTING

Land use designation: Downtown (D)
Zoning district: Downtown Commercial (DC)
Parcel size: 1.83 acres
Topography: Relatively Flat **Average Slope:** >5% average
Vegetation: Urban Infill, previously developed
Existing use: Retail, Food and Beverage, and vacant / underutilized buildings.

Surrounding Land Use:

North:	South:	East:	West:
Financial Services / Downtown Commercial (DC)	Right-of-Way / Non-Conforming Uses / Downtown Commercial (DC)	Right-of-Way / Park / Downtown Commercial (DC)	Highway 101 / Right-of-Way

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C. ENVIRONMENTAL ANALYSIS

During the initial study process, at least one issue was identified as having a potentially significant environmental effect (see following Initial Study). The potentially significant items associated with the proposed project can be minimized to less than significant levels.



**CITY OF ATASCADERO
 INITIAL STUDY CHECKLIST**

1. AESTHETICS – Will the project:

	Potentially Significant	Impact Requires Mitigation	Insignificant Impact	Not Applicable
a) Have a substantial adverse effect on an adopted scenic vista?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Substantially damage scenic resources, including, but not limited to, trees, rock outcroppings, and historic buildings within a state scenic highway?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c) Substantially degrade the existing visual character or quality of the site and its surroundings?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d) Create a new source of substantial light or glare which would adversely affect day or nighttime views in the area?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

EXISTING SETTING: The proposed project is located between Highway 101 and the City’s primary arterial corridor: El Camino Real. The site includes a vacant building (former jack-in-the-box), and small commercial structures, which have been determined to be more than 50 years of age. There is also an existing wireless telecommunications tower. The proposed project is not located within an adopted scenic vista and there are no natural scenic resources on site or within the vicinity. Surrounding uses include non-residential uses, Atascadero City Hall, and the Sunken Gardens Park. All existing structures on-site are one-story in height, and contain standard commercial lighting.

PROPOSED PROJECT: The proposed project includes high quality architecture that compliments existing historical development and includes design elements such as pop-outs, roofline variations, and other architectural features. The proposed project utilizes color and materials consistent with Historic City Hall, the Printery building, and the Carlton Hotel.

The three-story buildings would exceed the City’s height limit requirements per Atascadero Municipal Code (AMC) Section 9-3.347 and the applicant requests a height exception to allow buildings with a maximum height of 45-feet for occupied portions and 65 feet for unoccupied architectural features (tower elements).. The applicant is also proposing a freestanding pylon sign adjacent to Highway 101 with a maximum height of 70-feet. The AMC allows a total height

of 18-feet within the project site. This height requirement was set to protect the view-shed between Highway 101 and the Historic City Hall. Vegetation along the Highway, maintained by Caltrans, has grown and obstructs the view-shed to City Hall. Because of the proposed plaza to the south of the project, some visibility of the Historic City Hall will be maintained from Highway 101. The proposed architecture and its elements allow for views of the surrounding hillside from the development, as well as serving as an additional marker for motorists utilizing Highway 101.

Additional new light sources may be generated that may affect existing nighttime views in the area. All existing lighting and signage on site is proposed for removal. The Atascadero Municipal Code requires that all proposed lighting be minimized in intensity and shielded to reduce light spillage onto other structures. The applicant has not provided a proposed lighting plan to show the type of overhead lighting, nor has the applicant included any proposed street lights to be installed along with frontage improvements. To reduce potential environmental effects, mitigation will be needed to ensure pole heights do not contribute to excess light pollution.

The AMC contains language under section 9-4.137, exterior lighting, stating that “no light glare shall be transmitted or reflected in such concentration or intensity as to be detrimental or harmful to persons or to interfere with the use of surrounding properties or streets.” All lighting shall be designed to eliminate any off-site glare, consistent with the City’s existing municipal code. Additionally, the code requires that all exterior lighting shall utilize full cut-off, “hooded” lighting fixtures to prevent offsite light spillage and glare. To ensure consistency with the code, mitigation measures will be required.

MITIGATION / CONCLUSION: To reduce potential impacts from the creation of new nighttime light and glare sources, the following mitigation measures shall be incorporated. With these measures, the potential new sources of substantial light and glare are considered less than significant.

AES – 1: At the time of building permit submittal of the proposed project, applicant must submit a photometric plan showing locations of proposed on-sight lighting. Prior to final occupancy, City Staff and the applicant shall meet on-site and review lights at dusk condition to ensure off-site light spillage and glare are limited.

AES – 2: Any luminaire pole height shall not exceed 14-feet in height to minimize off-site light spillage for consistency with the Atascadero Municipal Code.

AES – 3: Limit intensity to up to 3.0 foot candles at ingress/egress, and otherwise 0.6 foot candle minimum to 1.0 maximum in parking areas and/or for street lighting, bollards, etc. to be reviewed at the time of building final.

2. AGRICULTURE RESOURCES – Will the project:

	Potentially Significant	Impact Requires Mitigation	Insignificant Impact	Not Applicable
a) Convert Prime Farmland, Unique Farmland, or Farmland of Statewide Importance (Farmland) to nonagricultural use?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Conflict with existing zoning for agricultural use, or a Williamson Act contract?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c) Conflict with existing zoning for, or cause rezoning of, forest land, timberland or timberland zoned Timberland Production?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d) Result in the loss of forest land or conversion of forest land to non-forest use?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
e) Involve other changes in the existing environment which, due to their location or nature, could result in conversion of Farmland to non-agricultural use or conversion of forest land to non-forest use?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

EXISTING SETTING: The current general plan land use designation is downtown (D) and is currently contains existing non-residential development and a wireless communications facility which will remain. There are no agriculture activities occurring on-site.

MITIGATION / CONCLUSION: Agriculture resources are not located on-site, therefore no impact.

3. AIR QUALITY – Will the project:

	Potentially Significant	Impact Requires Mitigation	Insignificant Impact	Not Applicable
a) Conflict with or obstruct implementation of the applicable air quality plan?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
b) Violate any air quality standard or contribute substantially to an existing or projected air quality violation?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
c) Result in a cumulatively considerable net increase of any criteria pollutant for which the project region is non-attainment under an applicable federal or state ambient air quality standard (including	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

	Potentially Significant	Impact Requires Mitigation	Insignificant Impact	Not Applicable
releasing emissions which exceed quantitative thresholds for ozone precursors)?				
d) Expose sensitive receptors to substantial pollutant concentrations?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
e) Create objectionable odors affecting a substantial number of people?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

EXISTING SETTING: The project site is currently contains two retail uses, a drive-up coffee shop and a beauty supply store. Additionally, the site contains a vacant building, and a cellular communications facility that will remain. The remainder of the site is considered underutilized. The site is previously disturbed, as each parcel has been developed. There are two (2) schools and one (1) public park that fall within 1,000-feet of the project site, and are therefore are considered locations with sensitive receptors (children and park users). Within the 1,000-foot buffer from the project site are Atascadero Middle School, North County Christian School (K-12), and the Sunken Gardens Park, all of which are in use during typical construction hours.

PROPOSED PROJECT: City Staff, in concert with the San Luis Obispo Air Pollution Control District (SLOAPCD) screened the project for its potential air pollution impacts for both operational emissions (when the buildings are completed and occupied) and construction phase emissions. The screening criteria assumed the following:

- 3,749 sf quality restaurant;
- 40 low rise apartments;
- 9,070 sf of general office;
- 11,761 sf of City Park (Plaza);
- 24,563 sf of parking lot;
- 14,251 sf of strip mall retail.

According to the San Luis Obispo Air Pollution Control District (SLOAPCD), Operational screening Criteria for Project Air Quality Analysis (Table 1-1, SLOAPCD, 2012), the proposed project operational phase precursor would result in 15.286 pounds of Reactive Organic Gas (ROG) and Nitrogen Oxide (NOx) per day of operational emissions. This is under SLOAPCD's threshold of 25 pounds per day of operational emission, therefore the proposed project is considered less than significant. Typically, when a project is under the operational threshold for air emissions, it is assumed that the construction portion of the project is also under the district's established emissions thresholds. Based on the screening criteria established by SLOAPCD, the proposed project required additional analysis for GHG (See section 4). During this analysis, City Staff utilized the California Emissions Estimator Model (CalEEmod). Based on this analysis, mitigation to reduce architectural coating emissions of ROG and NOx was required to ensure the proposed project was below construction emission standards. Those mitigation measures are included in this section.

Additionally, the SLOAPCD requires mitigation measures be implemented for any development that may potentially emit diesel PM, to use precaution when within 1,000-feet of a sensitive receptor. SLOAPCD defines a sensitive receptor as “locations that include schools, parks and playgrounds, day care centers, nursing homes, hospitals, and residential dwelling units.” Mitigation measures included will help reduce the potential impacts on these sensitive receptors. Because the proposed project is located within 1,000 feet of sensitive receptors, there are applicable mitigation measures to reduce potential nuisance air quality impacts to less than significant level.

MITIGATION / CONCLUSION: To mitigate potential air quality impacts, the proposed project will require implementing the following mitigation measure. With this proposed mitigation measure, the project’s potential impacts will be considered less than significant.

AQ – 1: Demolition activities can have potential negative air quality impacts, including issues surrounding proper handling, abatement, and disposal of asbestos containing material (ACM). Asbestos containing materials could be encountered during the demolition or remodeling of existing structures or the disturbance, demolition, or relocation of above or below ground utility pipes/pipelines (e.g., transit pipes or insulation on pipes). If this project will include any of these activities, then it may be subject to various regulatory jurisdictions, including the requirements stipulated in the National Emission Standard for Hazardous Air Pollutants. These requirements include, but are not limited to: 1) written notification, within at least 10 business days of activities commencing, to the APCD, 2) asbestos survey conducted by a Certified Asbestos Consultant, and, 3) applicable removal and disposal requirements of identified ACM. Please contact the APCD Engineering & Compliance Division at (805) 781-5912 for further information or go to slocleanair.org/rules-regulations/asbestos.php for further information. To obtain a Notification of Demolition and Renovation form go to the “Other Forms” section of slocleanair.org/library/download-forms.php.

AQ – 2: Based on the information provided, San Luis Obispo APCD is unsure of the types of equipment that may be present during the project’s construction phase. Portable equipment, 50 horsepower (hp) or greater, used during construction activities may require California statewide portable equipment registration (issued by the California Air Resources Board) or an APCD permit.

The following list is provided as a guide to equipment and operations that may have permitting requirements, but should not be viewed as exclusive. For a more detailed listing, refer to the Technical Appendices, page 4-4, in the APCD’s 2012 CEQA Handbook.

- Power screens, conveyors, diesel engines, and/or crushers;
- Portable generators and equipment with engines that are 50 hp or greater;
- Electrical generation plants or the use of standby generator;

Internal combustion engines;

- Rock and pavement crushing;
- Unconfined abrasive blasting operations;
- Tub grinders;
- Trommel screens; and,
- Portable plants (e.g. aggregate plant, asphalt batch plant, concrete batch plant, etc).

To minimize potential delays, prior to the start of the project, please contact the APCD Engineering & Compliance Division at (805) 781-5912 for specific information regarding permitting requirements.

AQ – 3: Effective February 25, 2000, the APCD prohibited developmental burning of vegetative material within San Luis Obispo County. If you have any questions regarding these requirements, contact the APCD Engineering & Compliance Division at (805) 781-5912.

AQ – 4. Construction activities can generate fugitive dust, which could be a nuisance to local residents and businesses in close proximity to the proposed construction site. Projects with grading areas that are greater than 4-acres or are within 1,000 feet of any sensitive receptor shall implement the following mitigation measures to manage fugitive dust emissions such that they do not exceed the APCD's 20% opacity limit (APCD Rule 401) or prompt nuisance violations (APCD Rule 402):

- a. Reduce the amount of the disturbed area where possible;
- b. Use of water trucks or sprinkler systems in sufficient quantities to prevent airborne dust from leaving the site and from exceeding the APCD's limit of 20% opacity for greater than 3 minutes in any 60 minute period. Increased watering frequency would be required whenever wind speeds exceed 15 mph. Reclaimed (non-potable) water should be used whenever possible. **Please note that since water use is a concern due to drought conditions, the contractor or builder shall consider the use of an APCD-approved dust suppressant where feasible to reduce the amount of water used for dust control.** For a list of suppressants, see Section 4.3 of the CEQA Air Quality Handbook
- c. All dirt stock pile areas should be sprayed daily and covered with tarps or other dust barriers as needed;
- d. Permanent dust control measures identified in the approved project revegetation and landscape plans should be implemented as soon as possible, following completion of any soil disturbing activities;
- e. Exposed ground areas that are planned to be reworked at dates greater than one month after initial grading should be sown with a fast germinating, non-invasive, grass seed and watered until vegetation is established;
- f. All disturbed soil areas not subject to revegetation should be stabilized using approved chemical soil binders, jute netting, or other methods approved in advance by the APCD;
- g. All roadways, driveways, sidewalks, etc. to be paved should be completed as soon as possible. In addition, building pads should be laid as soon as possible after grading unless seeding or soil binders are used;
- h. Vehicle speed for all construction vehicles shall not exceed 15 mph on any unpaved surface at the construction site;
- i. All trucks hauling dirt, sand, soil, or other loose materials are to be covered or should maintain at least two feet of freeboard (minimum vertical distance between top of load and top of trailer) in accordance with CVC Section 23114;
- j. "Track-Out" is defined as sand or soil that adheres to and/or agglomerates on the exterior surfaces of motor vehicles and/or equipment (including tires) that may then fall onto any highway or street as described in California Vehicle Code Section 23113 and California Water Code 13304. To prevent 'track out', designate access points and require all employees, subcontractors, and others to use them. Install and operate a 'track-out prevention device' where vehicles enter and exit unpaved roads onto paved streets. The 'track-out prevention device' can be any device or combination of devices that are effective at preventing track out, located at the point of intersection of an unpaved area and a paved road. Rumble strips or steel plate devices need periodic cleaning to be effective. If paved roadways accumulate tracked out soils, the track-out prevention device may need to be modified;
- k. Sweep streets at the end of each day if visible soil material is carried onto adjacent paved roads. Water sweepers shall be used with reclaimed water should be used where

- feasible. Roads shall be pre-wetted prior to sweeping when feasible;
- I. All PM₁₀ mitigation measures required should be shown on grading and building plans; and,
 - m. The contractor or builder shall designate a person or persons to monitor the fugitive dust emissions and enhance the implementation of the measures as necessary to minimize dust complaints, reduce visible emissions below the APCD's limit of 20% opacity for greater than 3 minutes in any 60 minute period. Their duties shall include holidays and weekend periods when work may not be in progress. The name and telephone number of such persons shall be provided to the APCD Engineering & Compliance Division prior to the start of any grading, earthwork or demolition.

AQ – 5. This project is in close proximity to nearby sensitive receptors. Projects that will have diesel powered construction activity in close proximity to any sensitive receptor shall implement the following mitigation measures to ensure that public health benefits are realized by reducing toxic risk from diesel emissions:

To help reduce sensitive receptor emissions impact of diesel vehicles and equipment used to construct the project, the applicant shall implement the following idling control techniques:

1. California Diesel Idling Regulations
 - a. **On-road diesel vehicles** shall comply with Section 2485 of Title 13 of the California Code of Regulations. This regulation limits idling from diesel-fueled commercial motor vehicles with gross vehicular weight ratings of more than 10,000 pounds and licensed for operation on highways. It applies to California and non-California based vehicles. In general, the regulation specifies that drivers of said vehicles:
 1. Shall not idle the vehicle's primary diesel engine for greater than 5 minutes at any location, except as noted in Subsection (d) of the regulation; and,
 2. Shall not operate a diesel-fueled auxiliary power system (APS) to power a heater, air conditioner, or any ancillary equipment on that vehicle during sleeping or resting in a sleeper berth for greater than 5.0 minutes at any location when within 1,000 feet of a restricted area, except as noted in Subsection (d) of the regulation.
 - b. **Off-road diesel equipment** shall comply with the 5-minute idling restriction identified in Section 2449(d) (2) of the California Air Resources Board's In-Use Off-Road Diesel regulation.
 - c. Signs must be posted in the designated queuing areas and job sites to remind drivers and operators of the state's 5-minute idling limit.
 - d. The specific requirements and exceptions in the regulations can be reviewed at the following web sites: www.arb.ca.gov/msprog/truck-idling/2485.pdf and www.arb.ca.gov/regact/2007/ordiesl07/frooal.pdf.

AND/OR

2. Diesel Idling Restrictions Near Sensitive Receptors

In addition to the state required diesel idling requirements, the project applicant shall comply with these more restrictive requirements to minimize impacts to nearby sensitive receptors:

 - a. Staging and queuing areas shall not be located within 1,000 feet of sensitive receptors;
 - b. Diesel idling within 1,000 feet of sensitive receptors shall not be permitted;
 - c. Use of alternative fueled equipment is recommended; and
 - d. Signs that specify the no idling areas must be posted and enforced at the site.

AQ – 6. Exterior architectural coating shall be limited to Volatile Organic Compound (VOC) of 50 grams per liter maximum to the extent feasible.

AQ – 7. Interior architectural coating shall be limited to Volatile Organic Compound (VOC) of 0 grams per liter maximum to the extent feasible within residential and non-residential spaces.

4. GREENHOUSE GAS EMISSIONS – Will the project:

	Potentially Significant	Impact Requires Mitigation	Insignificant Impact	Not Applicable
a) Generate greenhouse gas emissions, either directly or indirectly, that may have a significant impact on the environment?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
b) Conflict with an applicable plan, policy or regulation adopted for the purpose of reducing the emissions of greenhouse gases? b) Conflict with an applicable plan, policy or regulation adopted for the purpose of reducing the emissions of greenhouse gases?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

EXISTING SETTING: The project site consists of two small retail operations (coffee shop, beauty shop), a wireless telecommunications facility, and a vacant building. The site has been previously developed with a gas station, a hotel, and retail spaces. The gas station has been converted into a drive-up coffee shop. The hotel and many of the previous retail spaces were destroyed in fires, with the hotel destroyed in 1965, and many of the retail spaces in 2009. None of the existing uses generate greenhouse gas emissions (GHG) that have a significant impact on the environment, as most of the emissions are created from non-point source entities (i.e. vehicles).

PROPOSED PROJECT: The proposed project includes a true “mixed-use” project with 40 residential units, 18,000 sf of non-residential uses, and 9,700 sf of office uses. The proposed project is located on an existing transit line with a bus stop less than 500 feet away. With a mix of office, retail, restaurant type uses, and residential uses, the project more than likely will lead to a reduction of vehicle miles traveled within the City. The proposed project includes high quality landscaping along the street frontage, in the parking lot, and within a proposed plaza. This landscaping reduces Carbon Dioxide and increases oxygen in the environment, as well as, improves visual site quality. Landscaping within the parking portion of the project also reduces the “heat island” effect caused by new additional paving of the site for parking purposes.

Because the proposed project is a vertical mixed-use project, City Staff reached out to the San Luis Obispo Air Pollution Control District (APCD) for screening analysis for GHG emissions based on the district’s GHG brightline Metric Tons (MT/yr) per year threshold. The proposed project is estimated to produce 1,098.83 metric tons of GHG per year. APCD’s threshold for GHG annual operation is 1,035 MT/yr and the district recommended that a more detailed modeling be completed. City Staff utilized CalEEMod for its analysis and assumed the following:

- 3,749 sf quality restaurant;
- 40 low rise apartments;
- 9,070 sf of general office;
- 11,761 sf of City Park (Plaza);

- 24,563 sf of parking lot;
- 14,251 sf of strip mall retail.

The initial screening criterion only takes into account the uses of the proposed project. The CalEEMod model takes into account other factors, specifically GHG operations and land use patterns such as distance to transit, improvement to pedestrian facilities, the number of trees replanted, reduction in parking etc. City Staff utilized a number of land use site enhancements that are proposed by the project as “self-mitigating” features. Those include the following:

- Increase in density based on the model’s assumption (22 du/acre);
- Increase in diversity of the development (land use mix);
- Improve walkability design (design of wide pedestrian sidewalks, private plazas, and proposed reduction of Atascadero Mall);
- Distance to downtown / destination accessibility;
- Increase in transit accessibility;
- Improvement of the pedestrian network, particularly connecting Downtown Atascadero to Atascadero High School and other uses to the west, including higher density multi-family, through a public plaza;
- Traffic calming measures including the use of bulb-outs;
- Limit parking supply through the use of the City’s Downtown parking standards for non-residential uses;
- Implementation of a neighborhood electric vehicle network (NEV).

All of these with the exception of the NEV are already included by design of the project. In order to obtain credit for the NEV network implementation, approximately 5% of designated parking spaces will need to provide an Electric Vehicle charger. This equates to a total of 4 parking spaces that would be dedicated to either electric hybrid or all electrical vehicles. With the land use and site enhancements, Annual operational GHG emissions from the proposed project is 596.1848 of CO²e, which is 57% below APCD’s brightline threshold. To ensure that mitigated GHG emissions is consistent with the model performed by Staff, a mitigation measure has been added for all land use and site enhancements not proposed in site plans, or architectural details.

The City of Atascadero has adopted a Climate Action Plan (CAP) that addresses GHG. The CAP is a long-range policy geared towards reducing greenhouse gas (GHG) emissions and maintaining safe air quality as development within the City increases. The plan also outlines several community goals such as lowering energy costs, reducing air pollution, supporting local economic development, and improving public health and quality of life.

The CAP estimates that upon General Plan buildout by the year 2020, the City will have increased GHG emissions by 22% (Table ES-1 of the ACAP). The proposed project site is designated Downtown Commercial (DC) and is accounted for in the GHG emissions forecast generated by the City. The proposed project does not pass the threshold of GHG emissions as identified in the SLOAPCD, therefore is in compliance with the Climate Action Plan’s goal of a 15% emission decrease by the year 2020. Transportation is the largest emission source, accounting for 39% of the overall GHG emissions forecast. The projects central location and mix of uses more than likely reduce single occupancy vehicle dependency because of the close availability of transit and active transportation network that is in place. Therefore the project is consistent with the Atascadero Climate Action Plan.

MITIGATION / CONCLUSION: The proposed project is consistent with the City’s adopted Climate Action Plan and will reduce the amount of VMT throughout the community with additional office and retail commercial uses. VMT is the primary contributor to GHG emissions within the City. Additionally, the proposed project is under the operational thresholds established

by the SLOAPCD for GHG when the proposed project's mitigated factors are included. The proposed project includes a mix of uses, parking reduction, pedestrian linkage enhancements, etc. The following are mitigation measures utilized to ensure the proposed is under the brightline threshold established by SLOAPCD. Implementation of this measure will render the potential impact to less than significant levels.

GHG – 1. The proposed project shall include a minimum of four (4) electric vehicle (EV) charging stations within the proposed parking lot. Vehicle charging stations shall be counted as parking spaces for the purposes of parking calculations.

5. BIOLOGICAL RESOURCES – Will the project:

	Potentially Significant	Impact Requires Mitigation	Insignificant Impact	Not Applicable
a) Have a substantial adverse effect, either directly or through habitat modifications, on any species identified as a candidate, sensitive, or special status species in local or regional plans, policies, or regulations, or by the California Department of Fish and Wildlife (CDFW) or U.S. Fish and Wildlife Service (USFWS)?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Have a substantial adverse effect on any riparian habitat or other sensitive natural community identified in local or regional plans, policies, regulations or CDFW and USFWS?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c) Have a substantial adverse effect on federally protected wetlands as defined by Section 404 of the Clean Water Act through direct removal, filling, hydrological interruption, or other means?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d) Interfere substantially with the movement of any native resident or migratory fish or wildlife species or with established native resident or migratory wildlife corridors, or impede the use of native wildlife nursery sites?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
e) Conflict with policies or ordinances protecting biological resources, such as the tree native tree ordinance?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
f) Conflict with the provisions of an adopted Habitat Conservation Plan, Natural Community Conservation Plan, or other approved local, regional, or state habitat conservation plan?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

EXISTING SETTING: The project site consists of previously developed parcels that are presently underdeveloped, either vacant or in use as non-residential uses in the downtown. All areas of the site have been previously disturbed and are not home to any known native or threatened species. The site has been previously graded and has an average slope below 5%. The site is adjacent to El Camino Real, Highway 101, and is in the Downtown. Because of the highly urbanized surroundings, as well as the previously disturbed site, there are no significant biological factors on the site (wetlands, creeks, etc.). Atascadero Creek is approximately 300-foot south of the project site, however fully developed and active downtown commercial sites lie between the project site and the creek. Based on a site visit and the final proposed site plan, there are no native trees on site to be removed.

PROPOSED PROJECT: The project is a redevelopment of existing and previously developed sites, and does not have any significant biological factors that will be affected by the project.

MITIGATION / CONCLUSION: No biological resources are on-site, nor directly adjacent to the project site, nor are any trees proposed to be removed. The site has been previously developed and is located between an arterial road and Highway 101, therefore no biological impacts are anticipated.

6. CULTURAL RESOURCES – Will the project:

	Potentially Significant	Impact Requires Mitigation	Insignificant Impact	Not Applicable
a) Cause a substantial adverse change in the significance of a historical resource?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
b) Cause a substantial adverse change in the significance of an archaeological resource?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c) Directly or indirectly destroy a unique paleontological resource or site or unique geologic feature?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d) Disturb any human remains, including those interred outside of formal cemeteries?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

EXISTING SETTING: The project site contains two (2) structures built prior to 1966. Both buildings contain commercial-retail businesses. The structures are not on a national, state, or local historic registry. Additional structures on the property include a vacant building (former jack-in-the-box) and a telecommunication facilities tower.

There are no known human remains or cultural resources on the site; however, in accordance with AB 52, tribal consultations with local tribes were conducted to assess potential conflict with ancestral tribal sites. Responses received acknowledged this.

PROPOSED PROJECT: The proposed project will remove all structures on site, with the exception of the telecommunications facility. The applicant submitted a cultural resource assessment of the site that specifically focused on the building located at 6490 El Camino Real (Malibu Brew) and 6452 El Camino Real (Beauty Supply Store). The project site had two significant developments prior to existing conditions: Golden Way Auto Camp, and the Atascadero Hotel. This area was commonly referred to the Golden Way Block. The majority of the Golden Way Camp has been demolished or destroyed over time. The last remaining building is 6452 El Camino Rea, which was once used as a hotel. The Golden Way Auto Service center has morphed into a gasoline station since its construction in 1948, to a food service establishment today.

State guidelines for determining historical significance (36 CFR Part 60 and Calif. Pub. Res. Code, 5024.1, Title 14 CCR, Sect. 4852) indicate that a structure must be at least 50 years old and meet one of the following criteria in order to be considered a historic resource:

- A. It is associated with events that have made a significant contribution to the broad patterns of our history;
- B. It is associated with the lives of persons important in our past;
- C. It embodies the distinctive characteristics of a type, period, region, or method of construction, or represents the work of a master, or that possess high artistic values; or
- D. It has yielded or may be likely to yield information important in history.

The existing buildings are not listed as a National Historic Landmark, or on the National Register of Historic Places. They are not been listed in the California Register of Historical Resources as a Registered Historic Landmark or as a Point of Historical interest. They are not included in any listing of historic resources within the City of Atascadero. The existing buildings do not appear to meet any of the criteria for eligibility on a national or state level. The historic analysis prepared for the property did not find the existing site or buildings to meet criteria for historic listing. The historical analysis is included as an attachment to this document.

. Mitigation measures have been included to ensure that the existing buildings are appropriately documented prior to demolition.

Because the site has been previously disturbed, partially developed, and is not located near any known or mapped areas of potential cultural significance, a Phase 1 survey is not warranted. The site has been fully graded due to previous buildout of the location. However, to ensure human remains are not located on-site, a mitigation measure will need to be added.

MITIGATION / CONCLUSION: Implementation of the following mitigation measures will lower the threshold of impacts to less than significant.

CR – 1: Recordation and documentation shall be produced according to the California Register of Historic Resources prior to issuance of demolition permits for both 6452 and 6490 El Camino Real. Documentation shall include the following:

- ;
- Large scale color photographs of exterior and interior with scale and caption;
- Large scale site plan and floor plan of existing building(s) with legend and callouts of location photographs taken;
- Copies of selected historic photographs;
- Written description of construction and use of the structures;
- Digital copies of all materials submitted in PDF format.

CR – 2: In the event that human remains are discovered on the property, all work on the project shall stop and the Atascadero Police Department and the County Coroner shall be contacted immediately. The Atascadero Community Development Director shall be notified. If the human remains are identified as being Native American, the California Native American Heritage Commission (NAHC) shall be contacted at (916) 653-4082 within 24 hours. A representative from both the Chumash Tribe and the Salinan Tribe shall be notified during the excavation of any remains.

7. GEOLOGY AND SOILS – Will the project:

	Potentially Significant	Impact Requires Mitigation	Insignificant Impact	Not Applicable
a) Result in the exposure to or production of unstable earth conditions including the following: <ul style="list-style-type: none"> • Landslides; • Earthquakes; • Liquefaction; • Land subsidence or other similar hazards? 	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
b) Be within a California Geological Survey “Alquist-Priolo” Earthquake Fault Zone, or other known fault zone? (consultant Division of Mines and Geology Special Publication #42)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c) Result in soil erosion, topographic changes, loss of topsoil or unstable soil conditions from proposed improvements such as grading, vegetation removal, excavation or use of fill soil?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
d) Include any structures located on known expansive soils?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
e) Be inconsistent with the goals and policies of the City’s Safety element relating to geologic and seismic hazards?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
f) Have soils incapable of adequately supporting the use of septic tanks or alternative waste water disposal systems where sewers are not available for the disposal of waste water?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

EXISTING SETTING: Based on the City’s Geographical Information Systems (GIS), the project site is in a location with a high risk of liquefaction and low risk of landslide or subsidence. The site is located relatively close to a known fault line but is not located within a California Geological Survey “Alquist-Priolo” Earthquake Fault Zone. A GIS expansion determination indicates that the bearing soils lie in the “Moderate” expansion potential ranges.

PROPOSED PROJECT: Although there are no known faults within the project area, there are faults located near the City that have been known to create seismic events. Due to high liquefaction risk on site, mitigation to reduce liquefaction risk in the case of a seismic event will be required. The City adopts the California Building Code (CBC) as its building code and updates this code during each required adoption cycle. This code is continually updated with requirements to make buildings safer during a seismic event. Additionally, the CBC requires new development over 500-square feet to conduct soil reports to determine potential impacts of development, and to establish mitigation options to reduce that potential impact. Incorporation of the 2016 CBC requirements upon building permit submittal will reduce the exposure of people and structures to strong ground shaking to a less than significant level.

The proposed project will remove existing topsoil, pavement, and sidewalk areas to construct new foundations and for parking lots and driveways. A total of 74,232-sf of net impervious surfaces are proposed upon site completion. Consistent with both the Municipal Code and the City's adopted Stormwater Management Plan, sedimentation and erosion control shall include, but are not limited to: slope surface stabilization through temporary mulching or seeding, or natural or paved interceptors and diversions installed at the top of cut or fill slopes.

Erosion or sedimentation control devices can be used in order to prevent polluting sedimentation discharges. Control devices may include, but are not limited to: energy absorbing structures or devices to reduce the velocity of runoff water, sediment debris basin and traps, dispersal of water runoff over undisturbed areas, and implementing multiple discharge points to reduce volume of runoff over localized areas. A requirement of the Municipal Code and the City's adopted Stormwater Management Plan requires inclusion of an erosion and sediment control plan, to be reviewed and approved by the City Engineer or their designee. Implementation of existing City codes and adopted policies renders this potential impact to less than significant.

MITIGATION / CONCLUSION: Implementation of the City's Municipal Code, the CBC and the Stormwater Management Plan renders potential impacts to less than significant levels.

8. HAZARDS AND HAZARDOUS MATERIALS – Will the project:

	Potentially Significant	Impact Requires Mitigation	Insignificant Impact	Not Applicable
a) Create a significant hazard to the public or the environment through the routine transport, use, or disposal of hazardous materials?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Create a hazard to the public or the environment through reasonably foreseeable upset and accident conditions involving the release of hazardous materials into the environment?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c) Emit hazardous emissions or handle hazardous or acutely hazardous materials, substances, or waste within one-quarter mile of an existing or proposed school?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d) Be located on a site which is included on a list of hazardous materials sites compiled pursuant to Government Code Section 65962.5 and, as a result, would it create a significant hazard to the public or the environment?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
e) For a project located within an airport land use plan or, where such a plan has not been adopted, within two miles of a public airport or public use airport, would the project result in a safety hazard for people residing or working in the project area?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
f) For a project within the vicinity of a private airstrip, would the project result in a safety hazard for people residing or working in the project area?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
g) Impair implementation of or physically interfere with an adopted emergency response plan or emergency evacuation plan?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
h) Expose people or structures to a significant risk of loss, injury or death involving wildland fires, including where wildlands are adjacent to urbanized areas or where residences are intermixed with wildlands?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

EXISTING SETTING: The project site does not have any documented hazardous materials on or around the site. The development is also in a medium- risk fire hazard zone, therefore not posing a significant threat of wildfire damage to people and structures.

PROPOSED PROJECT: The proposed project does not generate or involve the use of significant amounts of hazardous materials. There are no known hazardous materials on the site or nearby, therefore, there is no impact. The project will not impair implementation of an adopted emergency response plan within the City.

The proposed project is within the urban core and not located near wildlands. Geographical Information Systems (GIS) shows the project site to be in a medium fire hazard zone, which may potentially pose risks to residents and structures that are constructed on-site. The City of Atascadero adopts the California Building Code; in addition to the 2015 Wildlife Urban Interface Code that specifically regulates construction methodology in high fire risk areas. During building permit review, the fire department will inspect all structures for consistency with this code. Because the code and its implementation reduces fire risk, potential impacts are considered less than significant.

MITIGATION / CONCLUSION: Implementation of the City’s adopted building code renders potential impacts less than significant.

9. WATER QUALITY / HYDROLOGY – Will the project:

	Potentially Significant	Impact Requires Mitigation	Insignificant Impact	Not Applicable
a) Violate any water quality standards or waste discharge requirements?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
b) Substantially deplete groundwater supplies or interfere substantially with groundwater recharge such that there would be a net deficit in aquifer volume or a lowering of the local groundwater table level (e.g., the production rate of preexisting nearby wells would drop to a level which would not support existing land uses or planned uses for which permits have been granted)?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c) Substantially alter the existing drainage pattern of the site or area, including through the alteration of the course of a stream or river, in a manner which would result in substantial erosion or siltation on- or off-site?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>

	Potentially Significant	Impact Requires Mitigation	Insignificant Impact	Not Applicable
d) Substantially alter the existing drainage pattern of the site or area, including through the alteration of the course of a stream or river, or substantially increase the rate or amount of surface runoff in a manner which would result in flooding on- or off-site?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
e) Create or contribute runoff water which would exceed the capacity of existing or planned stormwater drainage systems or provide substantial additional sources of polluted runoff?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
f) Otherwise substantially degrade water quality?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
g) Place housing within a 100-year flood hazard area as mapped on a federal Flood Hazard Boundary or Flood Insurance Rate Map or other flood hazard delineation map?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
h) Place within a 100-year flood hazard area structures which would impede or redirect flood flows?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
i) Expose people or structures to a significant risk of loss, injury or death involving flooding, including flooding as a result of the failure of a levee or dam?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
j) Inundation by seiche, tsunami, or mudflow?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

EXISTING SETTING: There are four (4) existing structures on-site, one (1) vacant, two (2) commercial businesses and a wireless telecommunications facility. The site has been previously developed. The site does not lie within a 100-year flood plain; however the City's GIS data shows the site to be within the 500- year floodplain, meaning there is a 0.2% chance of flooding, standards established by the Federal Emergency Management Agency (FEMA). Atascadero Creek runs approximately 300- feet to the south of the site, however several commercial establishments lie on the lots between the site boundary and the creek, obstructing impacts the proposed development could potentially have on the Creek. The Stormwater Control Plan identifies no existing natural drainage features and no areas of high permeability.

The site is a redevelopment site with previous development including the Atascadero Hotel, Golden Way auto camp, and other various uses. These uses combined have paved the majority

of the site. In 2009, a fire destroyed a good portion of the previous development and buildings that were not destroyed by the fire have since been demolished.

PROPOSED PROJECT: The proposed project consists of paving the majority of the site that currently exists as a partial dirt lot due to the burning down of previous commercial structures. The existing drainage pattern of the site has been previously altered to accommodate previous development such as the Atascadero Hotel, and the Golden Way Block. Upon completion, the project will have 74,232 sf of net impervious surface. The project utilizes bio-swales, permeable pavers, and landscaping to help slow and capture water runoff and drainage on-site, channeling all possible impervious area runoff to vegetated areas before runoff enters any stormwater conveyance systems. Site vegetation and pervious pavers was maximized to be approximately 15,125 sf.

Post-stormwater construction standards require drainage patterns to mimic up to 50% of pre-development status for redevelopment projects, as required by the Regional Water Quality Control Board. The project proposes a series of underground retention and infiltration systems that meets this standard, as well as bio-swales to catch storm water runoff. The site was identified as not having any natural drainage features, and site drainage improvements will improve site permeability and capture of storm water runoff, and therefore have no impact.

The City, consistent with the Municipal Code and its Stormwater Management Plan, requires a Stormwater Pollution Prevention Plan (SWPPP)/ Erosions Control Plan to be submitted and approved by the City Engineer, or their designee prior to the issuance of the building permit. The plan must include storm water measures for the operation and maintenance of the project for their review and identify Best Management Practices (BMPs) appropriate to the uses conducted on site that effectively prohibit the entry of pollutants into stormwater runoff.

MITIGATION / CONCLUSION: With implementation of the City’s Municipal Code, as well as the policies outlines in the adopted Stormwater management Plan, the potential impacts are rendered to a less than significant level.

10. LAND USE & PLANNING – Will the project:

	Potentially Significant	Impact Requires Mitigation	Insignificant Impact	Not Applicable
a) Physically divide an established community?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Conflict with any applicable land use plan, policy, or regulation of an agency with jurisdiction over the project adopted for the purpose of avoiding or mitigating an environmental effect?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c) Conflict with any applicable habitat conservation plan or natural community conservation plan?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

EXISTING SETTING: The site’s general plan designation is Downtown (D). The site’s zoning district is Downtown Commercial (DC) with an allowed density of 20 maximum residential units

per acre. Surrounding properties are also zoned DC as well as Public Right-of-Way for both Highway 101 to the west of the site and El Camino Real to the east.

PROPOSED PROJECT: The Atascadero Zoning Ordinance indicated that residential units are allowed in the Downtown Commercial zone at the maximum of 20 units per acre. The applicant is requesting a density bonus in exchange for “high quality architectural design.” This bonus allows for additional project density based on the overall design of the project. The Density Bonus may be approved by the Planning Commission, if the commission makes the required findings consistent with the City’s adopted General Plan. The applicant is proposing a total of 40 residential units located on the 2nd and 3rd floors of the development. The overall project density is 22 dwelling units per acre. The proposed density bonus amounts to less than the maximum density bonus that can be awarded, which is 15%.

The proposed project will be developed in accordance with the City of Atascadero General Plan and will infill an underutilized lot in the downtown core, and will not physically divide an established community. The proposed project is in compliance with the General Plan Policy 2.1 ensuring that new development is compatible with existing and surrounding development.

MITIGATION / CONCLUSION: The proposed project will not have any adverse effects on land use and planning.

11. MINERAL RESOURCES – Will the project:

	Potentially Significant	Impact Requires Mitigation	Insignificant Impact	Not Applicable
a) Result in the loss of availability of a known mineral resource that would be of value to the region and the residents of the state?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Result in the loss of availability of a locally important mineral resource recovery site delineated on a local general plan, specific plan or other land use plan?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

EXISTING SETTING: There are no known mineral resources on the site.

MITIGATION / CONCLUSION: Mineral resources are not located on-site, therefore no impact.

12. NOISE – Will the project result in:

	Potentially Significant	Impact Requires Mitigation	Insignificant Impact	Not Applicable
a) Exposure of persons to or generation of noise levels in excess of standards established in the local general plan or noise ordinance, or applicable standards of other agencies?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
b) Exposure of persons to or generation of excessive groundborne vibration or groundborne noise levels?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

	Potentially Significant	Impact Requires Mitigation	Insignificant Impact	Not Applicable
c) A substantial permanent increase in ambient noise levels in the project vicinity above levels existing without the project?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
d) A substantial temporary or periodic increase in ambient noise levels in the project vicinity above levels existing without the project?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
e) For a project located within an airport land use plan or, where such a plan has not been adopted, within two miles of a public airport or public use airport, would the project expose people residing or working in the project area to excessive noise levels?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
f) For a project within the vicinity of a private airstrip, would the project expose people residing or working in the project area to excessive noise levels?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

EXISTING SETTING: The City’s General plan identified the site to be within noise contours generated by both El Camino Real and Highway 101. The City’s Noise Contour Map has identified the site as sitting in the 65 decibel contour due to its proximity to El Camino Real and the Highway 101. The airport is not located within an airport land use plan or private airstrip.

PROPOSED PROJECT: The proposed project does not anticipate noise generation levels exceeding existing City standards. Construction of the project is expected to generate a significant amount of noise on and around the site.

Upon completion, the proposed project will not result in a significant increase in generation of noise levels or ground borne vibration. However, construction is expected to involve some heavy machinery and use of tools that will temporarily increase the ambient noise levels in the project vicinity. The AMC outlines noise generation regulations such as established hours of operations to keep off-site noise pollution at a minimum during the day. The AMC limits construction activity from 7am to 9pm. Implementation of the City’s noise ordinance ensures construction activities do not take place early in the morning and into the late evening hours. With the City’s ordinance, the potential impact is considered less than significant.

The proposed project is located within noise contours of 65 decibels, identified by the City’s General Plan. Because of this, a noise study shall be conducted to ensure noise impacts are reduced to thresholds allowable (45 decibels for residential projects) by the implementation measures and policies set forth by the City’s General Plan.

To achieve the required performance of less than 45 dBA, consistent with the City’s General Plan, interior noise level along the critical sides of dwelling units nearest the noise source (only for second and third story construction on facades or surfaces directly facing the noise source), the noise study shall recommend construction specifications to be incorporated into the building

plans. These measures are considered mitigation to meet the City’s general plan goals and policies.

MITIGATION / CONCLUSION: Implementation of the following noise mitigation measures will reduce potential noise impacts to less than significant thresholds.

NOI – 1: Vents and roof penetrations. Soffit vents, eave vents, dormer vents and other wall and roof penetrations shall be on the walls and roofs facing away from the noise source wherever possible.

NOI – 2: The walls of habitable spaces and office space on second and third stories nearest the noise source shall have wall construction with an S.T.C. (Sound Transmission Class) rating of 30 or greater. For instance, stucco exterior or equivalent on 2” x 4” stud walls with minimum R-13 batt insulation and two layers of ½” gypsum board on the interior will provide an S.T.C. rating of 30 or greater along these walls.

NOI – 3: Common acoustic leaks, such as electrical outlets, pipes, vents, ducts, flues, and other breaks in the integrity of the wall, ceiling or roof construction on the side of the dwellings nearest transportation noise sources shall receive special attention during construction. All construction openings and joints on the walls on the noise facing side of the site shall be insulated, sealed and caulked with a resilient, non-hardening, acoustical caulking material. All such openings and joints shall be airtight to maintain sound isolation.

NOI – 4: To meet the interior LDN 45 dBA requirements, windows for habitable spaces on the second and third floors of affected units facing the noise source shall be of double-glazed construction with one light of laminated glass, and installed in accordance with the recommendations of the manufacturer. The windows shall have full gaskets, with an S.T.C. rating of 30 or better, as determined in testing by an accredited acoustical laboratory.

13. POPULATION & HOUSING – Will the project:

	Potentially Significant	Impact Requires Mitigation	Insignificant Impact	Not Applicable
a) Induce substantial population growth in an area, either directly (for example, by proposing new homes and businesses) or indirectly (for example, through extension of roads or other infrastructure)?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
b) Displace substantial numbers of existing housing, necessitating the construction of replacement housing elsewhere?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c) Displace substantial numbers of people, necessitating the construction of replacement housing elsewhere?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

EXISTING SETTING: There are no existing residential units on the project site, and therefore no displacing of residents will be required upon development.

PROPOSED PROJECT: The population increase resulting from the proposed project is in accordance with the City of Atascadero General Plan. The development will not displace any current residences.

The project proposes 40 residential units. New development is expected to increase housing opportunities and population in the area in accordance with the City's General Plan. The project will also help the City meet Regional Housing Needs goals. Based on the 2010 US Census, the City's average household size is 2.51 persons per unit. The total projected population of the project at build out is approximately 100 persons. This represents less than 1% of the City's population of 30,900 based on the State of California Department of Finance population housing estimates for 2017. The proposed project also meets the General Plan's goal of providing more housing in the downtown core to facilitate more activity in the downtown.

MITIGATION / CONCLUSION: The proposed project will not have any significant negative impact on population and housing.

14. PUBLIC SERVICE:

Will the proposed project have an effect upon, or result in the need for new or altered public services in any of the following areas:

	Potentially Significant	Impact Requires Mitigation	Insignificant Impact	Not Applicable
a) Emergency Services (Atascadero Fire)?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>
b) Police Services (Atascadero Police)?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
c) Public Schools?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
d) Parks?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
e) Other public facilities?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

EXISTING SETTING: Currently there are two operating commercial businesses and a vacant building on-site. All existing structures are set to be demolished as part of the proposed project.

PROPOSED PROJECT: The proposed project is within the Atascadero Urban Services Line and will not result in the need for new or altered public services outside of the population increase potentially to occur upon project completion due to new residential dwelling units.

Development Impact Fees will be required of any new project for which a building permit is issued. The concept of the impact fee program is to fund and sustain improvements which are needed as a result of new development as stated in the General Plan and other policy documents within the fee program. These fees include park fees and fire services. The collection of these fees helps to offset additional new residences serviced. Collection of impact fees, the impact is considered less than significant.

At buildout, the City's population will overburden the existing school system unless additional classroom space is added. The Atascadero Unified School District charges impact fees to fund additional schools as needed. State law restricts mitigation of school impact fees to fund additional schools as needed. State law restricts mitigation of school impacts to the levying of

these fees and other measures adopted by the school district. Provision of adequate facilities for the population is the responsibility of the school district. Payment of these fees is required to be completed prior to the issuance of building permits on residential units, per City policy. With the collections of these fees, the impact is considered less than significant.

The proposed project will increase demand on existing City parks in the Downtown. Additionally, plazas and small outdoor gathering spaces will add passive recreational space for site visitors and residences. The project does not propose subdivision of any land, and therefore does not require any parkland dedication as part of its mitigation measures. However, the applicant is proposing to construct a new public plaza directly adjacent to the project site in the Right-of-Way of Atascadero Avenue. This would require partial closure of vehicular traffic on Atascadero Avenue. The applicant may be eligible for parkland credit for creation of new public spaces dedicated to the public. Any fee credit would be given at the time of building permit issuance.

MITIGATION / CONCLUSION: With collection of development impact fees for fire, police, and parks, the impacts are considered less than significant.

15. RECREATION:

	Potentially Significant	Impact Requires Mitigation	Insignificant Impact	Not Applicable
a) Would the project increase the use of existing neighborhood and regional parks or other recreational facilities such that substantial physical deterioration of the facility would occur or be accelerated?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
b) Does the project include recreational facilities or require the construction or expansion of recreational facilities which might have an adverse physical effect on the environment?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

EXISTING SETTING: There are no existing recreational facilities currently on-site. Additionally, there are currently no residents on-site. An existing park facility is across the street from the proposed project, Sunken Gardens, which is utilized by the residents for various activities including sporting activities for children, festivals, and other large events.

PROPOSED PROJECT: The proposed project will not increase the use of existing neighborhood and regional parks, as the proposed population increase is less than 1% of the overall existing population of the City, as estimated by the California Department of Finance. Therefore, the proposed project will have a less than significant impact on existing neighborhood and regional recreational facilities.

The proposed project does in a passive recreational space, a public plaza. The plaza is proposed to be constructed on City right-of-way (Atascadero Avenue), which will cause a partial closure of Atascadero Avenue. This closure will not have an adverse physical effect on the environment, as the existing right-of-way, including on-street parking, will be transformed into a public plaza, aiding in the walkability from multi-family units and Atascadero High School to the west, to the Downtown corridor destination point. The proposed plaza is considered a less than significant impact.

MITIGATION / CONCLUSION: The proposed project will not have any significant impacts on recreational uses.

16. TRANSPORTATION / TRAFFIC – Will the project:

	Potentially Significant	Impact Requires Mitigation	Insignificant Impact	Not Applicable
a) Conflict with an applicable plan, ordinance or policy establishing measures of effectiveness for the performance of the circulation system, taking into account all modes of transportation including mass transit and non-motorized travel and relevant components of the circulation system, including but not limited to intersections, streets, highways and freeways, pedestrian and bicycle paths, and mass transit?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
b) Conflict with an applicable congestion management program, including, but not limited to level of service standards and travel demand measures, or other standards established by the county congestion management agency for designated roads or highways?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c) Result in a change in air traffic patterns, including either an increase in traffic levels or a change in location that results in substantial safety risks?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d) Substantially increase hazards due to a design feature (e.g., sharp curves or dangerous intersections) or incompatible uses (e.g., farm equipment)?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
e) Result in inadequate emergency access?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
f) Conflict with adopted policies, plans, or programs regarding public transit, bicycle, or pedestrian facilities, or otherwise decrease the performance or safety of such facilities?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

EXISTING SETTING: There are two existing businesses on-site, one (1) vacant building, and a telecommunications facility. The telecommunications facility is currently unmanned, therefore does not generate traffic. Based on an ITE trip modeling, the existing site generates 458 daily trips, with 58 AM peak hour trips, and 26 PM peak hour trips. The site is directly adjacent to two (2) public right-of-ways: El Camino Real and Atascadero Avenue, which is signed and known as “Atascadero Mall”. El Camino Real is the main frontage of the existing site. El Camino Real is the City’s main arterial, and has a 100-foot right-of-way and contains four (4) lanes of travel,

sidewalks, parallel on-street parking, and center turn lane. Its current level of Service (LOS) is B, as determined by the City Engineer. Project frontage along El Camino Real includes two intersecting street segments: Entrada Avenue on the northern edge of the project site, and West Mall. Nearby intersections include Highway 41/Morro Road, East Mall, and Traffic Way. The West Mall / El Camino Real intersection experiences a period of peak traffic during school drop off times as Atascadero Middle School and the Atascadero Fine Arts Academy are located east of the existing site. Additional peak periods include the afternoon between 2:50 and 3:30, as school lets out. This also coincides with heavy pedestrian use of crossings between El Camino Real, East Mall, West Mall, and Traffic Way.

Atascadero Avenue is directly adjacent to the site and has a right-of-way of 100 feet, however a portion is owned “in fee” by the project applicant but access easements still exist. Atascadero Avenue includes 11-foot diagonal parking on both sides, 30-foot center median, 10-foot sidewalks and 14-foot travel lanes. Atascadero Avenue terminates at Highway 101, however there is a pedestrian tunnel that goes under Highway 101 and connects with Atascadero Avenue west of Highway 101. This pedestrian tunnel is a main access route for high school students and residents connecting the neighborhoods on the westside of Atascadero to Downtown Atascadero. Since Atascadero Avenue (Atascadero Mall) terminates at Highway 101, the street acts as a “driveway” for existing business. It operates at a LOS A.

The existing site is serviced by transit. There is a southbound Regional Transit Authority (RTA) stop for route 9, that serves San Miguel, Paso Robles, Atascadero, and San Luis Obispo is located at the intersection of Entrada and El Camino Real. A northbound stop is located 550 feet to the south of the site at the intersection of Traffic Way and El Camino Real. The City transit station is located 2,000 feet from project site to the east.

Access from Highway 101 is available through two freeway entrances: Traffic Way and Highway 41 / Morro.

PROPOSED PROJECT: The proposed project includes a mix of residential and non-residential uses. City Staff completed an ITE trip generation model for the proposed project. The following ITE codes were utilized:

- 40 Low Rise Apartment Units (ITE 221)
- 0.30 acres City Park (ITE 411);
- 9,700 sf of General Office (ITE 710);
- 14,300 sf of Specialty Retail Center (ITE 826);
- 3,850 sf of Quality Restaurant (ITE 931).

ITE trip generation model rounds up square footage / acreage to nearest tenth. Based on this information, the proposed project would add 1,340 daily trips, or an increase of 65% of the total daily trips from existing development. AM Peak Hour trips are 135 trips generated and PM peak hour trips are 105 trips total. The ITE trips model is based on a suburban land use model and assumes singular uses for each separate and does not take into account a mix of uses within a one building. Caltrans published studies in regards to urban infill and traffic generate. In 2009, Caltrans concluded that in many cases, urban infill development such as La Plaza trip generate rates are generally lower when compared to the ITE model (Caltrans: Trip Generation Rates for Urban Infill Land Uses in California).

Based on the amount of trips the proposed project will generate, the City Engineer has determined that the proposed project will not contribute to the derogation of LOS on El Camino

Real to a LOS C or below, the threshold established by the City's General Plan. This includes both the PM and AM peak hour trips. This impact is considered less than significant.

The proposed project also includes a partial closure of Atascadero Avenue to accommodate a pedestrian plaza that connects the pedestrian tunnel to the Downtown. This closure would reduce Atascadero Avenue to 20-foot wide, placing vehicles on the southern end of Atascadero Avenue eliminating the sidewalk adjacent to the existing gas station and service station. These proposed improvements would be located on City right-of-way, which is owned by the City. The remainder of the right-of-way (80-feet) would be turned into a pedestrian plaza. Atascadero Avenue is, although a dedicated right-of-way, is currently used more as a driveway than a roadway, as vehicles use it to "cut-through" for access to the existing gas station, and cars are parked that are associated with the automotive repair shop, also adjacent to Atascadero Avenue. The proposed reduction in width of Atascadero Avenue will not cut-off access to any of the existing businesses, nor will this degrade the LOS of Atascadero Avenue. This impact is considered less than significant.

With elimination of the sidewalk on the southern portion of Atascadero Avenue, the existing pedestrian crossing at East Mall and El Camino Real will be affected creating a potential conflict with an existing pedestrian facility. The East Mall / El Camino Real pedestrian crossing includes warning lights. These warning lights were recently removed due to issues with the lights malfunctioning. Additionally, this intersection has experienced six (6) pedestrian or bicycle collisions with vehicles from 2011-2016 according to the California Highway Patrol. The applicant proposes to relocate the pedestrian walkway to the signalized intersection of West Mall and El Camino Real. City Staff is in favor of this proposed relocation as the crossing would eliminate a "mid-block" crossing, in favor of a crossing that is controlled by a traffic signal. In order to reduce the loss of a pedestrian crossing to a less than significant impact, a mitigation measure is included to ensure the crossing is replaced. Inclusion of this new crossing would render this impact to less than significant.

The proposed project includes elimination of an existing driveway adjacent to the project site to reduce driveway conflicts and place a new driveway at the location of the existing bus stop. The placement of this driveway will affect the existing transit stop. A mitigation measure has been included to ensure that the City and the applicant work with the Regional Transit Authority to site a new transit stop, with bus shelter at an appropriate location, either on the project site, or off the project site.

The City's Emergency Services Department (Fire) reviewed the proposed project, including reduction of right-of-way of Atascadero Avenue, emergency access for the proposed project, and elimination of existing driveways. The fire department has approved the site design and relocation of pavement on Atascadero Avenue, and has determined that the proposed project has sufficient emergency access. The impact is considered less than significant.

The proposed project will be contributing additional traffic on roadways and freeway intersections, particularly traffic way and Morro Road / Highway 41. The City has established a Traffic Impact Fee (TIF) that goes towards improvements identified in the City's Capital Improvement Program (CIP). This fee is to be paid on all new residential development on a per dwelling unit basis, and per square foot (sf) on non-residential development. Based on the proposed project statistics, the applicant would owe the following, less any fee credits available based on the adopted CIP. Credits are capped at 66.6% of the total cost of improvements.

- \$467,760 TIF – 40 residential units;
- \$170,208 – 18,000 sf retail;

- \$91, 723.20 – 9,700 sf of Office.

TIF is based on the City’s adopted Development Impact Fee schedules as of October 18, 2017. The applicant or developer will be required to pay TIF based on the date a building permit is submitted, consistent with California Subdivision Map Act since the applicant has submitted a tentative parcel map. Payment of these fees will render increased traffic to a less than significant threshold.

MITIGATION / CONCLUSION: The following are mitigation measure necessary in response to transportation / traffic impacts. Implementation of these mitigation measures will reduce the potential project impacts to a less than significant threshold.

TP – 1: The applicant or developer must relocate the existing pedestrian crossing at the midblock crossing at the intersection of East Mall and El Camino Real to a location to the north subject to the approval of the Public Works Director. The preferred location would be at a signalized intersection. The applicant shall re-stripe and provide conduit for a new pedestrian crossing signal adjacent to the project site. The proposed improvement may be eligible for TIF credits based on the adopted Capital Improvement Plans at the time of building permit issuance.

TP – 2: The applicant shall work with City Staff and the Regional Transit Authority to relocate the adjacent southbound RTA stop that currently exists near the intersection of Entrada and El Camino Real. This includes relocation or re-construction of a bus shelter, as deemed appropriate by the City Engineer.

17. UTILITIES AND SERVICE SYSTEMS – Will the project:

	Potentially Significant	Impact Requires Mitigation	Insignificant Impact	Not Applicable
a) Exceed wastewater treatment requirements of the applicable Regional Water Quality Control Board?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
b) Require or result in the construction of new water or wastewater treatment facilities or expansion of existing facilities, the construction of which could cause significant environmental effects?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
c) Require or result in the construction of new storm water drainage facilities or expansion of existing facilities, the construction of which could cause significant environmental effects?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
d) Have sufficient water supplies available to serve the project from existing entitlements and resources, or are new or expanded entitlements needed?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

	Potentially Significant	Impact Requires Mitigation	Insignificant Impact	Not Applicable
e) Result in a determination by the wastewater treatment provider which serves or may serve the project that it has adequate capacity to serve the project's projected demand in addition to the provider's existing commitments?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
f) Be served by a landfill with sufficient permitted capacity to accommodate the project's solid waste disposal needs?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
g) Comply with federal, state, and local statutes and regulations related to solid waste?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

EXISTING SETTING: There are two existing businesses on-site, a vacant building, and an unmanned wireless telecommunications facility. It is unknown how many fixture units are currently within the existing buildings. All buildings on-site have sewer service from the City and water services from Atascadero Mutual Water Company (AMWC). Existing drainage facilities on-site includes run-off from private drainage systems to the City's storm drain system, as the existing parcels were previously developed. The site is currently serviced by Atascadero Waste Alternatives, which transports solid waste to the Chicago Grade landfill.

PROPOSED PROJECT: The proposed project is required to connect all residential units, landscaping irrigation, and new non-residential spaces to water services. The Atascadero Mutual Water Company (AMWC) has indicated that it can provide water to the proposed project. All property within the City limits is entitled to water from the AMWC. The project is not expected to require a significant quantity of water for the proposed use. Water is pumped from several portions of the underground, the Atascadero Sub-Basin, using a series of shallow and deep wells. The water company anticipates that it will be able to meet the city's needs through build out and beyond. The following is the projected water use factors for the proposed project provided by the AMWC:

- 0.20 acre feet per year (AFY), per unit for the multi-family residential portion;
- 0.30 per 1,000 square feet of non-residential uses;
- 0.34 per 1,000 sf for restaurant uses,
- 2.20 per acre for landscaped areas.

Based on these calculations, the following is an estimated projected annual water demand for the entire project:

Table 17-1: Projected Annual Water Usage

Land Use	Units / Sf (1,000) / Acre	Factor	Acre Feet per Year (AFY)
Residential Apartments	40 units	0.20 AFY per unit	8.00
Non-Residential	23,321 sf	0.30 AFY per 1,000 sf	7.00
Restaurant Use	3,749 sf	0.34 AFY per 1,000 sf	1.27
Landscape Areas	0.726 acres	2.20 AFY per acre	0.33
Project Total			16.6 AFY

Source: Atascadero Mutual Water Company, Eagle Ranch Specific Plan Draft EIR

The total project water demand for the proposed project is 16.6 AFY, per year. The AWMC projects a supply of 8,700 AFY for the year 2020 with a demand in a single dry year at 6,788 afy for its entire system for both existing and future demand. Based on these statistics, the proposed project’s impact on water supply is considered less than significant, as this project, along with other projects including the proposed Eagle Ranch Specific Plan, Hartberg Planned Development, and other either under construction or included in the CEQA baseline will be met based on the supply and demand for water.

The proposed project will require additional sewer taps into the City’s sewer system. Based on the findings from the Final March 2016 “City of Atascadero Water Reclamation Facility Master Plan Update”, the current average daily flow is 1.38 millions of gallons per day (mgd), which is 99 percent of the original 1.40 mgd design capacity, leaving the facility with 0.02 mgd available. Based on this study, the City is currently conducting a re-rating study to determine the final remaining capacity that is left within the water reclamation facility. The following is the projected wastewater flows for the proposed project:

Table 17-2: Projected Wastewater Flow

Land Use	Number of Units / square foot	Residents Per Unit	Flow Factor (GPDU)	Average Daily Flow (GDP)	Peak Hour Flow (GDP) 3.8 PHF
Residential Apartments	40 units	2.62	70	7,336	27,877
Non-Residential	23,321 sf		0.1	2,332	8,862
Restaurant Use	3,749 sf		0.1	375	1,425
Project Total				10,043 gdp	38,164 gdp
Project Total Million Gallons per Day (mgd)				0.008 mgd	0.03 mgd

Source: Atascadero Collection Reclamation Facility Master Plan Update, March 2016

. Capacity at the plan fluctuates, depending on plan conditions, weather, etc. The General Plan envisioned mixed-use development within the Downtown and the General Plan Final EIR analyzed some residential development in this area, coupled with non-residential development.

In the event that capacity is maximized at the City’s treatment facility, there are a number of improvements that may be made at the existing plan to add capacity, including, but not limited to the following:

- Aeration;
- Partial dredging from the polishing pond; or
- Modification of the existing recirculation pumping stations.

Implementation of the listed mitigation measures would reduce the potential impacts to the wastewater treatment facility to a level of less than significant.

Solid waste will be collected by the City of Atascadero, through provide contracts, and processed to the Chicago Grade landfill. The current capacity of the Chicago Grade Landfill is 6.12 million cubic yards. The proposed project will produce both construction waste and operational waste. The following is the estimated Construction Waste to be generated by the proposed project:

Table 17-3: Estimated Construction Waste Generation

Land Use	Waste Generation Rate	Units / Square Feet	Annual Waste Generation	
			Tons	Cubic Yards
Residential	4.38 pounds per unit	40 units	0.09	0.12
Non-Residential	3.89 pounds per sf	27,070 sf	13.54	18.95
Total			13.63	19.07

Source: United States EPA, Eagle Ranch Specific Plan Draft EIR

The construction waste generated by the project (62.23 cubic yards) would represent less than 0.01 percent of the remaining. Construction waste generation is considered less than significant.

Operational solid waste generation is calculated for both non-residential and residential uses by Cal Recycle using a statewide per person waste generation (4.7 pounds / capital/day).

Table 17-4: Estimated Operational Waste Generation

Land Use	Waste Generation Rate	Count	Annual Waste Generation	
			Tons	Cubic Yards
Residential	1,716 pounds per person	100 persons	85.80	0.10
Non-Residential	4.8 pounds per sf	27,070 sf	64.97	90.95
Total			13.63	91.05

Source: Cal Recycle, Eagle Ranch Specific Plan Draft EIR

The operational waste generated by the project (91.05 cubic yards) would represent less than 0.01 percent of the remaining landfill. To ensure that the proposed project complies with State waste diversion, a mitigation measure is included to install recycling receptacles throughout the project.

MITIGATION / CONCLUSION: The following mitigation measures must be implemented to ensure potential impacts are less than significant.

UT – 1. The proposed project must pay all applicable sewer connection fees at the time of building permit issuance. A fee credit may be applicable for all existing fixtures and connections listed in demolition permits for buildings removed.

UT – 2. The applicant shall be responsible for providing fair share impact fees for wastewater in effect at the time of building permit issuance.

UT – 3. The developer must include trash and recycle receptacles near public entries and a minimum of (3) trash and recycle receptacles within the proposed plaza to be dispersed throughout the length of the plaza. Receptacles design shall be approved by Staff and included on landscaping plans at the time of building permit submittal.

18. TRIBAL & CULTURAL RESOURCES – Will the project:

	Potentially Significant	Impact Requires Mitigation	Insignificant Impact	Not Applicable
a) Cause a substantial adverse change in the significance of a tribal cultural resource, defined in Public Resources Code section 21074 as either a site, feature, place, cultural landscape, sacred place, or object with cultural value to a California Native American tribe?:	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Impact a listed or eligible for listing in the California Register of Historical Resources, or in a local register of historical resources as define in Public Resources Code Section 5020.1(k)?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
c) Impact a resource determined by the lead agency, in its discretion and supported by substantial evidence, to be significant pursuant to criteria set forth in subdivision (c) of Public Resources Code Section 5024.1. In applying the criteria set forth in subdivision (c) of Public Resources Code Section 5024.1. the lead agency shall consider the significance of the resource to a California native American Tribe?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

EXISTING SETTING: There are two existing businesses on-site, a vacant building, and an unmanned wireless telecommunications facility. The site is not listed as a significant tribal cultural resource, nor is it located near one. The site is not listed, but buildings may be eligible for listing through local register of places significant to Atascadero’s history.

PROPOSED PROJECT: The existing buildings are not listed as a National Historic Landmark, or on the National Register of Historic Places. They are not been listed in the California Register of Historical Resources as a Registered Historic Landmark or as a Point of Historical interest. They are not included in any listing of historic resources within the City of Atascadero, which tends to focus on resources of the Atascadero Colony Period. The existing building does not appear to meet any of the criteria for eligibility on a national or state level. The existing buildings have the potential to be considered a local historic resource due to its association with the post-war auto-centric commercial development of Atascadero along El Camino Real.

Therefore, building at 6490 and 6452 El Camino Real may be demolished following recordation and documentation as mitigation. Recordation and documentation should be produced according to the California Register of Historic Resources. A mitigation measures has been included to mitigate this potentially significant impact listed as CR – 1. Implementation of these measures renders the impact to less than significant.

MITIGATION / CONCLUSION: Implementation of mitigation measures CR – 1 renders this impact less than significant.

19. MANDATORY FINDINGS OF SIGNIFICANCE:

	Potentially Significant	Impact Requires Mitigation	Insignificant Impact	Not Applicable
a) Does the project have the potential to degrade the quality of the environment, substantially reduce the habitat of a fish or wildlife species, cause a fish or wildlife population to drop below self-sustaining levels, threaten to eliminate a plant or animal community, reduce the number or restrict the range of a rare or endangered plant or animal or eliminate important examples of the major periods of California history or prehistory?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Does the project have impacts that are individually limited, but cumulatively considerable? ("Cumulatively considerable" means that the incremental effects of a project are considerable when viewed in connection with the effects of past projects, the effects of other current projects, and the effects of probable future projects)	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
c) Does the project have environmental effects which will cause substantial adverse effects on human beings, either directly or indirectly?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

EXISTING SETTING: There are two existing businesses on-site, a vacant building, and an unmanned wireless telecommunications facility. The site has been previously developed dating back to the early 1920's and had been graded and disturbed. There are no known wetlands, significant tree vegetation or know habitat for rare or endangered species. The site has no known pre-historic resources or examples of California history.

PROPOSED PROJECT: The proposed project is a mixed-use project that includes 40 residential units and 27,070 sf of non-residential uses, including offices and retail / personal services, etc. The project is consistent with the underlying zoning district, Downtown Commercial (DC), and meets the goals, policies, and implementation of both the General Plan, and the Downtown Revitalization Plan. The proposed project and the cumulative effects will not have an impact on existing and future projects, nor does the proposed project have any environmental effects which will cause substantial adverse effects on residents, either directly or indirectly.

MITIGATION / CONCLUSION: The proposed project will not have a significant cumulative impact.

For further information on California Environmental Quality Act (CEQA) or the City's environmental review process, please visit the City's website at www.atascadero.org under the Community Development Department or the California Environmental Resources Evaluation System at: <http://resources.ca.gov/ceqa/> for additional information on CEQA.

Exhibit A – Initial Study References & Outside Agency Contacts

The Community Development Department of the City of Atascadero has contacted various agencies for their comments on the proposed project. With respect to the proposed project, the following outside agencies have been contacted (marked with an ☒) with a notice of intent to adopt a proposed negative / mitigated negative declaration.

- | | |
|--|---|
| <input checked="" type="checkbox"/> Atascadero Mutual Water Company | <input checked="" type="checkbox"/> Native American Heritage Commission |
| <input checked="" type="checkbox"/> Atascadero Unified School District | <input checked="" type="checkbox"/> San Luis Obispo Council of Governments |
| <input checked="" type="checkbox"/> Atascadero Waste Alternatives | <input checked="" type="checkbox"/> San Luis Obispo Air Pollution Control District |
| <input checked="" type="checkbox"/> AB 52 – Salinan Tribe | <input type="checkbox"/> San Luis Obispo Integrated Waste Management Board |
| <input checked="" type="checkbox"/> AB 52 – Northern Chumash Tribe | <input checked="" type="checkbox"/> Regional Water Quality Control Board District 3 |
| <input checked="" type="checkbox"/> AB 52 – Xolon Salinan Tribe | <input checked="" type="checkbox"/> HEAL SLO – Healthy Communities Workgroup |
| <input type="checkbox"/> AB 52 – Other | <input checked="" type="checkbox"/> US Postal Service |
| <input type="checkbox"/> California Highway Patrol | <input checked="" type="checkbox"/> Pacific Gas & Electric (PG&E) |
| <input type="checkbox"/> California Department of Fish and Wildlife (Region 4) | <input checked="" type="checkbox"/> Southern California Gas Co. (SoCal Gas) |
| <input checked="" type="checkbox"/> California Department of Transportation (District 5) | <input checked="" type="checkbox"/> San Luis Obispo County Assessor |
| <input checked="" type="checkbox"/> Pacific Gas & Electric | <input type="checkbox"/> LAFCO |
| <input type="checkbox"/> San Luis Obispo County Planning & Building | <input type="checkbox"/> Office of Historic Preservation |
| <input type="checkbox"/> San Luis Obispo County Environmental Health Department | <input type="checkbox"/> Charter Communications |
| <input type="checkbox"/> Upper Salians – Las Tablas RCD | <input type="checkbox"/> CA Housing & Community Development |
| <input checked="" type="checkbox"/> Central Coast Information Center (CA. Historical Resources Information System) | <input type="checkbox"/> CA Department of Toxic Substances Control |
| <input type="checkbox"/> CA Department of Food & Agriculture | <input type="checkbox"/> US Army Corp of Engineers |
| <input type="checkbox"/> CA Department of Conservation | <input type="checkbox"/> Other: |
| <input type="checkbox"/> CA Air Resources Board | <input type="checkbox"/> Other: |
| <input type="checkbox"/> Address Management Service | <input type="checkbox"/> Other: |

The following checked (“☒”) reference materials have been used in the environmental review for the proposed project and are hereby incorporated by reference into the Initial Study. The following information is available at the Community Development Department and requested copies of information may be viewed by requesting an appointment with the project planner at (805) 461-5000.

- | | |
|---|---|
| <input checked="" type="checkbox"/> Project File / Application / Exhibits / Studies | <input checked="" type="checkbox"/> Adopted Atascadero Capital Facilities Fee Ordinance |
| <input checked="" type="checkbox"/> Atascadero General Plan 2025 / Final EIR | <input type="checkbox"/> Atascadero Inclusionary Housing Policy |
| <input checked="" type="checkbox"/> Atascadero Municipal Code | <input checked="" type="checkbox"/> SLO APCD Handbook |
| <input checked="" type="checkbox"/> Atascadero Appearance Review Manual | <input checked="" type="checkbox"/> Regional Transportation Plan |
| <input checked="" type="checkbox"/> Atascadero Urban Stormwater Management Plan | <input checked="" type="checkbox"/> Flood Hazard Maps |
| <input type="checkbox"/> Atascadero Hillside Grading Guidelines | <input checked="" type="checkbox"/> CDFW / USFW Mapping |
| <input checked="" type="checkbox"/> Atascadero Native Tree Ordinance & Guidelines | <input type="checkbox"/> CA Natural Species Diversity Data Base |
| <input checked="" type="checkbox"/> Atascadero Climate Action Plan (CAP) | <input checked="" type="checkbox"/> Archeological Resources Map |
| <input checked="" type="checkbox"/> Atascadero Downtown Revitalization Plan | <input checked="" type="checkbox"/> Atascadero Mutual Water Company Urban Water Management Plan |
| <input checked="" type="checkbox"/> Atascadero Bicycle Transportation Plan | <input checked="" type="checkbox"/> CalEnvironScreen |
| <input checked="" type="checkbox"/> Atascadero GIS mapping layers | <input type="checkbox"/> Other _____ |
| <input type="checkbox"/> Other _____ | <input type="checkbox"/> Other _____ |

EXHIBIT B – MITIGATION SUMMARY TABLE
La Plaza Downtown Redevelopment
PLN 2017-1649

Per Public Resources Code § 21081.6, the following measures also constitutes the mitigation monitoring and/or reporting program that will reduce potentially significant impacts to less than significant levels. The measures will become conditions of approval (COAs) should the project be approved. The City of Atascadero, as the Lead Agency, or other responsible agencies, as specified, are responsible to verify compliance with these COAs.

MITIGATION MEASURE	TIMING
Aesthetics	
AES-1 At the time of building permit submittal of the proposed project, applicant must submit a photometric plan showing locations of proposed on-sight lighting. Prior to final occupancy, City Staff and the applicant shall meet on-site and review lights at dusk condition to ensure off-site light spillage and glare are limited.	Prior to Building Permit Issuance
AES-2 Any luminaire pole height shall not exceed 14-feet in height to minimize off-site light spillage for consistency with the Atascadero Municipal Code.	Prior to Building Permit Issuance
AES-3 Limit intensity to up to 3.0 foot candles at ingress/egress, and otherwise 0.6 foot candle minimum to 1.0 maximum in parking areas and/or for street lighting, bollards, etc to be reviewed at the time of building final.	Prior to Building Permit Final
Air Quality	
AQ-1 Demolition activities can have potential negative air quality impacts, including issues surrounding proper handling, abatement, and disposal of asbestos containing material (ACM). Asbestos containing materials could be encountered during the demolition or remodeling of existing structures or the disturbance, demolition, or relocation of above or below ground utility pipes/pipelines (e.g., transit pipes or insulation on pipes). If this project will include any of these activities, then it may be subject to various regulatory jurisdictions, including the requirements stipulated in the National Emission Standard for Hazardous Air Pollutants. These requirements include, but are not limited to: 1) written notification, within at least 10 business days of activities commencing, to the APCD, 2) asbestos survey conducted by a Certified Asbestos Consultant, and, 3) applicable removal and disposal requirements of identified ACM. Please contact the APCD Engineering & Compliance Division at (805) 781-5912 for further information or go to slocleanair.org/rules-regulations/asbestos.php for further information. To obtain a Notification of Demolition and Renovation form go to the "Other Forms" section of slocleanair.org/library/download-forms.php .	Prior to Building Permit Issuance



MITIGATION MEASURE	TIMING
<p>AQ-2 Based on the information provided, San Luis Obispo APCD is unsure of the types of equipment that may be present during the project's construction phase. Portable equipment, 50 horsepower (hp) or greater, used during construction activities may require California statewide portable equipment registration (issued by the California Air Resources Board) or an APCD permit.</p> <p>The following list is provided as a guide to equipment and operations that may have permitting requirements, but should not be viewed as exclusive. For a more detailed listing, refer to the Technical Appendices, page 4-4, in the APCD's 2012 CEQA Handbook.</p> <ul style="list-style-type: none"> • Power screens, conveyors, diesel engines, and/or crushers; • Portable generators and equipment with engines that are 50 hp or greater; • Electrical generation plants or the use of standby generator; <p>Internal combustion engines;</p> <ul style="list-style-type: none"> • Rock and pavement crushing; • Unconfined abrasive blasting operations; • Tub grinders; • Trommel screens; and, • Portable plants (e.g. aggregate plant, asphalt batch plant, concrete batch plant, etc). <p>To minimize potential delays, prior to the start of the project, please contact the APCD Engineering & Compliance Division at (805) 781-5912 for specific information regarding permitting requirements.</p>	<p>During Construction</p>
<p>AQ-3 Effective February 25, 2000, the APCD prohibited developmental burning of vegetative material within San Luis Obispo County. If you have any questions regarding these requirements, contact the APCD Engineering & Compliance Division at (805) 781-5912</p>	<p>During Construction</p>
<p>AQ-4 Construction activities can generate fugitive dust, which could be a nuisance to local residents and businesses in close proximity to the proposed construction site. Projects with grading areas that are greater than 4-acres or are within 1,000 feet of any sensitive receptor shall implement the following mitigation measures to manage fugitive dust emissions such that they do not exceed the APCD's 20% opacity limit (APCD Rule 401) or prompt nuisance violations (APCD Rule 402):</p>	<p>During Construction</p>

MITIGATION MEASURE

TIMING

- n. Reduce the amount of the disturbed area where possible;
- o. Use of water trucks or sprinkler systems in sufficient quantities to prevent airborne dust from leaving the site and from exceeding the APCD's limit of 20% opacity for greater than 3 minutes in any 60 minute period. Increased watering frequency would be required whenever wind speeds exceed 15 mph. Reclaimed (non-potable) water should be used whenever possible. **Please note that since water use is a concern due to drought conditions, the contractor or builder shall consider the use of an APCD-approved dust suppressant where feasible to reduce the amount of water used for dust control.** For a list of suppressants, see Section 4.3 of the CEQA Air Quality Handbook
- p. All dirt stock pile areas should be sprayed daily and covered with tarps or other dust barriers as needed;
- q. Permanent dust control measures identified in the approved project revegetation and landscape plans should be implemented as soon as possible, following completion of any soil disturbing activities;
- r. Exposed ground areas that are planned to be reworked at dates greater than one month after initial grading should be sown with a fast germinating, non-invasive, grass seed and watered until vegetation is established;
- s. All disturbed soil areas not subject to revegetation should be stabilized using approved chemical soil binders, jute netting, or other methods approved in advance by the APCD;
- t. All roadways, driveways, sidewalks, etc. to be paved should be completed as soon as possible. In addition, building pads should be laid as soon as possible after grading unless seeding or soil binders are used;
- u. Vehicle speed for all construction vehicles shall not exceed 15 mph on any unpaved surface at the construction site;
- v. All trucks hauling dirt, sand, soil, or other loose materials are to be covered or should maintain at least two feet of freeboard (minimum vertical distance between top of load and top of trailer) in accordance with CVC Section 23114;
- w. "Track-Out" is defined as sand or soil that adheres to and/or agglomerates on the exterior surfaces of motor vehicles and/or equipment (including tires) that may then fall onto any highway or street as described in California Vehicle Code Section 23113 and California Water Code 13304. To prevent 'track out', designate access points and require all employees, subcontractors, and others to use them. Install and

MITIGATION MEASURE

TIMING

operate a 'track-out prevention device' where vehicles enter and exit unpaved roads onto paved streets. The 'track-out prevention device' can be any device or combination of devices that are effective at preventing track out, located at the point of intersection of an unpaved area and a paved road. Rumble strips or steel plate devices need periodic cleaning to be effective. If paved roadways accumulate tracked out soils, the track-out prevention device may need to be modified;

- x. Sweep streets at the end of each day if visible soil material is carried onto adjacent paved roads. Water sweepers shall be used with reclaimed water should be used where feasible. Roads shall be pre-wetted prior to sweeping when feasible;
- y. All PM₁₀ mitigation measures required should be shown on grading and building plans; and,
- z. The contractor or builder shall designate a person or persons to monitor the fugitive dust emissions and enhance the implementation of the measures as necessary to minimize dust complaints, reduce visible emissions below the APCD's limit of 20% opacity for greater than 3 minutes in any 60 minute period. Their duties shall include holidays and weekend periods when work may not be in progress. The name and telephone number of such persons shall be provided to the APCD Engineering & Compliance Division prior to the start of any grading, earthwork or demolition.

AQ-5 This project is in close proximity to nearby sensitive receptors. Projects that will have diesel powered construction activity in close proximity to any sensitive receptor shall implement the following mitigation measures to ensure that public health benefits are realized by reducing toxic risk from diesel emissions:

During
 Construction

To help reduce sensitive receptor emissions impact of diesel vehicles and equipment used to construct the project, the applicant shall implement the following idling control techniques:

- 3. California Diesel Idling Regulations
 - e. ***On-road diesel vehicles*** shall comply with Section 2485 of Title 13 of the California Code of Regulations. This regulation limits idling from diesel-fueled commercial motor vehicles with gross vehicular weight ratings of more than 10,000 pounds and licensed for operation on highways. It applies to California and non-California based vehicles. In general, the regulation specifies that

MITIGATION MEASURE

TIMING

drivers of said vehicles:

3. Shall not idle the vehicle's primary diesel engine for greater than 5 minutes at any location, except as noted in Subsection (d) of the regulation; and,
4. Shall not operate a diesel-fueled auxiliary power system (APS) to power a heater, air conditioner, or any ancillary equipment on that vehicle during sleeping or resting in a sleeper berth for greater than 5.0 minutes at any location when within 1,000 feet of a restricted area, except as noted in Subsection (d) of the regulation.
- f. **Off-road diesel equipment** shall comply with the 5-minute idling restriction identified in Section 2449(d) (2) of the California Air Resources Board's In-Use Off-Road Diesel regulation.
- g. Signs must be posted in the designated queuing areas and job sites to remind drivers and operators of the state's 5-minute idling limit.
- h. The specific requirements and exceptions in the regulations can be reviewed at the following web sites: www.arb.ca.gov/msprog/truck-idling/2485.pdf and www.arb.ca.gov/regact/2007/ordiesl07/frooal.pdf.

AND/OR

4. Diesel Idling Restrictions Near Sensitive Receptors
 In addition to the state required diesel idling requirements, the project applicant shall comply with these more restrictive requirements to minimize impacts to nearby sensitive receptors:
 - e. Staging and queuing areas shall not be located within 1,000 feet of sensitive receptors;
 - f. Diesel idling within 1,000 feet of sensitive receptors shall not be permitted;
 - g. Use of alternative fueled equipment is recommended; and
 - h. Signs that specify the no idling areas must be posted and enforced at the site.


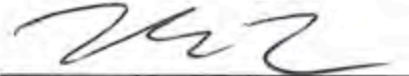
AQ-6	Exterior architectural coating shall be limited to Volatile Organic Compound (VOC) of 50 grams per liter maximum to the extent feasible.	During Construction
AQ-7	Interior architectural coating shall be limited to Volatile Organic Compound (VOC) of 0 grams per liter maximum to the extent feasible within residential and non-residential spaces.	During Construction

MITIGATION MEASURE	TIMING
Green House Gas Emissions	
GHG-1 The proposed project shall include a minimum of four (4) electric vehicle (EV) chargers within the proposed parking lot. Vehicle chargers shall be counted as parking spaces for the purposes of parking calculations.	Prior to Building Permit Final
Cultural Resources	
CR-1 Recordation and documentation shall be produced according to the California Register of Historic Resources prior to issuance of demolition permits for both 6452 and 6490 El Camino Real. Documentation shall include the following: <ul style="list-style-type: none"> • Completion of State of California Form DPR 523B; • Large scale color photographs of exterior and interior with scale and caption; • Large scale site plan and floor plan of existing building(s) with legend and callouts of location photographs taken; • Copies of selected historic photographs; • Written description of construction and use of the structures; • Digital copies of all materials submitted in PDF format. 	Prior to Demo Permit Issuance
CR-2 In the event that human remains are discovered on the property, all work on the project shall stop and the Atascadero Police Department and the County Coroner shall be contacted immediately. The Atascadero Community Development Director shall be notified. If the human remains are identified as being native American, the California Native American Heritage Commission (NAHC) shall be contacted at (916) 653-4082 within 24 hours. A representative from both the Chumash Tribe and the Salinan Tribe shall be notified during the excavation of any remains.	During Construction
Noise	
NO-1 Soffit vents, eave vents, dormer vents and other wall and roof penetrations shall be on the walls and roofs facing away from the noise source wherever possible.	Prior to Building Permit Issuance
NO-2 The walls of habitable spaces and office space on second and third stories nearest the noise source shall have wall construction with an S.T.C. (Sound Transmission Class) rating of 30 or greater. For instance, stucco exterior or equivalent on 2" x 4" stud walls with minimum R-13 batt insulation and two layers of ½" gypsum board on the interior will provide an S.T.C. rating of 30 or greater along these walls.	Prior to Building Permit Issuance
NO-3 Common acoustic leaks, such as electrical outlets, pipes, vents, ducts, flues, and other breaks in the integrity of the wall, ceiling or roof construction on the side of the dwellings nearest transportation noise sources shall receive special attention during construction. All construction openings and	Prior to Building Permit Issuance

MITIGATION MEASURE	TIMING	
NO-4	<p>joints on the walls on the noise facing side of the site shall be insulated, sealed and caulked with a resilient, non-hardening, acoustical caulking material. All such openings and joints shall be airtight to maintain sound isolation.</p> <p>To meet the interior LDN 45 dBA requirements, windows for habitable spaces on the second and third floors of affected units facing the noise source shall be of double-glazed construction with one light of laminated glass, and installed in accordance with the recommendations of the manufacturer. The windows shall have full gaskets, with an S.T.C. rating of 30 or better, as determined in testing by an accredited acoustical laboratory.</p>	<p>Prior to Building Permit Issuance</p>
Transportation & Traffic		
TP-1	<p>The applicant or developer must relocate the existing pedestrian crossing at the midblock crossing at the intersection of East Mall and El Camino Real to a location to the north subject to the approval of the Public Works Director. The preferred location would be at a signalized intersection. The applicant shall re-stripe and provide conduit for a new pedestrian crossing signal adjacent to the project site. The proposed improvement may be eligible for TIF credits based on the adopted Capital Improvement Plans at the time of building permit issuance.</p>	<p>Prior to release final sign off of off-site improvements.</p>
TP-2	<p>The applicant shall work with City Staff and the Regional Transit Authority to relocate the adjacent southbound RTA stop that currently exists near the intersection of Entrada and El Camino Real. This includes relocation or re-construction of a bus shelter, as deemed appropriate by the City Engineer.</p>	<p>Prior to release final sign off of off-site improvements.</p>
Utilities Services		
UT-1	<p>The proposed project must pay all applicable sewer connection fees at the time of building permit issuance. A fee credit may be applicable for all existing fixtures and connections listed in demolition permits for buildings removed.</p>	<p>Prior to Building Permit Issuance</p>
UT-2	<p>The applicant shall be responsible for providing fair share impact fees for wastewater in effect at the time of building permit issuance.</p>	<p>Prior to Building Permit Issuance</p>
UT-3	<p>The developer must include trash and recycle receptacles near public entries and a minimum of (3) trash and recycle receptacles within the proposed plaza to be dispersed throughout the length of the plaza. Receptacles design shall be approved by Staff and included on landscaping plans at the time of building permit submittal.</p>	<p>Prior to release final sign off of on-site improvements.</p>

The applicant agrees to incorporate the above measures into the project. These measures become a part of the project description and therefore become a part of the record of action upon which the environmental determination is based. All development activity must occur in strict compliance with the above mitigation measures. The measures shall be perpetual and run with the land. These measures are binding on all successors in interest of the subject property.

The applicant understands that any changes made to the project description subsequent to this environmental determination must be reviewed by the Community Development Director or their designee and may require a new environmental analysis for the project. By signing this agreement, the owner(s) agrees to and accepts the incorporation of the above mitigation measures into the proposed project description.

 Signature of Owner(s)	Michael Zappas Name (Print)	11/16/17 Date
 Signature of Owner(s)	Max Zappas Name (Print)	11/16/17 Date

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Figure 1 – Location Map / General Plan & Zoning



Figure 2 – Aerial Mapping



Figure 3 – Site Plan

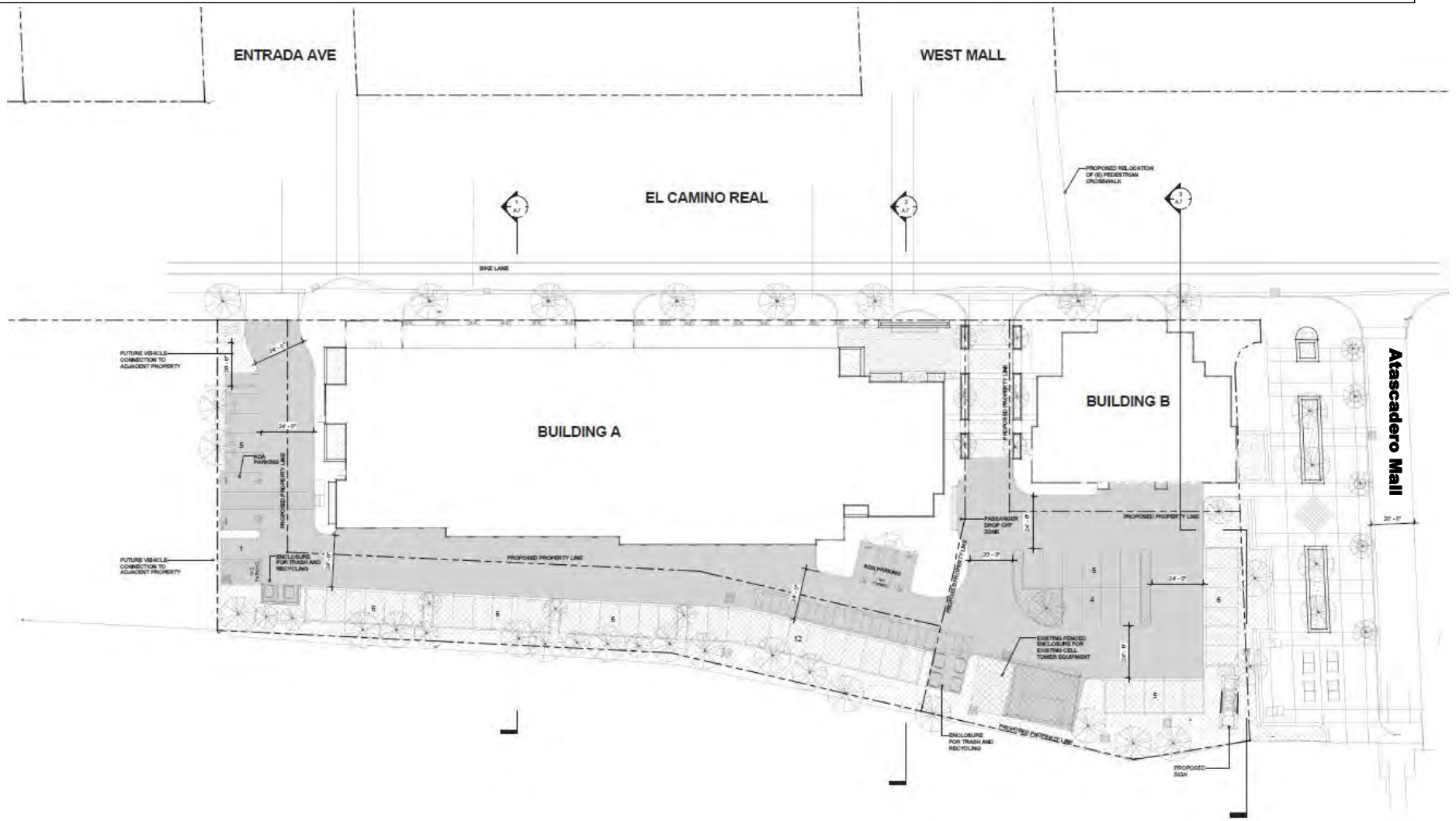


Figure 4 – Elevations



BUILDING B
1 EAST ELEVATION
1/16" = 1'-0" (24 x 36 SHEET)

BUILDING A



2 BUILDING A PERSPECTIVE



3 NORTH ELEVATION BUILDING A
1/16" = 1'-0" (24 x 36 SHEET)



4 SOUTH ELEVATION BUILDING A
1/16" = 1'-0" (24 x 36 SHEET)



1 WEST ELEVATION
1/16" = 1'-0" (24 x 36 SHEET)



2 BUILDING B PERSPECTIVE



3 NORTH ELEVATION BUILDING B
1/16" = 1'-0" (24 x 36 SHEET)



4 SOUTH ELEVATION BUILDING B
1/16" = 1'-0" (24 x 36 SHEET)

Figure 4 – Elevations

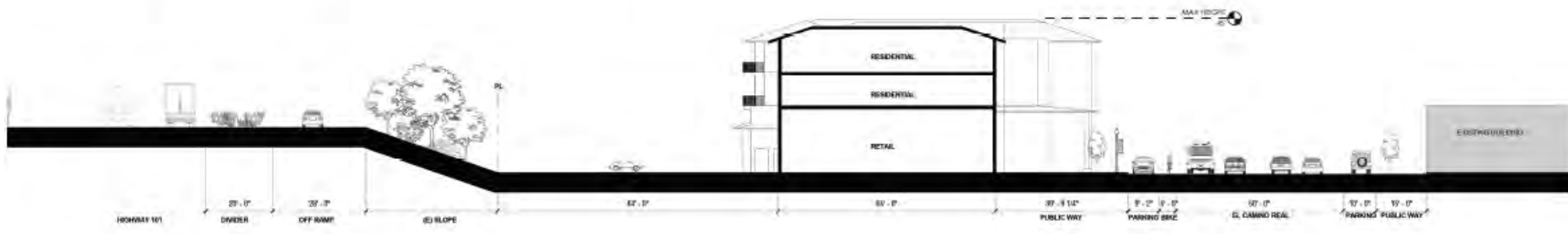
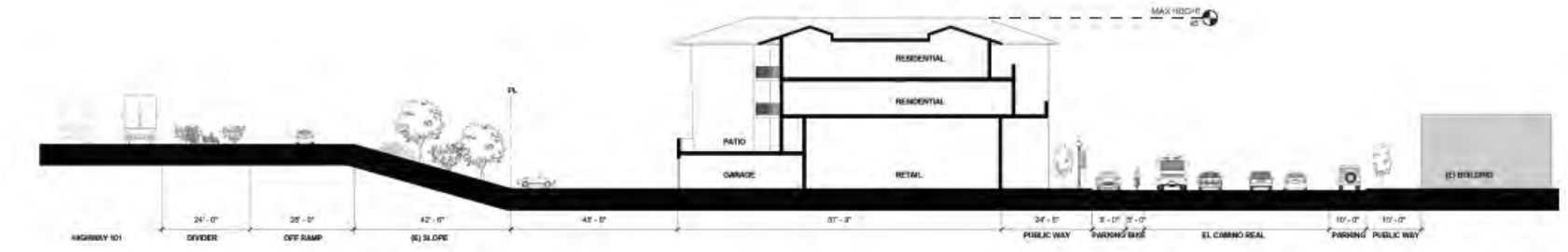


Figure 5 – Site Photos



View of the vacant lot and the former Jack-in-the-Box site.



View of Karen's Beauty supply. This is the last building of the former Golden Way Block.



**View of pedestrian tunnel along Atascadero Avenue
(Atascadero Mall)**



Unmanned Wireless telecommunication facility.



Proposed beauty supply store to be demolished.



Proposed coffee shop to be demolished.



View of Atascadero Avenue at the intersection of El Camino Real



View of City all from proposed public plaza. Existing buildings have been demolished.

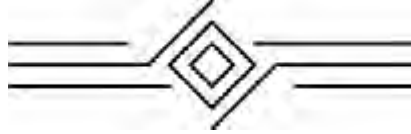
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Figure 6 – Historical Report

See Following



CULTURAL RESOURCES ASSESSMENT



MALIBU BREW BUILDING 6490 EL CAMINO REAL, ATASCADERO

September 2017

Location: 6490 El Camino Real
City of Atascadero, San Luis Obispo County, CA

APN: 030-191-017

USGS Quad: Atascadero, Calif.
UTM Zone 10, 39299471 N, 711485 E

Prepared for: Mike Zappas
8189 San Dimas Lane
Atascadero, CA 93422

Prepared by: Rebecca Loveland Anastasio
Cultural Resource Services
7800 Santa Cruz Road
Atascadero, CA 93422

Results: Positive for historic structure

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FIGURE 1: PROJECT PARCEL (APN MAP)

APPENDIX: CHAIN OF TITLE

1.0 INTRODUCTION

In August 2017, Mr. Mike Zappas authorized a historic property assessment of an existing structure with a street address 6490 El Camino Real in the city of Atascadero, San Luis Obispo County, California. This cultural resources evaluation was requested in order to fulfill the various mandates of the California Environmental Quality Act (CEQA) and planning directives of the City of Atascadero which are required for the proposed redevelopment of the project parcel. The assessment was carried out in accordance with the Recommendations for Archaeological Impact Evaluations: Procedures and Guidelines for Archaeological Consultant Qualifications (Edwards 1979) and the County of San Luis Obispo guidelines for survey reports (n.d.). As such, this report consists of the results of a review of pertinent literature, maps, and images; a chain of title search; and a field assessment of the existing building.

2.0 PROJECT LOCATION AND DESCRIPTION

The project property is identified as a Portion of Lot 18, Block HB in the City of Atascadero (APN 030-191-017), San Luis Obispo County, California [Fig. 1] with an address of 6490 El Camino Real. The property is situated in the historic downtown of Atascadero, across El Camino Real from Sunken Gardens and the Atascadero City Hall, which is on the US Register of Historic Places. The project parcel is located at the northwest corner of El Camino Real and Atascadero Mall, which at one time (prior to the construction of Highway 101) connected Sunken Gardens and the City civic center with Atascadero High School. The project parcel is bounded to the north by 6452 El Camino Real, a commercial parcel which is the site of an old small motel re-purposed as a beauty supply store. The project parcel is bordered to the west by Highway 101, to the east by El Camino Real, and to the south by Atascadero Mall – formerly the site of a small building used by the Atascadero Chamber of Commerce (but now demolished). Atascadero Mall is no longer a through street and is currently used primarily for parking.

The project parcel is part of a larger property which is slated for redevelopment as “La Plaza,” a mixed-use commercial development. At present, the project property is occupied by a small former service station that is being used as a coffee shop known as Malibu Brew. Current plans propose the total demolition of the existing structure, along with other structures on adjacent parcels. One of the adjacent properties which will be redeveloped as part of the proposed La Plaza project was the site of a historic commercial development known as “The Golden Way Block,” which unfortunately burned down in 2009. Because the existing building was more than 50 years old, and had the potential to be associated with the early commercial development of Atascadero, under the California Environmental Quality Act (CEQA), the existing building needed to be assessed for both architectural and historic significance.

3.0 BACKGROUND

3.1 HISPANIC PERIOD

The first Europeans to explore the Central Coast were seafarers. Although Juan Rodriguez Cabrillo sailed the California coast in 1542, there is no indication that any of his ships made landfall in San Luis Obispo County. In contrast, Pedro de Unamuno visited Morro Bay in 1587, and Sebastian Cermeno entered San Luis Obispo Bay in 1595 (Greenwood 1978:520). The Central Coast then remained unexplored by the Spanish until the overland expeditions in the late 1760s and 1770s. The first party, that of Gaspar de Portola and Father Juan Crespi, followed what became the traditional coastal route

between San Diego and Monterey. They hugged the coast north of Point Concepcion, but turned inland near Point Sal to avoid the sand dunes of Nipomo and Pismo Beach. They camped near San Luis Obispo, and then turned toward the coast again along Morro Bay. Other explorers who followed this route included Juan Bautista de Anza, Friar Garces, and Friar Juan Diaz in 1774, Anza and Pedro Font in 1776, and Friar Garces with three Indian guides in 1776 (Beck and Haase 1974:16).

The favorable reports of Portola and Crespi led to the founding of Mission San Luis Obispo de Tolosa in 1772. This mission, the 5th of the 21 missions founded in California, was established on September 1, 1772 (Beck and Haase 1974:19). As one of five missions located within Chumash territory, Mission San Luis Obispo would have been the mission with the greatest impact on the aboriginal population living in the study area (Grant 1978:506). In fact, the tribelet designation Obispeno reflects their association with the Mission (Greenwood 1978:520).

Through the 1820s and 1830s, Mission San Luis Obispo was the only real settlement in San Luis Obispo County (Angel 1883:217). Other settlements in the area were delayed due to frequent raids on the Mission by runaway neophytes and "wild" Yokut Indians. These raids were part of a trend toward physical resistance to the mission system, as well as a means of securing horses and cattle. By 1828, horsemeat and beef had replaced the acorn as a staple of the Indian diet; horses also resulted in increased mobility for the Indians and enabled them to be extremely effective raiders. At Mission San Luis Obispo, over a thousand head of stock were taken in a single raid (Beck and Haase 1974:22-23).

The transition from Spanish to Mexican rule in the 1820s led to several changes in the study area. The hide and tallow trade expanded rapidly after Mexican independence, with some fifty to eighty thousand hides being shipped annually. San Luis Obispo soon became a major hide and tallow port, and was visited by ships of many nations (particularly American and English) bearing trade items such as cloth, shoes, saddles, and hardware. Soon, foreign traders established local offices which evolved into full-time trading posts, and many foreign merchants became permanent residents of California (Beck and Haase 1974:41).

The process by which Mexicans replaced Spanish authority in California not only encouraged the opening of international trade but also transformed ideas of landholding. The Spaniards had kept most lands in the name of the crown, but Mexican officials distributed property more widely to individual owners by deeding 813 land grants, or ranchos, to private claimants throughout California between 1821 and 1846 (Beck and Haase 1974:24). During the Mexican period, the study area was situated within Rancho Atascadero [Beck and Haase 1974:35] [Fig. 1]. Rancho Atascadero, consisting of 4,348 acres, was granted to Trifon Garcia on May 6, 1842 [Abeloe 1966:387; Beck and Haase 1974:35]. Reportedly, Garcia lived and raised sheep on the Rancho for two years before repeated Indians raids caused him to abandon the land, selling it later to W. Breck for 500 pesos [Nicholson and Loughran 1981:48]. The location of his dwelling is unknown.

3.2 EARLY AMERICAN PERIOD

Throughout the Hispanic Period, land was abundant and settlers were few in number. Thus, land had minimal value so that boundaries between ranchos were vague and frequently overlapped. Not until the United States occupation in 1846 was land coveted. In the early years of California statehood, land claims became a question of some importance. Euro-American immigrants often became squatters on the ranchos, defending their claims by force and law. The political and economic savvy of the Americans soon put the Californios at a disadvantage; exorbitant property taxes were voted in by

Americans, and grant claims were disputed in court and before the California Land Claims Commission. Many rancho owners were forced to sell portions of their ranchos in order to pay the taxes and/or defray staggering legal expenses. Other complicating factors leading to division of the ranchos included inheritance disputes, intermarriage between Mexicans and Euro-Americans, and the demands of gambling debts (Bean 1978:182-189).

On March 6, 1855, Henry Haight filed claim to Rancho Atascadero before the U.S. Land Commission [Jespersen 1939:50], apparently having acquired it from W. Breck [Nicholson and Loughran 1981:48]. An appeal against the claim was denied in 1857 [Jespersen 1939:50]. In the tax records of 1860s, Atascadero Rancho is shown belonging to Joaquin Estrada, Mexican grantee of Rancho Santa Margarita (the former Asistencia) [Angel 1883:173]; however, on June 18 of that year, both Rancho Atascadero and Rancho Santa Margarita were patented to the wealthy land baron Martin Murphy [Jespersen 1939:50].

Through the 1860s, 70s and 80s, Martin's son, Patrick Murphy, ran and then owned the great Ranchos of Atascadero, Asuncion, and Santa Margarita. The headquarters of this 70,000 acre ranch was the Rancho Santa Margarita -- the buildings still stand and are in use north of town near the ruins of the Asistencia [Abeloe 1966:387; Angel 1883:32-33; USGS 1965]. This vast ranch was used almost solely for raising cattle, providing grazing land for thousands of head of stock [Abeloe 1966:387]. However, beginning in the 1870s, Murphy was forced to sell portions of his ranch in order to pay various debts and mortgages [Jespersen 1939:129; Harris 1874]. As a result, in the 1880s, Rancho Atascadero was owned by Patrick Murphy's brother, James [Morrison and Haydon 1917:76]. In the 1890s Rancho Atascadero and the eastern portion of Rancho Asuncion came into the hands of J.H. Henry [Jespersen 1939:131; Krieger 1988:83; Nicholson and Loughran 1981:49]. Much of the remainder of Rancho Asuncion became the Eagle Ranch of Johann Henrik, Baron von Schroeder [Krieger 1988:82].

3.3 LATE AMERICAN PERIOD

In 1912, Edward Gardner Lewis, a successful magazine publisher, bought Rancho Atascadero from J.H. Henry. Lewis founded the utopian, planned community of Atascadero in 1913. He had previously created such a community, at University City, Missouri. Lewis put together a group of investors, paid J.H. Henry \$37.50 per acre, and celebrated acquisition of the ranch on July 4, 1913. As investors came to homestead the land that they had bought with their down payments, the area was transformed into a "tent city" with tents situated on land now occupied by Colony Square and Bank of America. Lewis employed the services of experts in agriculture, engineering and city planning to develop his dream colony for the anticipated 30,000 residents. In 1914 the land was surveyed, roads were laid out, and the former Rancho Atascadero subdivided into large family tracts, large enough for a house site and a small orchard or farm [Jespersen 1939:131; Krieger 1988:83]. The first homes 57 homes were built in 1915; by 1917 there were a total of 300 constructed [Lewis 1974]. The homes built on the Atascadero parcels ranged from modest single-story bungalows to multiple story "mansions" as people came to Lewis' new community to begin a new life. Thousands of acres of orchards were planted, a water system was installed, and construction began on an 18 mi road (now Highway 41 west) through the Santa Lucia Mountains to the ocean (Morro Bay), where Lewis built cottages and a beachfront hotel called the Cloisters [Lweis 1974; Travis 1960].

The history of development in Atascadero was rather convoluted and troubled. Over the years, the organization that owned and promoted this development changed, although the same principals were involved. Originally set up as the Lewis Foundation Corporation, the holdings were transmitted to

The Colony Holding Company in order to issue bonds [Travis 1960]. At the petition of Lewis, who felt the name “Colony” had developed religious or socialist connotations, the court ordered the name changed to Atascadero Estates Inc. in August 1921, although promotional documents as far back as 1916 used the name Atascadero Estates [Allan 2016; Unknown 1916]. Interestingly, all three names (Lewis Foundation Corporation, Colony Holding Company, and Atascadero Estates) appear apparently interchangeably in various title documents through the 1930s. [Chain of Title].

During World War I, Lewis entered a period of financial difficulty. He had already sold thousands of parcels and had contracted for infrastructure improvements at fixed prices. The advent of the war caused the cost of labor and materials to more than double. According to his autobiography, “Construction and development matters were now at a standstill, but interest, upkeep, taxes, and carrying charges kept right on and began to mount by the hundreds of thousands of dollars until I was heavily in debt.” [Lewis 1969]. A \$1,750,000 deed of trust, covering the entire real and personal property then owned by the Colony Holding Corporation which secured the bond issue, was filed May 19, 1916 with the Anglo-California Trust Company as the trustee. The bond issue called for a 6% first mortgage with 15-year convertible gold bonds of the Colony Holding Corporation. A large number of the bonds were initially purchased, enabling the Holding Corporation to go on with their task of building a new community [Travis 1960]. However, Lewis was also in a great deal of personal debt. To defray this debt, Lewis invested in a chemical plant in Oakland and oil fields in California, Montana, Wyoming, using investor money [Lewis 1969].

After the war, Lewis and his investors were caught in the sudden drop of crop prices as war-time government subsidies ended. In addition, his oil investments did not pan out as he had hoped [Lewis 1969]. By 1924 Lewis was bankrupt and the entire community in debt. Lewis was forced into involuntary bankruptcy when he was sued for \$20 million by investors in 1925 [Megowan 2013]. The Colony Holding Company was dissolved, and the property placed under Seattle attorney Oscar L. Willett as trustee of the newly constituted Atascadero Development Syndicate. These events, plus the ensuing great Depression, slowed the growth of Atascadero until after World War II [Linn 2009; Jespersen 1939:132; Krieger 1988:73].

3.4 PARCEL HISTORY

The history of the project parcel, its ownership, and uses is somewhat convoluted, with much conflicting information available. According to the Chain of Title by First American Title, the first owner of the property, William E. Kullgren, purchased the property from Oscar L. Willett and the Atascadero Development Syndicate on July 18, 1926. However, other records show that he had constructed the Golden Way Auto Camp on the parcel and adjacent properties to the north by 1925 [Allan 2013:27]. This suggests that Kullgren had originally acquired the parcel from Lewis, but the legal ownership of the parcel was clouded by the Lewis bankruptcy.

William E. Kullgren was an early booster of Atascadero, and wrote many articles and pamphlets praising the qualities and potential of Atascadero despite all the problems engendered by the Lewis bankruptcy. Kullgren is also considered to be one of the early pioneers of the poultry industry in Atascadero, as well as one of the first commercial developers. Prior to his development of the Golden Way Block, all of the commerce in Atascadero was conducted in the Mercantile Building at the corner of Traffic Way and Olmeda [Allan 2008:85].

Kullgren was born on May 30, 1885 in Plymouth, Devon, England, and immigrated to the U.S. in 1918, first settling in New York, where he met and married Lois C. Eldridge in 1923. Lois was 6 years his senior, and the part-owner and treasurer of “New York Metal Store” while William Kullgren was a dry good clerk [US Census 1920; US Draft Registration 1942]. Soon after their marriage, the couple must have moved to Atascadero, as William Kullgren makes many appearances in the Atascadero News by 1925. The US Census of 1930 lists the Kullgrens as living on Olemda Avenue; William is listed as a builder and self-employed business person; Lois is listed as an unpaid business manager [US Census 1930].

Kullgren also gained notoriety as one of 28 people indicted for sedition during World War 2. After the collapse of his business enterprises during the Great Depression, Kullgren became an astrologer and publisher of a monthly newsletter called “The Beacon Light.” He was very much against the US involvement in the war, held rather anti-Semitic views, and had an absolute distrust of Franklin D. Roosevelt. According to some sources, Kullgren was a member of the Silver Shirts or Sliver Legion of America, a paramilitary organization modeled after Hitler’s Brownshirts. The publication that got Kullgren charged with sedition was the Beacon Light edition of January 1942 – 74 pages (including advertisements) of appeals to get out of the war, astrological predictions of the ascendancy of Hitler, the inevitability of a German empire, the positive changes such an empire would bring to the world, and the evil plans FDR had to become an American dictator, along with various conspiracy theories involving Jews and the corrupt pre-war governments of most European nations [Middlecamp 2012; Astro Databank 2010; Kullgren 1942; San Luis Obispo Telegram-Tribune 1942]. In July 1942, he was formally indicted on sedition charges and jailed in Los Angeles with \$25,000 bail set. After a number of prominent Atascaderans appeared at his trial in October 1942 and testified on his behalf, he was determined not to be a threat and was released [Middlecamp 2012; Wilkins personal communication 2017-08-28].

Kullgren's first development of the project area was the Golden Way Auto Camp, which in 1926 consisted of a cluster of buildings on Lots 18 and 19 (the project parcel was undeveloped). The Golden Way Auto Camp contained a building with two offices, “gas and oils” with gas pumps, a barber shop, a cleaners, and storage (this became the Golden Way Service Station). A small fruit stand stood at the edge of the state highway, although Allan identified it as an ice cream stand [Allan 2008:85]. Further back on the parcel were a kitchen building, and an auto repair shop with battery storage and a bath house, and in back of those were two buildings providing rooms and “auto shelters” [Sanborn 1926] (Figure 2). By 1927, the Golden Way Block included the Golden Way Tavern, constructed north of the service station, and in 1928 Kullgren announced a large expansion of the Golden Way Block [Allan 2013:27; Atascadero News 1928:1]. In 1929 and 1930, several transactions took place regarding adjacent roads, with Kullgren deeding an eastern strip of his parcels to the State of California for the State Highway, Willett deeding the southeast corner of Lot 18 (including the project parcel) to Kullgren, and Kullgren deeding a portion of Lot 18 (probably Atascadero Mall) to San Luis Obispo County [First American Title].

Photographs of the project parcel in the early 1930s (available at the Atascadero Historical Society) indicate that it was used as a miniature golf course during this time. It is first visible on an aerial photo dated 1930, and it also appears on the Sanborn Fire Insurance Map of 1931 (Figure 3). Known as “El Camino Links” and possibly owned by Harold Wilkins [Wilkins, personal communication 2017-08-28], the miniature golf course covered most of the project parcel. By 1931, Kullgren had expanded the Golden Way to include a large garage, which overlapped the project parcel. The Golden Way Garage could store 50 cars, and eventually became a Ford dealership, and later a Chevrolet dealership [Sanborn

1931; Allan 2008: 85]. The Golden Way Service Station, which had become an Associated Gasoline station, was still situated just to the north of the garage, along with the bath house and auto repair facility behind it; the auto camp had been consolidated and converted to an auto court with 8 rooms and 12 garage spaces. The former kitchen had been converted to hotel rooms, and a new building, the Golden Way Tavern, was built at the northern end of the Golden Way Block. The Golden Way Tavern was two stories, with hotel rooms on the second floor, and a lobby, restaurant, pool hall, and pharmacy on the first [Sanborn 1931; Allan 2013:27-28].

In 1933, Kullgren took out a \$29,525 deed of trust from the National Bank and Trust Co. of Santa Barbara, probably to complete his vision for the Golden Way Block [First American Title]. The expanded block, which extended for an additional 144 feet north of the Golden Way Tavern, does not appear on the 1931 Sanborn, despite Kullgren having declared his intentions of that expansion in 1928. The additional structure, which became known as the Atascadero Hotel, had space for six commercial enterprises on the first floor, and 40 hotel rooms upstairs [Allan 2013:28], and appears in many photos dated to the mid-1930s and 1940s.

The El Camino Links miniature golf course was advertised in the 1932 Polk City/County Directory, but had no listings or ads after that year, although it still can be seen in photos that may date to the mid-1930s. By 1940, the El Camino Links had clearly been abandoned, and one of the buildings associated with the golf course may have been re-purposed as The Log Cabin Cafe since it appears in aerial photos from the late 1930s and 1940. The restaurant first appears in clearly recognizable form in a 1940 postcard showing El Camino Real from the Atascadero Creek Bridge. It next appears in a 1942 City/County Directory as owned and operated by Richard Kay [Polk 1942], although in 1940 Kay worked as a cook at Pittenger Rest Home in Atascadero [Polk 1040]. In December 1942, Kay closed the restaurant to go into defense work for World War 2 [Allan 2008:108]. The Log Cabin Cafe next appears in a 1943 article in the San Luis Obispo Telegram-Tribune, as one establishment of many robbed in late September. At that time, the restaurant may have been run by Karl and Theresa Pierce, who sold their San Luis Obispo restaurant in the same year [San Luis Obispo Telegram-Tribune 1943 p. 9]; another article in the same year advises people wishing to join the San Luis Obispo Sportsmen's Association to sign up with Karl Pierce "at his Atascadero restaurant" [San Luis Obispo Telegram-Tribune 1943:10]. A 1945 article mentions "Karl and Theresa Pierce of Atascadero [San Luis Obispo Telegram-Tribune 1945 p. 1], and an advertisement in January 1946 is for the "Log Cabin Cafe, Atascadero, Karl and Theresa Pierce" [Atascadero News 1946 p. 6].

On August 6, 1946, Gordon and Dorothy Fox, who had been employed at the Atascadero Hotel since 1940 [Polk 1940], purchased Lots 16-20 and Lot 22 (the entire Golden Way Block) from San Barbara Mutual Building and Loan [First American Title], including the buildings of the "Atascadero Hotel, The Log Cabin, and some other smaller buildings" [Allan 2008:9]. This purchase may have caused the relocation of the Log Cabin Cafe across the street to 6455 El Camino Real. Although the advertisement in the Atascadero News in January 1946 associates the Log Cabin Cafe of Atascadero with Karl and Theresa Pierce" [Atascadero News 1946 p. 6], historian Lon Allan has the Log Cabin Cafe relocated across the State Highway (El Camino Real) and purchased by Florence and Earl Johnson in the same year [Allan 2013:40]. Interestingly, a mid-1940s photo of El Camino Real from the Atascadero Creek Bridge clearly shows two cafes in existence across the street from each other, suggesting that both cafes may have been in business at the same time, at least for a short period. In October 1948, an ad appeared in the San Luis Obispo Telegram-Tribune stating the "Log Cabin Cafe, Atascadero, now open for business," [San

Luis Obispo Telegram-Tribune 1948:14], which suggests that the Johnson's may have actually purchased the Log Cabin Cafe at 6455 El Camino Real in 1947 or 1948, rather than 1946.

The existing building on the project parcel was in place in 1949, based on an aerial photo of Atascadero dated to 1949 [EDR 2017]. Assessment records at the San Luis Obispo County Assessor's office, which have a first appraisal date of 1965, use a 1948 valuation table for the main building, canopy, islands, two auto repair wells, light tower, 3000 gallon tank, and fence, suggesting that all these were in place on or about 1948. In 1948, the Atascadero service station listing in the Polk City/County directory includes a listing for Petersen's Service Station, South State Highway, corner the Mall [Polk 1948]. This could have been the Richfield Station at 6700 El Camino Real; however the 1950 listing includes both the Richfield Service Station, South State Highway corner The Mall AND Petersen's Service Station South State Highway corner The Mall [Polk 1950:460], suggesting that the Petersen station was the subject building on the project parcel.

Samuel Petersen, known more often as Sam, was born in Hayward California in 1895 [World War 2 Draft Registration Card]. He married his wife Esther in 1922, and lived in Alameda County until 1930 [US Census 1930]. By 1931, Sam and Esther had relocated to Atascadero, where Sam was first employed at Wards Garage (5860 El Camino Real in 1931 [San Luis Obispo Telegram-Tribune 1931:5]. By 1937, he was managing the "new Richfield Garage" which was probably the service station at 6700 El Camino Real [San Luis Obispo Telegram-Tribune 1937:4]; the last mention of Sam Petersen in conjunction to the Richfield Station was in 1941 [San Luis Obispo Telegram-Tribune:4]. His employment is not further noted until 1954, when he was employed by Walter Leverenz's Ford dealership until his death in 1955 [Polk 1954; California Death Records 1955].

The first directory listing of the subject building with a property address of 6490 El Camino Real was in 1954, when "Simpson's Associated Service 6490 El Camino Real Atascadero" appears under service station listings and "Simpson's Associated Service (Lorenzo P. Simpson) gas sta 6490 El Camino Real" appears in the personal listings. However, an ad in the 1953 Atascadero High School Year book shows a picture of the subject building with a caption "Simpson's Associated, the busiest corner in town." The 1953 Polk directory does also list "Lorrie P Simpson, serv sta S state hwy" in both the service station and personal listings. Lorenzo P. Simpson was born in Illinois in 1905 [Ancestry Public Family Tree]. By 1930, he had married Phyllis Deborah DeCou and they were living in San Bernadino and was working as a salesman [1930 Census]. By 1935, they had relocated to Atascadero; various records indicated that he was working as an independent salesman [U.S. Census 1940] or as a "coin opr" [Polk 1946-47]. Simpson operated the subject service station from at least 1953 until 1960 [Polk 1955, 1957, 1958; Pacific Telephone 1960], although it became listed as a "Flying A" Service Station in 1957. In 1961, Simpson turned the Flying A station over to John H. Taylor [Pacific Telephone 1961], who continued to operate the facility as a Flying A until 1965, and then as a Phillips 66 until 1969, when it became Dan's Phillips 66 [Pacific Telephone 1963; 1965-1969].

The history of Associated Oil is informative. In 1900, the five largest companies in the Kern River Oil Field of California entered into an agreement to form the Associated Oil Company, which was incorporated in 1901 and began actively producing and marketing crude oil in 1902. In 1926, the Associated Oil Company was sold to a new holding company, the Tide Water Associated Oil Company. In 1938, the Tide Water and Associated companies were dissolved to form the Tidewater Associated Oil Company. Across the western U.S., service stations became "Associated Flying A" while stations in the East became "Tydol Flying A". During the 1950s, the Associated and Tydol brands gradually fell into disuse, and were dropped entirely in 1956. That same year, "Associated" was removed from the

corporate name. In 1966, Phillips Petroleum Company purchased Tidewater's western refining, distribution and retailing network. Phillips immediately rebranded all Flying A stations in the region to Phillips 66 [Wikipedia]. This history is reflected in both the history of the parcel and the service station, not only in the name changes from Associated Service Station to Associated Flying A to Flying A to Phillips 66, but also in other records. In 1965, after the Atascadero Hotel burned down [Allan 2012], Gordon and Dorothy Fox sold the project parcel to Tidewater Oil Company, which appears in both the Grant Deeds [First American Title], and the County Assessor records in 1965 since it was the lot split and sale which probably triggered the assessment. The following year, Tidewater sold the station and parcel to Phillips Petroleum, when it became John's 66 service [First American Title; Pacific Telephone 1967].

At some point between 1969 and 1980, the project parcel was acquired by Desert Petroleum [Chain of Title], and there are no directory listings for the address or ads for the service station, which suggests a period of disuse. This is corroborated by the 1971 assessment of the parcel, which indicates that the gasoline tanks had been removed. In 1980, Desert Petroleum sold the property to Ben Hoff, and by 1986, the building was being operated as Ed's Garage [First American Title; Pacific Bell 1986]. The building spent one year as "Pipe Dream" in 1994 [Pacific Bell 1994] which was probably associated with the conversion of an auto repair garage to a retail establishment, including the removal of the repair wells, lights and signage, and the infill of the garage doors. In 1998, the building was used as a retail flower shop, including the removal of a 60' freestanding tower sign [Assessment 1998]. Today the building is in use as a Coffee Shop known as Malibu Brew. In 2009, most of last remaining portions of the remaining historic Golden Way Block burned to the ground, leaving only the facade of the small 1925 service station, which was torn down in 1912 [Allan 1912].

4.0 FIELD ASSESSMENT

An intensive architectural field assessment was conducted on August 23, 2017 by the author. This assessment takes into account both the structural and architectural integrity of the building.

● Massing and Structure

The construction materials used for this service station indicate that it was built either before World War 2 or after it. Box-type services stations were in common use since the 1930s, and most commonly used light steel construction. However, due to steel shortages during the war, most gas stations during that time were built of concrete block or more rarely poured concrete. The history of the parcel and the operational history of the gas station strongly suggests that it was built post-war, in 1948.

The building is of a classic Streamline Moderne style with its rounded corners and flat roof. It consists of a 611 square foot rectangular structure; the dimensions are roughly 21'-8" by 28'-3", with the long axis parallel to El Camino Real. The northeast corner features a pop-out, original to the building, extending roughly 18 inches beyond the primary building walls. This pop-out and the space behind it once contained the service station office, which now serves as the coffee shop kitchen. The former auto repair garage makes up most of the existing interior space, and currently contains the dining area and the service counter. A 144 square foot canopy (roughly 9'-6" wide by 15 feet long), which once covered the gasoline pumps, now serves as a drive-through for the coffee shop. The back of the building has a smaller 4' by 10' post-service station era addition that holds water heater and furnace. The back of the building also features a flat shed roof of indeterminate origin that covers both the cafe back door and the addition.

The building as it exists today has a concrete slab floor, with concrete perimeter footings. Interestingly, the concrete footings have a beveled top that extends about three inches past the walls. The walls are light steel frame, with the exception of the rear addition which is wood-framed. The flat roof is framed with a mixture of steel and wood, and the roof structure is exposed on the interior. Stout truss girders extend from the back wall, over the front wall as a cantilever to create the canopy, with smaller box beams (possibly steel or wood) over the former repair garage. The roof sheathing appears to be 12x boards or tongue and groove.

● **Fenestration**

In addition to minor modifications of size and massing, there were major changes made to fenestration. The original large roll-up garage door, which extended all the way to the corner of the pop-out, has been mostly filled in, probably by wood framing, with an 8 foot by 8 foot wood double patio door in the center. The original grouping of three windows in the southern wall of the repair garage also appears to have been replaced by a tripartite bay window consisting of two fixed windows flanking an operable window. The opening appears to be the same size as the original, but the window frame extends about foot from the wall. In contrast, the front windows that comprised the service station office appear to be intact but are partially painted opaque for signage. The north side office windows have been replaced by two smaller windows flanking a man-door. Also in the north wall of the building, one of the original exterior restroom doors has been replaced, and the single-sex restroom converted to a unisex restroom. The other restroom door has been filled in and replaced by a window; that restroom is currently used for storage.

There are no photos of the back of the building during its service station days, but a steel door near the northwest corner appears original. According to the owner of Malibu Brew, that door was originally for a utility closet, possibly electrical service. Another door, a wood single French door, is located in the middle of the wall next to the modern wood-frame addition. The door is certainly not original, but the opening may have held a man-door to the repair garage.

● **Finishes and Details**

The existing roof is flat, and was not accessible during the field assessment. During the time when the building was used as a service station, the roof featured a large tower sign with the Flying A logo, which was probably removed when Phillips 66 bought out Tidewater and converted all the Flying A stations to Phillips 66. The 1965 assessment notes that the roof was “metal.” The majority of the exterior walls are sheathed in thin steel panels which have been painted, which included the canopy sides and ceiling. The current building is white with black trim, while original Associated/Flying A color scheme was white with red and green trim. The non-original wall, in the garage door infill, is sheathed in painted shingles, while the rear addition is sheathed in painted T-111 siding.

The 1965 assessment lists a concrete floor. The existing floor is black and white vinyl tile. Interior walls in the former garage area are sheathed with a mixture of wooden beadboard and corrugated galvanized steel panels. The former office, now the kitchen, shows the exposed steel structure and exterior steel panels. Obviously, all of the former service station equipment has been removed.

5.0 SUMMARY AND CONCLUSIONS

The project parcel has played a significant role in the early commercial development of Atascadero, being associated with Kullgren's Golden Way Block, which was the first major commercial

development along the state highway (El Camino Real) and the site of the El Camino Links miniature gold course. The historical significance of the existing building, however, is much more subtle. The exact construction date of the building is not currently known despite extensive research, but was certainly in place by 1949 or possibly as early as 1948. It is highly unlikely that the building is any older than that.

During the first years of the building's existence, the parcel was still apparently owned by Gordon and Barbara Fox, while the building itself may have been either owned or leased by Associated Oil or Samuel L. Petersen. By 1953, Lorenzo P. Simpson had taken over the service station, and was running it as an Associated station. In 1965, both the parcel and the building were sold to Tidewater Oil Company, and in the following year sold to Phillips 66. The service station was operated by Lorenzo P. Simpson until 1961, when it was taken over by John H. Taylor who ran it until 1969.

Although 1948 does not seem very old for an historic building, it is technically old enough to be reviewed for historical significance. State guidelines for determining historical significance (36 CFR Part 60 and Calif. Pub. Res. Code, 5024.1, Title 14 CCR, Sect. 4852) indicate that a structure must be at least 50 years old and meet one of the following criteria in order to be considered a historic resource:

- A. It is associated with events that have made a significant contribution to the broad patterns of our history;
- B. It is associated with the lives of persons important in our past;
- C. It embodies the distinctive characteristics of a type, period, region, or method of construction, or represents the work of a master, or that possess high artistic values; or
- D. It has yielded or may be likely to yield information important in history.

Although the parcel may be associated with historic persons, the building is not. It is also not likely to yield new information important to history. The existing building therefore needs to be assessed only with respect to its architectural significance and its contributions to broad patterns of history.

The existing building is of marginal architectural significance. It is neither unique, of high quality craftsmanship, nor an outstanding example of its type. It does however, embody some of the distinctive characteristics of its type and period and has retained a fair amount of architectural integrity. Integrity is assessed based on seven criteria:

1. Location (is the building in its original location?)
2. Design (does the building have its original form and style?)
3. Setting (has the building retained important relationships with the landscape and other structures around it?)
4. Materials (does the building retain key original materials?)
5. Workmanship (methods of construction, finishes, and detailing)
6. Feeling (does the building still evoke a sense of the past)
7. Association (does the building retain a direct link to an important person or event)

It is clear that the building has retained integrity of location, setting, and feeling. It has retained moderate integrity of design, materials and workmanship, although this has been compromised by major alterations to fenestration and by the removal of the Flying A roof tower and all the service station equipment. Nevertheless, it still is recognizable as a post-war Streamline Moderne service station. It also directly associated with the post-war auto-centric commercial development of Atascadero along El Camino Real, and is the only remaining service station structures from the 1940s in Atascadero that has retained its classic Streamline Moderne style. All of the other remaining former service station structures are either of a more recent vintage, or have been altered to no longer resemble their original form.

6.0 MANAGEMENT RECOMMENDATIONS

The existing building has not been listed as a National Historic Landmark, or on the National Register of Historic Places. It has not been listed in the California Register of Historical Resources as a Registered Historic Landmark or as a Point of Historical interest. It is not included in any listing of historic resources within the City of Atascadero, which tends to focus on resources of the Atascadero Colony Period. The existing building does not appear to meet any of the criteria for eligibility on a national or state level.

The existing building has the potential to be considered a local historic resource due to its association with the post-war auto-centric commercial development of Atascadero along El Camino Real, and the unique retention of a classic Streamline Moderne style which embodies the distinctive characteristics of its type and period. The conversion of the building from an active service station to a retail building has taken a toll on some important aspects of the design, particularly with the removal of the Flying A tower, and the infill of the repair garage doors, but it is still more intact than any other service station form the period in Atascadero.

In general, when dealing with historic structures, there are six possible management recommendations:

1. Preservation in place, with structure left in its current condition.
2. Preservation in place, with appropriate restoration.
3. Preservation in place with adaptive re-use. Adaptive re-use could involve anything from minor modernization to extensive architectural modification, or even incorporation into another structure.
4. Removal of the building to another location, followed by preservation, restoration, or adaptive re-use.
5. Demolition following documentation to California Register of Historic Resources standards.
6. Demolition with no further mitigation.

Because the existing building has the possibility to be locally significant, the final possible alternative, demolition with no further mitigation is NOT recommended. The first alternative, preservation in place in current condition, is also NOT recommended since the building has already had enough alteration to impact its integrity. Options 2 and 3, restoration or adaptive re-use in place, also do not appear to be viable when considering the best and highest use of the land for the City of Atascadero.

As a result, two alternatives remain: relocation of the building, or demolition following documentation. From an idealistic point of view, relocation would appear to be the perfect solution, with a possible site at the nearby Atascadero Historical Society property. Realistically, the effort and expense in moving and restoring the building may not be warranted for level of significance of this building. Therefore, it is my considered opinion that the Malibu Brew building at 6490 El Camino Real may be demolished following recordation and documentation as mitigation. Recordation and documentation should be produced according to the California Register of Historic Resources. Required recordation and documentation shall include:

- Completed State of California Form DPR 523B
- Black and white large-scale photographs of exterior and interior, with scale and caption.
- Copies of selected historic photographs
- Written description and history of construction and use of the structure (this report will suffice).

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PERSONAL COMMUNICATIONS

Lon Allen	Past Historian Atascadero Historical Society 6600 Lewis Avenue Atascadero, CA 93422 (805) 466-8341
Marie Allen	Title Officer First American Title Company 899 Pacific Street San Luis Obispo, CA 93401 (805) 786-2025
Bob Wilkins	PO Box 1064 Atascadero, CA 93423 (805) 466-0730

**ATTACHMENT 4: DRAFT PC RESOLUTION 2017-B – MASTER PLAN OF
DEVELOPMENT**

DRAFT PC RESOLUTION 2017-B

**RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF
ATASCADERO, CALIFORNIA, APPROVING A MASTER PLAN OF
DEVELOPMENT FOR THE LA PLAZA DEVELOPMENT**

**PLN 2017-1649
LA PLAZA DOWNTOWN REDEVELOPMENT
Z3 LLC**

WHEREAS, an application has been received from Z3 LLC, Owner / Applicant (8189 San Dimas Lane, Atascadero, CA 93422), to consider Planning Application PLN 2017-1649, for a project consisting of a master plan of development (CUP), tentative parcel map, roadway abandonment, and certification of a Mitigated Negative Declaration ("MND") on a 1.83 acre site located on 6300 through 6500 El Camino Real (APN's 030-193-001,003,017,016, 031,030,033); and,

WHEREAS, the site's current General Plan Land Use Designation is Downtown (D); and

WHEREAS, the site's current Zoning District is Downtown Commercial (DC); and

WHEREAS, Atascadero Municipal Code § 9-2.110(a)(2) requires that multiple-family developments consisting of twelve (12) or more units, even if such development is listed as an allowed use in a particular zoning district, receive an approval of master plan of development in the form of a Conditional Use Permit (CUP); and

WHEREAS, Atascadero Municipal Code Section 9-4.113(b)(1) allows the Planning Commission modification of height limitations, provided the Commission first finds the project will not result in substantial detrimental effects on the enjoyment and use of adjoining properties and that the modified height will not exceed the lifesaving equipment capabilities of the Fire Department; and,

WHEREAS, Atascadero Municipal Code Section 9-15.1011 allows the Planning Commission the ability to modify signage standards, provided that the Planning Commission make the appropriate four (4) required findings; and,

WHEREAS, the Atascadero General Plan Policy 2.1.2 allows the Planning Commission to grant up to a fifteen percent (15%) density bonus for "exceptionally high quality architecture" through required findings; and,



WHEREAS, an Initial Study and proposed Mitigated Negative Declaration, Environmental Document No. 2017-0018 was prepared for the project and made available for public review in accordance with the requirements of the California Environmental Quality Act (CEQA); and

WHEREAS, the Planning Commission of the City of Atascadero, at a Public Hearing held on November 21, 2017, studied and considered PLN 2017-1649 to approve a master plan of development, after first studying and considering the proposed Mitigated Negative Declaration prepared for the project, and

NOW, THEREFORE BE IT RESOLVED by the Planning Commission of the City of Atascadero:

SECTION 1. The Planning Commission makes the following findings, determinations and approvals with respect to the proposed Master Plan of Development:

A. Findings for Approval of Height Waiver

1. **FINDING:** The proposed project will not result in substantial detrimental effects on the enjoyment and use of adjoining properties.

FACT: The proposed project does not result in substantial detrimental effects to the enjoyment of the use of adjoining properties, as the three-story is necessary to meet the proposed density, as required by the California Department of Housing and Community Development, and the proposed height is consistent with the vision established by the Downtown Revitalization Plan. Additionally, all proposed structures are well articulated and consist of high quality architectural design, similar to the existing surrounding development.

2. **FINDING:** The modified height will not exceed the lifesaving equipment capabilities of the Fire Department.

FACT: The Atascadero Emergency Services Department (Atascadero Fire Department) has reviewed the proposed project and determined that, based on building placement including setbacks, location of turnouts and fire lanes, the proposed project will not exceed the lifesaving equipment capabilities of the Department.

B. Findings for Sign Ordinance Design Exception

1. **FINDING:** The sign is consistent with the purposes set forth in Section 9-15.002;.

FACT: The proposed signage meets the intent established by the City's Signage Ordinance, in particular the proposed signage will minimize the adverse impacts of signage and maintain a positive City image, and reasonably allow for the identification of businesses within La Plaza.



2. **FINDING:** The opportunity to combine signs for more than one (1) use on a single sign structure has been considered.

FACT: The applicant is proposing the use of both a freeway oriented pylon sign as a monument sign. While it may be optimal to include just signage over a business, the applicant wishes to increase visibility and marketability of spaces for lease with additional signage oriented towards El Camino Real and Highway 101. Ensuring viability and marketability of these spaces is a priority for City Staff and the City Council.

3. **FINDING:** For freeway-oriented signs, the sign area and height are the minimum needed to achieve adequate visibility along the freeway due to highway ramp locations and grade differences.

FACT: The height of the freeway oriented sign is a similar height based on a balloon test completed for the existing 76 gasoline station on the other side of the freeway completed in 2013. The 7-foot clearance is tall enough that it will not be lost in the existing foliage that is adjacent to Highway 101.

4. **FINDING:** Conformance with all other applicable codes and ordinances of the City, including, but not limited to, the Zoning Ordinance, General Plan and its several elements, and the appearance review guidelines.

FACT: The proposed project is consistent with all applicable codes and ordinances, and meets the intent of the appearance review guidelines, and City Way Finding program.

C. Findings for Density Bonus

1. **FINDING:** The proposed project utilizes high quality materials and finishes through all four sides of the building(s) located on-site.

FACT: The proposed project is inspired by City landmarks including Historic City Hall and the Printery. The proposed project is an Italianate design and a visual appealing project. The proposed project utilizes similar materials found in both these historic structures and appropriately uses tower elements, pop-outs, and other architectural features to create a design that will become one of the center-pieces of Downtown Atascadero, complimenting Historic City Hall and the Sunken Gardens.

2. **FINDING:** The proposed project utilizes reduction of massing and other techniques to reduce appearance of bulk, and density.

FACT: The proposed project utilizes materials, pop-outs, decking, and other features to reduce bulk and density.



3. **FINDING:** The proposed project implements the use of varied rooflines, pop-outs, architectural appendages and other elements that create visually appealing projects.

FACT: The proposed project includes tower elements, varied rooflines, columns, and the use of colonnades to create a visually appealing project within Downtown Atascadero.

4. **FINDING:** The proposed project utilizes architectural materials appropriate with the vernacular of the architectural theme.

FACT: The proposed project utilizes Italianate design similar to Historic City Hall and the Printery. The applicant has included the use of brick material, and Spanish tile in similar color schemes to that of City Hall to present a unified design theme for buildings near the Sunken Gardens.

5. **FINDING:** The proposed project incorporates significant contributions to the community such as a park, historic preservation, or road dedications.

FACT: The proposed project includes a significant contribution to the Community in the form a new pedestrian plaza that will be constructed within the right-of-way of Atascadero Avenue (Mall). This key pedestrian linkage was envisioned by the City's Vision Plan for Downtown. The applicant may be afforded credits for open space / parkland dedication in exchange for construction of the proposed project.

D. Findings for Approval of Master Plan of Development

1. **FINDING:** The proposed project or use is consistent with the General Plan.

FACT: The proposed project is consistent with the intention of vertical mixed-use in the Downtown. The proposed project meets the underlying zoning ordinance's prescribed density requirements for 20 units per acre and includes a mix of commercial, office, and residential uses.

2. **FINDING:** The proposed project or use satisfies all applicable provisions of the Title 9 of the Atascadero Municipal Code this title.

FACT: With adoption of findings made for the height waiver exception, and Density Bonus, the proposed project satisfied all applicable provisions of the Atascadero Municipal Code.

3. **FINDING:** The establishment, and subsequent operation or conduct of the use will not, because of the circumstances and conditions applied in the particular case, be detrimental to the health, safety or welfare of the general public or persons residing or working in the neighborhood of the use, or be detrimental or injurious to



property or improvements in the vicinity of the use.

FACT: The proposed project will not be detrimental to the health, safety or welfare of the general public or persons residing or working in the vicinity, as the proposed project includes a mix of uses including residential, non-residential uses such as retail, professional services, and tourist commercial uses that are consistent with the City's underlying zoning (DC), as well as consistent with the goals and policies of the City's General Plan and Housing Element. Residential uses will not involve hazardous waste handling.

4. **FINDING:** The proposed project or use will not be inconsistent with the character of the immediate neighborhood or contrary to its orderly development.

FACT: The proposed project is consistent with the immediate neighborhood as the proposed project is consistent with the adopted Downtown Revitalization Plan, and the Downtown Vision Plan.

5. **FINDING:** The proposed use or project will not generate a volume of traffic beyond the safe capacity of all roads providing access to the project, either existing or to be improved in conjunction with the project, or beyond the normal traffic volume of the surrounding neighborhood, that would result from full development in accordance with the land use element.

FACT: The proposed project will generate 1,304 average new daily trips on a new local roadway that intersects with El Camino Real, the City's primary north / south arterial. El Camino Real is a roadway facility that currently operates at an acceptable level of service, consistent with the City's adopted General Plan and will continue to operate at an acceptable level of service with the proposed project, as well as what is considered build-out for the General Plan. The proposed project has been conditioned to pay development impact fees for proposed increases in traffic on regional serving highways (US 101) and other transportation related impacts.

6. **FINDING:** The proposed project is in compliance with any pertinent City policy or criteria adopted by Ordinance or Resolution of the City Council.

FACT: The proposed project meets the City Council's Policy on vertical mixed-use processing and the project been conditioned to annex into the City's Community Facilities District to make the proposed project fiscally neutral. The proposed project meets the thresholds for high quality architectural design, landscaping, common open spaces. Therefore, the project, as proposed, is in compliance with pertinent City policies established by the City Council.

SECTION 2. Approval of Master Plan of Development. The Planning Commission of the City of Atascadero, in a regular session assembled on November 21, 2017, resolved to approve the Master Plan of Development subject to the following, on file in the Community



Development Office and incorporated herein by reference:

1. EXHIBIT A: Conditions of approval / Mitigation Monitoring Program
2. EXHIBIT B: Master Plan of Development / Site Plan
3. EXHIBIT C: Landscape Plan
4. EXHIBIT D: Elevations / Floor Plan / Sections
5. EXHIBIT E: Colors and Materials Board
6. EXHIBIT F: Signage
7. EXHIBIT G: Grading and Drainage Plan
8. EXHIBIT H: Utility Plan
9. EXHIBIT I: Proposed Plaza Improvements

BE IT FURTHER RESOLVED, that a copy of this Resolution be delivered forthwith by the Planning Commission Secretary to the City Council of the City of Atascadero.

On motion by Commissioner _____ and seconded by Commissioner ____ the foregoing resolution is hereby adopted in its entirety by the following roll call vote:

AYES:	0
NOES:	0
ABSTAIN:	0
ABSENT:	0
ADOPTED:	

CITY OF ATASCADERO, CA

Duane Anderson
Planning Commission Chairperson

Attest:

Phil Dunsmore
Planning Commission Secretary



Exhibit A
Conditions of Approval
City of Atascadero



PLN 2017-1649
LA PLAZA REDEVELOPMENT
6500 EL CAMINO REAL
PARENT APN 030-193-001,003,017,016, 031,030,033

The following conditions of approval apply to the project referenced above. The conditions of approval are grouped under specific headings that relate to the timing of required compliance. Additional language within a condition may further define the timing of the required compliance.

A. The following conditions shall be satisfied PRIOR TO THE RECORDATION OF A FINAL MAP, or at the time specified in the condition.

1. All Conditions, no matter what timing is specified in the above header, shall be completed that are attached as conditions of approval in PC Resolution 2017-D for Tentative Parcel Map AT 17-0088. PLN
2. For the purposes of development impact fee collection, impact fees shall be based on the date of building permit submittal, consistent with the California Subdivision Map Act. PLN

B. The following conditions shall be satisfied prior to the issuance of the first of any DEMOLITION PERMIT, BUILDING PERMIT, SUBDIVISION IMPROVEMENT, or at the time specified in the condition.

3. A landscape and irrigation plan shall be approved prior to the issuance of building permits and included as part of site improvement plan consistent with EXHIBIT C, with the exception of the proposed public plaza located within the right-of-way of Atascadero Avenue (Mall), and must include the following: PLN
 - All exterior meters, trash storage areas, air conditioning units and mechanical equipment shall be screened with landscape material and/or architecturally compatible enclosures.
 - London Plane Sycamore or similar trees with a minimum size of 24 inch box shall be provided along El Camino Real at a maximum spacing of 30 feet on center (tree variety may be adjusted upon approval of Community Development Director). Groupings of additional accent trees may be allowed to accommodate sight-distance, topographical features, subject to the approval of the Community Development Director and City Engineer.
4. The proposed public plaza landscape plan and design within the Atascadero Avenue (Mall) shall be approved by the Community Development Director prior to the commencement of 50% design package interval. PLN



5. Atascadero Avenue (Mall) paved area shall terminate approximately 50 feet north east as shown in Exhibit I. A turning radius may be required and approved by the City Engineer prior to approval of public improvement plans for modification of Atascadero Avenue (Mall) right-of-way. PWD
6. Fire hydrant locations shall be to the satisfaction of the City Fire Marshall. FD
7. Properties and/or areas that are managed or owned by a Home Owners Association, or other such private entity shall be metered separately, as required by the Community Development Director. PWD
8. Ground monument signage fronting El Camino Real shall be limited to a total of (2) monument signs and shall be consistent with Sign standards shown in Table 15-2 for ground monument signage with the following exceptions:
- Maximum of six (6) feet in total height
 - Total of 60 square feet of signage allowed for each side
 - Ground monument signage may be permitted directly adjacent to the back of sidewalk
 - Ground monument signage shall be located no closer than 200-feet from each other
9. A total of four (4) Building Name signs shall be permitted within the development with the name “La Plaza”. Size shall be proportional to wall façade or placement on tower element.
10. A freeway oriented pylon sign shall be permitted on-site with the maximum height of 75-feet. Additional height may be granted by the Design Review Committee. Signage shall include the City’s Logo and “Downtown Atascadero” on the faces of the sign. The City’s Logo and “Downtown Atascadero” shall not count against the maximum signage area. Maximum signage area shall be determined by the Community Development Director or their designee.
11. Maximum project density shall not exceed 40 dwelling units. Any additional dwelling units shall be approved by the Planning Commission as a minor conditional use permit or its equivalent.
12. Building heights shall not exceed the following:
- 45-Feet maximum for occupied spaces;
 - 70-Feet maximum for un-occupied spaces.
- Additional height exception may be granted by the Design Review Committee.
13. The El Camino Real pedestrian entry/exit doors serving Building B shall not swing into the public right-of-way more than one foot unless an encroachment permit is granted by the City. As proposed, the doors shall be recessed into the building. A detail shall be included in the submitted building permits prior to issuance to the satisfaction of the building official or City Engineer.
14. The proposed waterfall structure in front of Building A shall be located entirely within the subject property and shall not encroach into the public right-of-way.



This shall be shown on submitted on-site improvement plans and to be approved by the City Engineer.

15. The developer may enter into a development agreement (DA) memorandum of understanding (MOU), or other agreement with the City to receive development impact credits for improvements to public rights-of-way. Impact Fee credits may include, but are not limited to:
- Traffic Impact Fee, Schedule 5.3
 - Parkland, Schedule 10.1
 - Open Space Acquisition, Schedule 10.1
 - Any Additional Development Impact Fees as determined by the City Manager and the City Attorney with approval of the City Council

C. The following conditions shall be IMPLEMENTED DURING THE CONSTRUCTION PHASE OF THE PROJECT.

16. All site work, grading, and site improvements shall be in substantial conformance with the Master Plan of Development as shown in any of the enclosed exhibits. PLN
17. Native tree protection shall be installed and up at all times during construction related activities per the tree protection plan. PLN
18. Pedestrian ramps on El Camino Real shall be constructed on each side of the street type entrance that provides vehicular access to Shores' property (the existing gas station property) south of the subject property.
19. The applicant shall relocate the (E) sewer main that extends under proposed Building "B", to the satisfaction of the City Engineer. An easement over the re-routed sewer main shall be dedicated to the public, to the satisfaction of the City Engineer.
20. Construction of the new driveway approach, northwesterly of Building A, requires removal of the existing driveway approach serving the adjacent northwesterly property. Vehicular access to this property shall be combined with the subject property.
21. The applicant shall coordinate with the Regional Transit Authority regarding the relocation and modification to the existing bus shelter and bus stop. Final location shall be approved by the Regional Transit Authority and the City Engineer.

D. The following conditions shall be met prior to the RELEASE OF UTILITIES, FINAL INSPECTION, OR ISSUANCE OF A CERTIFICATE OF OCCUPANCY, whichever occurs first.

22. All landscaping is to be installed prior to final inspection of development within Parcel Map AT 17-0088, PLN



E. The following conditions shall be complied with AT ALL TIMES that the use permitted by this planning application occupies the premise, and shall be applied to the project in perpetuity until such time that the use is extinguished.

23. The entitlement described at the location per this resolution is determined to be vested with the property upon issuance of a building permit. PLN
24. Approval of this entitlement shall be final and effective consistent with Atascadero Municipal Code (AMC) section 9-1.111 seq. et. al. PLN
25. Project construction must be in accordance with provided Exhibit(s), adopted with this Resolution. Changes to architecture, landscaping design, and non-substantive subdivision design may be approved by the Community Development Director or the Design Review Committee (DRC). PLN
26. In accordance with the Atascadero Municipal Code section 9-8.105, any violation of any of the conditions of approval is unlawful and may be cause for revocation of this entitlement and subject the applicant and/or future property owners to the penalties set forth in the Atascadero Municipal Code, as well as any other available legal remedies. PLN
27. The applicant shall agree to indemnify and defend at his/her sole expense any action brought against the City, its present or former agents, officers, or employees because of the issuance of this approval, or in any way relating to the implementation thereof, or in the alternative, to relinquish such approval. The applicant shall reimburse the City, its agents, officers, or employees, for any court costs and attorney's fees which the City, its agents, officers or employees may be required by a court to pay as a result of such action. The City may, at its sole discretion, participate at its own expense in the defense of any such action, but such participation shall not relieve applicant of his/her obligations under this condition. CM
28. Should the described use be abandoned or extinguished, the property may be used and / or developed with any use allowed by the underlying zoning district. PLN
29. Discharges to the public storm drain system are subject to review under the City's MS4 State Permit and the boundaries established by the Regional Water Quality Control Board for discharges to waters of the United States. Illicit discharges shall not be approved and shall be eliminated where known to exist or are identified. Certain non-storm water discharges may not be considered illicit where it can be shown that these waters are not contaminated. Uncontaminated spring water, pumped ground water, and water from crawl space pumps are not considered illicit discharges and are therefore not prohibited from discharging to the storm drain system. The applicant shall provide verification to the satisfaction of the City Engineer that the proposed discharge waters are not contaminated. PWD



F. The following are mitigation measures that mitigate adverse environmental effects identified in the prepared Environmental Document. Any change in these measures may affect the validity of the adopted Environmental Document, and a new or amended Environmental Document may be required.

MITIGATION MEASURE	TIMING
Aesthetics	
AES-1 At the time of building permit submittal of the proposed project, applicant must submit a photometric plan showing locations of proposed on-sight lighting. Prior to final occupancy, City Staff and the applicant shall meet on-site and review lights at dusk condition to ensure off-site light spillage and glare are limited.	Prior to Building Permit Issuance
AES-2 Any luminaire pole height shall not exceed 14-feet in height to minimize off-site light spillage for consistency with the Atascadero Municipal Code.	Prior to Building Permit Issuance
AES-3 Limit intensity to up to 3.0 foot candles at ingress/egress, and otherwise 0.6 foot candle minimum to 1.0 maximum in parking areas and/or for street lighting, bollards, etc to be reviewed at the time of building final.	Prior to Building Permit Final
Air Quality	
AQ-1 Demolition activities can have potential negative air quality impacts, including issues surrounding proper handling, abatement, and disposal of asbestos containing material (ACM). Asbestos containing materials could be encountered during the demolition or remodeling of existing structures or the disturbance, demolition, or relocation of above or below ground utility pipes/pipelines (e.g., transit pipes or insulation on pipes). If this project will include any of these activities, then it may be subject to various regulatory jurisdictions, including the requirements stipulated in the National Emission Standard for Hazardous Air Pollutants. These requirements include, but are not limited to: 1) written notification, within at least 10 business days of activities commencing, to the APCD, 2) asbestos survey conducted by a Certified Asbestos Consultant, and, 3) applicable removal and disposal requirements of identified ACM. Please contact the APCD Engineering & Compliance Division at (805) 781-5912 for further information or go to slocleanair.org/rules-regulations/asbestos.php for further information. To obtain a Notification of Demolition and Renovation form go to the "Other Forms" section of slocleanair.org/library/download-forms.php .	Prior to Building Permit Issuance



	MITIGATION MEASURE	TIMING
AQ-2	<p>Based on the information provided, San Luis Obispo APCD is unsure of the types of equipment that may be present during the project's construction phase. Portable equipment, 50 horsepower (hp) or greater, used during construction activities may require California statewide portable equipment registration (issued by the California Air Resources Board) or an APCD permit.</p> <p>The following list is provided as a guide to equipment and operations that may have permitting requirements, but should not be viewed as exclusive. For a more detailed listing, refer to the Technical Appendices, page 4-4, in the APCD's 2012 CEQA Handbook.</p> <ul style="list-style-type: none">• Power screens, conveyors, diesel engines, and/or crushers;• Portable generators and equipment with engines that are 50 hp or greater; and,• Electrical generation plants or the use of standby generator; <p>Internal combustion engines;</p> <ul style="list-style-type: none">• Rock and pavement crushing;• Unconfined abrasive blasting operations;• Tub grinders;• Trommel screens; and,• Portable plants (e.g. aggregate plant, asphalt batch plant, concrete batch plant, etc). <p>To minimize potential delays, prior to the start of the project, please contact the APCD Engineering & Compliance Division at (805) 781-5912 for specific information regarding permitting requirements.</p>	During Construction
AQ-3	<p>Effective February 25, 2000, the APCD prohibited developmental burning of vegetative material within San Luis Obispo County. If you have any questions regarding these requirements, contact the APCD Engineering & Compliance Division at (805) 781-5912</p>	During Construction
AQ-4	<p>Construction activities can generate fugitive dust, which could be a nuisance to local residents and businesses in close proximity to the proposed construction site. Projects with grading areas that are greater than 4-acres or are within 1,000 feet of any sensitive receptor shall implement the following mitigation measures to manage fugitive dust emissions such that they do not exceed the APCD's 20% opacity limit (APCD Rule 401) or prompt nuisance violations (APCD Rule 402):</p> <ul style="list-style-type: none">n. Reduce the amount of the disturbed area where possible; and,o. Use of water trucks or sprinkler systems in sufficient quantities to prevent airborne dust from leaving the site	During Construction



MITIGATION MEASURE

TIMING

and from exceeding the APCD's limit of 20% opacity for greater than 3 minutes in any 60 minute period. Increased watering frequency would be required whenever wind speeds exceed 15 mph. Reclaimed (non-potable) water should be used whenever possible. **Please note that since water use is a concern due to drought conditions, the contractor or builder shall consider the use of an APCD-approved dust suppressant where feasible to reduce the amount of water used for dust control.** For a list of suppressants, see Section 4.3 of the CEQA Air Quality Handbook.

- p. All dirt stock pile areas should be sprayed daily and covered with tarps or other dust barriers as needed;
- q. Permanent dust control measures identified in the approved project revegetation and landscape plans should be implemented as soon as possible following completion of any soil disturbing activities;
- r. Exposed ground areas that are planned to be reworked at dates greater than one month after initial grading should be sown with a fast germinating, non-invasive, grass seed and watered until vegetation is established;
- s. All disturbed soil areas not subject to revegetation should be stabilized using approved chemical soil binders, jute netting, or other methods approved in advance by the APCD;
- t. All roadways, driveways, sidewalks, etc. to be paved should be completed as soon as possible. In addition, building pads should be laid as soon as possible after grading unless seeding or soil binders are used;
- u. Vehicle speed for all construction vehicles shall not exceed 15 mph on any unpaved surface at the construction site;
- v. All trucks hauling dirt, sand, soil, or other loose materials are to be covered or should maintain at least two feet of freeboard (minimum vertical distance between top of load and top of trailer) in accordance with CVC Section 23114;
- w. Track-Out" is defined as sand or soil that adheres to and/or agglomerates on the exterior surfaces of motor vehicles and/or equipment (including tires) that may then fall onto any highway or street as described in California Vehicle Code Section 23113 and California Water Code 13304. To prevent 'track out', designate access points and require all employees, subcontractors, and others to use them. Install and operate a 'track-out prevention device' where vehicles enter and exit unpaved roads onto paved streets. The 'track-out prevention device' can be any device or combination of devices that are effective at preventing track out, located at the point of intersection of



MITIGATION MEASURE

TIMING

an unpaved area and a paved road. Rumble strips or steel plate devices need periodic cleaning to be effective. If paved roadways accumulate tracked out soils, the track-out prevention device may need to be modified;

- x. Sweep streets at the end of each day if visible soil material is carried onto adjacent paved roads. Water sweepers shall be used with reclaimed water should be used where feasible. Roads shall be pre-wetted prior to sweeping when feasible;
- y. All PM₁₀ mitigation measures required should be shown on grading and building plans; and,
- z. The contractor or builder shall designate a person or persons to monitor the fugitive dust emissions and enhance the implementation of the measures as necessary to minimize dust complaints, reduce visible emissions below the APCD's limit of 20% opacity for greater than 3 minutes in any 60 minute period. Their duties shall include holidays and weekend periods when work may not be in progress. The name and telephone number of such persons shall be provided to the APCD Engineering & Compliance Division prior to the start of any grading, earthwork or demolition.

AQ-5

This project is in close proximity to nearby sensitive receptors. Projects that will have diesel powered construction activity in close proximity to any sensitive receptor shall implement the following mitigation measures to ensure that public health benefits are realized by reducing toxic risk from diesel emissions:

During
Construction

To help reduce sensitive receptor emissions impact of diesel vehicles and equipment used to construct the project, the applicant shall implement the following idling control techniques:

- 3. California Diesel Idling Regulations
 - e. **On-road diesel vehicles** shall comply with Section 2485 of Title 13 of the California Code of Regulations. This regulation limits idling from diesel-fueled commercial motor vehicles with gross vehicular weight ratings of more than 10,000 pounds and licensed for operation on highways. It applies to California and non-California based vehicles. In general, the regulation specifies that drivers of said vehicles:
 - 3. Shall not idle the vehicle's primary diesel engine for greater than 5 minutes at any location, except as noted in Subsection (d) of the regulation; and,
 - 4. Shall not operate a diesel-fueled auxiliary power system (APS) to power a heater, air conditioner, or any ancillary equipment on that vehicle during



MITIGATION MEASURE

TIMING

- sleeping or resting in a sleeper berth for greater than 5.0 minutes at any location when within 1,000 feet of a restricted area, except as noted in Subsection (d) of the regulation.
- f. **Off-road diesel equipment** shall comply with the 5-minute idling restriction identified in Section 2449(d)(2) of the California Air Resources Board’s In-Use Off-Road Diesel regulation.
- g. Signs must be posted in the designated queuing areas and job sites to remind drivers and operators of the state’s 5-minute idling limit.
- h. The specific requirements and exceptions in the regulations can be reviewed at the following web sites: www.arb.ca.gov/msprog/truck-idling/2485.pdf and www.arb.ca.gov/regact/2007/ordiesl07/frooal.pdf.

AND/OR

- 4. Diesel Idling Restrictions Near Sensitive Receptors
 In addition to the state required diesel idling requirements, the project applicant shall comply with these more restrictive requirements to minimize impacts to nearby sensitive receptors:
 - e. Staging and queuing areas shall not be located within 1,000 feet of sensitive receptors;
 - f. Diesel idling within 1,000 feet of sensitive receptors shall not be permitted;
 - g. Use of alternative fueled equipment is recommended; and
 - h. Signs that specify the no idling areas must be posted and enforced at the site.

AQ-6	Exterior architectural coating shall be limited to Volatile Organic Compound (VOC) of 50 grams per liter maximum to the extent feasible.	During Construction
AQ-7	Interior architectural coating shall be limited to Volatile Organic Compound (VOC) of 0 grams per liter maximum to the extent feasible within residential and non-residential spaces.	During Construction
AQ-8	Should hydrocarbon contaminated soil be encountered during construction activities, the APCD must be notified as soon as possible and no later than 48 hours after affected material is discovered to determine if an APCD Permit will be required. In addition, the following measures shall be implemented immediately after contaminated soil is discovered: <ul style="list-style-type: none"> • Covers on storage piles shall be maintained in place at all times in areas not actively involved in soil addition or 	Dirong Construction



MITIGATION MEASURE

TIMING

- removal;
- Contaminated soil shall be covered with at least six inches of packed uncontaminated soil or other TPH –non-permeable barrier such as plastic tarp. No headspace shall be allowed where vapors could accumulate;
- Covered piles shall be designed in such a way to eliminate erosion due to wind or water. No openings in the covers are permitted;
- The air quality impacts from the excavation and haul trips associated with removing the contaminated soil must be evaluated and mitigated if total emissions exceed the APCD’s construction phase thresholds;
- During soil excavation, odors shall not be evident to such a degree as to cause a public nuisance; and,
- Clean soil must be segregated from contaminated soil.

The notification and permitting determination requirements shall be directed to the APCD Engineering & Compliance Division at (805) 781-5912.

Green House Gas Emissions

GHG-1	The proposed project shall include a minimum of four (4) electric vehicle (EV) chargers within the proposed parking lot. Vehicle chargers shall be counted as parking spaces for the purposes of parking calculations.	Prior to Building Permit Final
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Cultural Resources

CR-1	<p>Recordation and documentation shall be produced according to the California Register of Historic Resources prior to issuance of demolition permits for both 6452 and 6490 El Camino Real. Documentation shall include the following:</p> <ul style="list-style-type: none"> • Completion of State of California Form DPR 523B; • Large scale color photographs of exterior and interior with scale and caption; • Large scale site plan and floor plan of existing building(s) with legend and callouts of location photographs taken; • Copies of selected historic photographs; • Written description of construction and use of the structures; and, • Digital copies of all materials submitted in PDF format. 	Prior to Demo Permit Issuance
CR-2	<p>In the event that human remains are discovered on the property, all work on the project shall stop and the Atascadero Police Department and the County Coroner shall be contacted immediately. The Atascadero Community Development Director shall be notified. If the human remains are identified as being native American, the California Native American Heritage Commission (NAHC) shall be contacted at (916) 653-4082 within</p>	During Construction



MITIGATION MEASURE	TIMING
<p>24 hours. A representative from both the Chumash Tribe and the Salinan Tribe shall be notified during the excavation of any remains.</p>	
<p>Noise</p> <p>NO-1</p>	<p>Soffit vents, eave vents, dormer vents and other wall and roof penetrations shall be on the walls and roofs facing away from the noise source wherever possible.</p> <p>Prior to Building Permit Issuance</p>
<p>NO-2</p>	<p>The walls of habitable spaces and office space on second and third stories nearest the noise source shall have wall construction with an S.T.C. (Sound Transmission Class) rating of 30 or greater. For instance, stucco exterior or equivalent on 2" x 4" stud walls with minimum R-13 batt insulation and two layers of ½" gypsum board on the interior will provide an S.T.C. rating of 30 or greater along these walls.</p> <p>Prior to Building Permit Issuance</p>
<p>NO-3</p>	<p>Common acoustic leaks, such as electrical outlets, pipes, vents, ducts, flues, and other breaks in the integrity of the wall, ceiling or roof construction on the side of the dwellings nearest transportation noise sources shall receive special attention during construction. All construction openings and joints on the walls on the noise facing side of the site shall be insulated, sealed and caulked with a resilient, non-hardening, acoustical caulking material. All such openings and joints shall be airtight to maintain sound isolation.</p> <p>Prior to Building Permit Issuance</p>
<p>NO-4</p>	<p>To meet the interior LDN 45 dBA requirements, windows for habitable spaces on the second and third floors of affected units facing the noise source shall be of double-glazed construction with one light of laminated glass, and installed in accordance with the recommendations of the manufacturer. The windows shall have full gaskets, with an S.T.C. rating of 30 or better, as determined in testing by an accredited acoustical laboratory.</p> <p>Prior to Building Permit Issuance</p>
<p>Transportation & Traffic</p>	
<p>TP-1</p>	<p>The applicant or developer must relocate the existing pedestrian crossing at the midblock crossing at the intersection of East Mall and El Camino Real to the signalized intersection of West Mall and El Camino Real. The applicant shall re-stripe and provide conduit for a new pedestrian crossing signal adjacent to the project site. The proposed improvement may be eligible for TIF credits based on the adopted Capital Improvement Plans at the time of building permit issuance.</p> <p>Prior to release final sign off of off-site improvements.</p>
<p>TP-2</p>	<p>The applicant shall work with City Staff and the Regional Transit Authority to relocated the adjacent southbound RTA stop that currently exists near the intersection of Entrada and El Camino Real. This includes relocation or re-construction of a bus shelter,</p> <p>Prior to release final sign off of off-site improvements.</p>



MITIGATION MEASURE

TIMING

as deemed appropriate by the City Engineer

Utilities Services

- | | | |
|------|--|--|
| UT-1 | The proposed project must pay all applicable sewer connection fees at the time of building permit issuance. A fee credit may be applicable for all existing fixtures and connections listed in demolition permits for buildings removed. | Prior to Building Permit Issuance |
| UT-2 | The applicant shall be responsible for providing fair share impact fees for wastewater in effect at the time of building permit issuance. | Prior to Building Permit Issuance |
| UT-3 | The developer must include trash and recycle receptacles near public entries and a minimum of (3) trash and recycle receptacles within the proposed plaza to be dispersed throughout the length of the plaza. Receptacle design shall be approved by Staff and included on landscaping plans at the time of building permit submittal. | Prior to release final sign off of on-site improvements. |

****END CONDITIONS****



EXHIBIT C: LANDSCAPE PLAN

- LEGEND**
- 1 RESTROOM COLLECTION
 - 2 OUTDOOR DINING
 - 3 PERMEABLE PAVES
 - 4 EXISTING VEGETATIVE BUFFER (CALTRANS)
 - 5 12 X 12 INTERACTIVE CHESS BOARD
 - 6 MOUNTAIN FOLI / BERRY FEATURE
 - 7 WATER WALL AND PLANTER
 - 8 SAVED PLANTER 24X24X18"
 - 9 TRASH ENCLOSURE
 - 10 DECORATIVE CONCRETE PAVING
 - 11 PAVED / ACCENT BANDS
 - 12 CONTAINERS GRASSES
 - 13 STREET TREES (SEE TREE SPECIM)
 - 14 FILTER STRIPS
 - 15 CONCRETE PIANO POLE / POSTRALL
 - 16 COLUMNING, SEE ARCH PLAN
 - 17 SWALE, SEE CIVIL PLAN
 - 18 FLOWERING TREES WITH TWINKLE LIGHTING
 - 19 RELOCATED CURB
 - 20 EQ. LEANS STATUE + HISTORICAL MOUNTAIN
 - 21 MIXED PLANTING BUFFER AT PARKING AREAS
 - 22 CELL TOWER BASEMENT
 - 23 SQUARDES
 - 24 STEEL TREE GRATES
 - 25 DECK-OFF
 - 26 DROUGHT TOLERANT LANDSCAPING
 - 27 CORNICLE COURT

METHOD OF IRRIGATION

THE IRRIGATION SYSTEM WILL UTILIZE THE FOLLOWING METHODS:
 CORE AND BUBBLE IRRIGATION WILL BE USED TO APPLY WATER ACCORDANTLY TO THE PLANT FOOTPRINT AT A RATE THAT CAN INFLUENCE TO IMPROVE IRRIGATION EFFICIENCY.
 LOW SPRINKLER HEADS WILL BE USED WHERE HEADED TO APPLY WATER UNIFORMLY AND SLOWLY.
 OVER-SPRAY AND EVAPORATION WILL BE MINIMIZED AND WATCHED PRECIPITATION RATE HOZZETS WILL BE USED WHEN EACH CONTROL VALVE AND CIRCUIT.
 A WEATHER BASED, SELF-ADJUSTING IRRIGATION CONTROL WITH A RAIN SHUT OFF DEVICE WILL BE INSTALLED. THE IRRIGATION SYSTEM WILL BE CONTROLLED BY A TIME CLOCK WITH THE ABILITY TO ADJUST RUNTIMES BY DAY, COLLECTED FROM RAIN AND ET DEMONDS.
 THE WATERING SCHEDULE WILL BE BASED UPON PLANT NEEDS, SOIL TYPE, SLOPE, AND SEASON. IRRIGATION WILL BE SCHEDULED TO AVOID WATERING DURING RAIN AND FREEZE EVENTS.

MWEO WATER CALCULATIONS

MAXIMUM ANNUAL APPLIED WATER ALLOWANCE:
 (MAXIMUM) 1,42,500 GALLONS, 191,000 UNITS

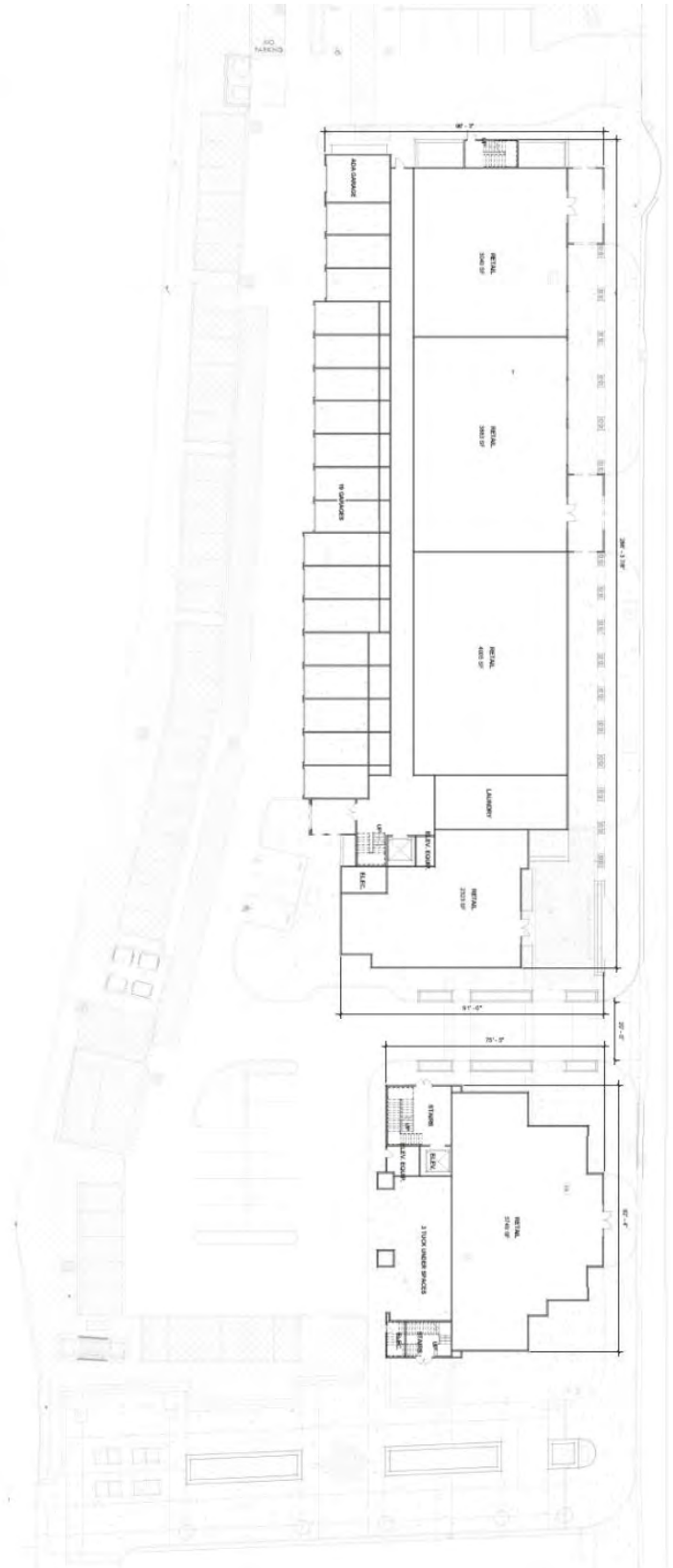
ESTIMATED TOTAL WATER USE:
 (ESTIM) 100,000 GALLONS, 142,500 UNITS

PROPOSED PROJECT IS USING 74.62% OF THE MAXIMUM WATER ALLOWANCE PER GOVERNMENT ORDER NO. 159-13

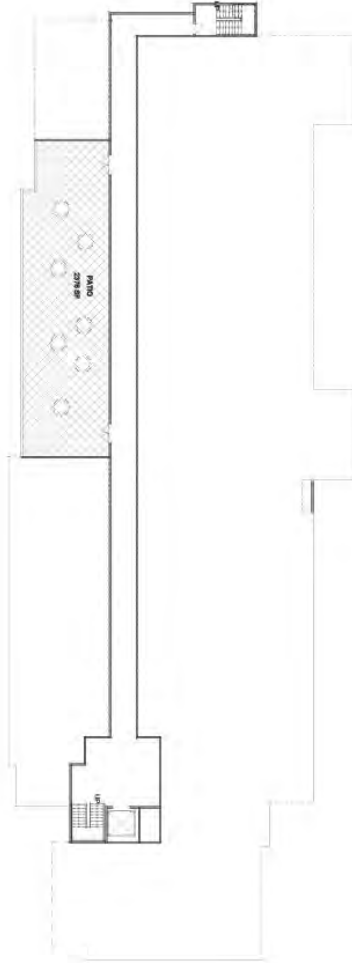


EXHIBIT D: Elevations / Floor Plan / Sections

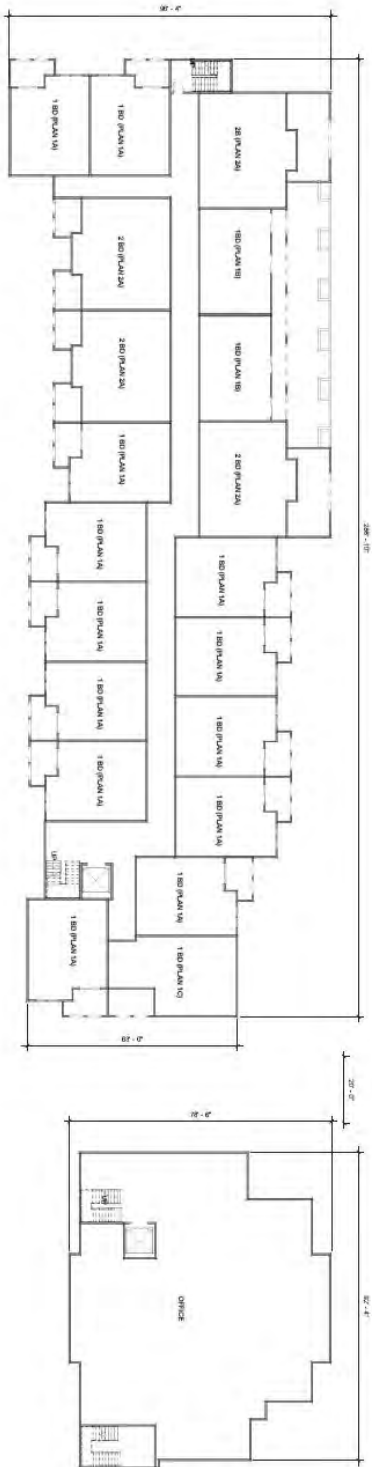
1 GROUND FLOOR PLAN
1/16" = 1'-0" (24 x 36 SHEET)



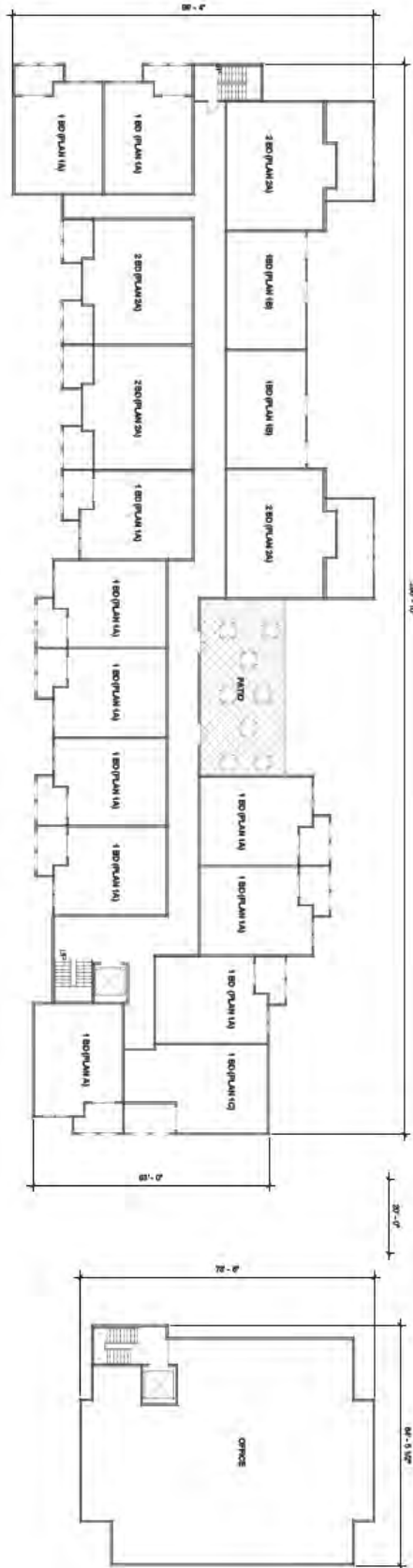
1 MID LEVEL AND EXTERIOR PATIO FLOOR PLAN
 1/16" = 1'-0" (24 X 36 SHEET)



2 SECOND FLOOR PLAN
 1/16" = 1'-0" (24 X 36 SHEET)



1 THIRD FLOOR PLAN
1/16" = 1'-0" (24 X 36 SHEET)





1 EAST ELEVATION
 1/16" = 1'-0" (24 x 36 SHEET)



BUILDING A



2 BUILDING A PERSPECTIVE



3 NORTH ELEVATION BUILDING A
 1/16" = 1'-0" (24 x 36 SHEET)



4 SOUTH ELEVATION BUILDING A
 1/16" = 1'-0" (24 x 36 SHEET)





1 WEST ELEVATION
 1/16" = 1'-0" (24 X 36 SHEET)

BUILDING A



BUILDING B



2 BUILDING B PERSPECTIVE

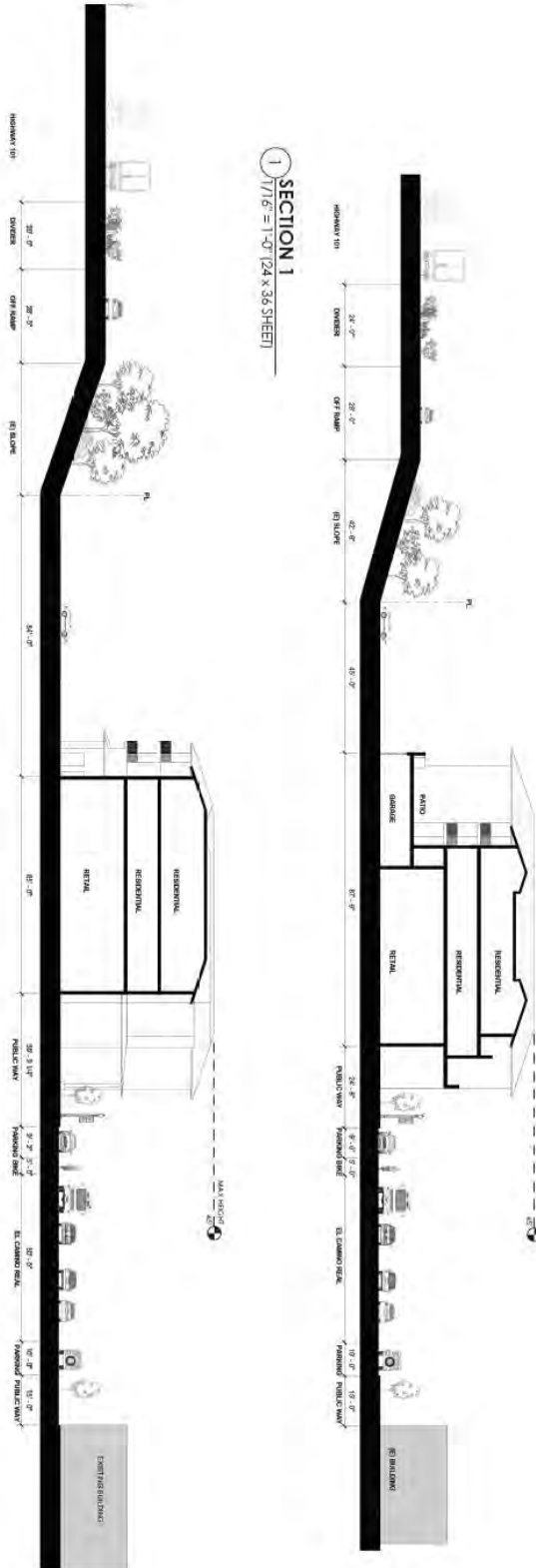


4 SOUTH ELEVATION BUILDING B
 1/16" = 1'-0" (24 X 36 SHEET)

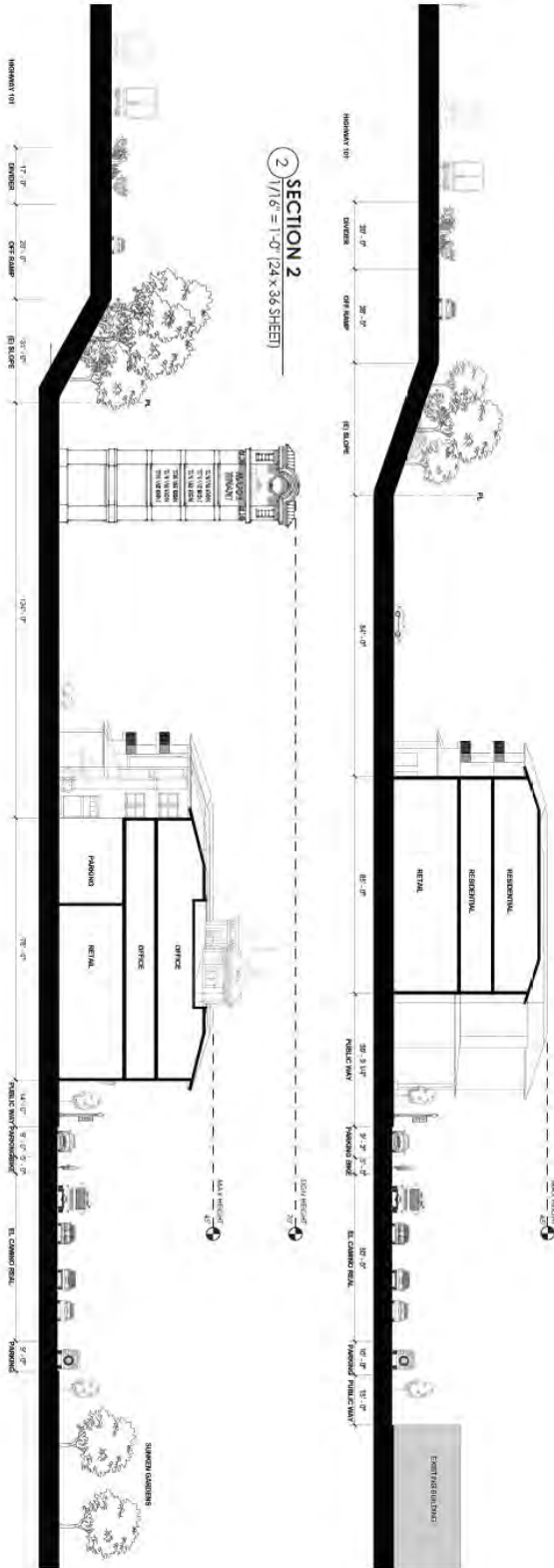


3 NORTH ELEVATION BUILDING B
 1/16" = 1'-0" (24 X 36 SHEET)

1 SECTION 1
 1/16" = 1'-0" (24 x 36 SHEET)



2 SECTION 2
 1/16" = 1'-0" (24 x 36 SHEET)



3 SECTION 3
 1/16" = 1'-0" (24 x 36 SHEET)

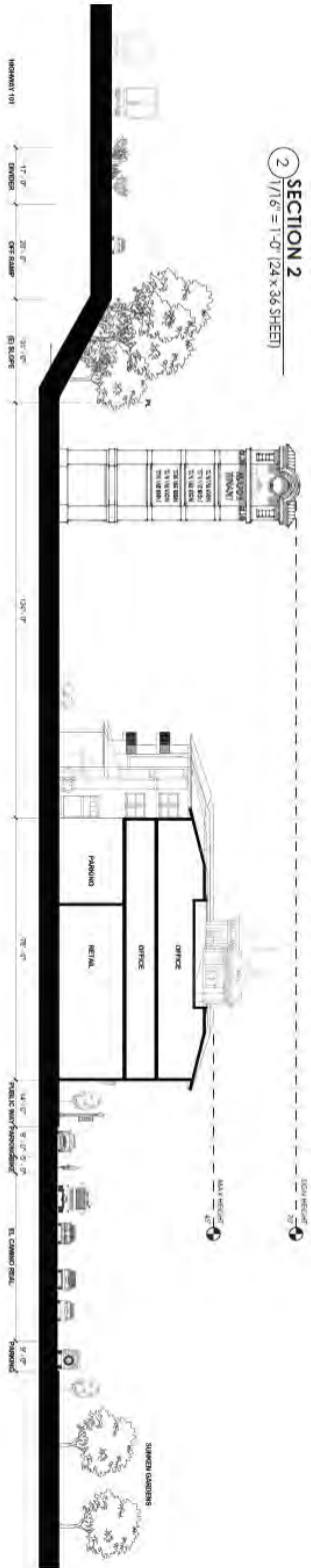


EXHIBIT E: COLORS AND MATERIALS BOARD

Eagle Roof Tile
 Capistrano SMC 8820
 Santa Clara Blend

Painted Clay Thin Brick
 Atascadero Dulce
 (same color as City Hall)

Precast GFRC Trim - Base
 Venetian-J, Ginger in Silverstone

Precast GFRC Concrete Panels
 Venetian-J, Ginger
 Sandalwood accents on Cornish base

Painted Stucco Accent Color
 Dunn Edwards DES376
 Looking Glass

Porcelain Tile Base Accent
 American Clean - Method
 MT05 Khaki Approach, polished

Painted Stucco Field Color
 Dunn Edwards DE6206
 Desert Suede

Painted Fassaia
 Dunn Edwards DE6105
 Weathered Leather

Painted Corbels & Rafter Tails
 Dunn Edwards DE5173
 Deep Coral

Aluminum Doors and Windows
 Medium Bronze



EXHIBIT F: SIGNAGE



Pylon Sign



**ATTACHMENT 5: DRAFT PC RESOLUTION 2017-C –
TENTATIVE PARCEL MAP
PLN 2017-1649**

DRAFT PC RESOLUTION 2017-C

**RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF
ATASCADERO, CALIFORNIA, APPROVING TENTATIVE PARCEL
MAP AT 17-0088**

**PLN 2017-1649
LA PLAZA DOWNTOWN REDEVELOPMENT
Z3 LLC**

WHEREAS, an application has been received from Z3 LLC, Owner / Applicant (8189 San Dimas Lane, Atascadero, CA 93422), to consider Planning Application PLN 2017-1647, for a project consisting of a master plan of development (CUP), Tentative Parcel Map (including condominiums), roadway abandonment, and certification of a Mitigated Negative Declaration ("MND") on a 1.83 acre site located on 6300 through 6500 El Camino Real (APN's 030-193-001,003,017,016, 031,030,033); and,

WHEREAS, the site's current General Plan Land Use Designation is Downtown (D); and

WHEREAS, the site's current Zoning District is Downtown Commercial (DC); and

WHEREAS, Atascadero Municipal Code § 9-2.110(a)(2) requires that multiple-family developments consisting of twelve (12) or more units, even if such development is listed as an allowed use in a particular zoning district, receive an approval of master plan of development in the form of a Conditional Use Permit (CUP); and

WHEREAS, Atascadero Municipal Code Section 9-4.113(b)(1) allows the Planning Commission modification of height limitations, provided the Commission first finds the project will not result in substantial detrimental effects on the enjoyment and use of adjoining properties and that the modified height will not exceed the lifesaving equipment capabilities of the Fire Department; and,

WHEREAS, Atascadero Municipal Code Section 9-15.1011 allows the Planning Commission the ability to modify signage standards, provided that the Planning Commission make the appropriate four (4) required findings; and,

WHEREAS, the Atascadero General Plan Policy 2.1.2 allows the Planning Commission to grant up to a fifteen percent (15%) density bonus for "exceptionally high quality architecture" through required findings; and,



WHEREAS, an Initial Study and proposed Mitigated Negative Declaration, Environmental Document No. 2017-0018 was prepared for the project and made available for public review in accordance with the requirements of the California Environmental Quality Act (CEQA); and

WHEREAS, the laws and regulations relating to the preparation and public notice of environmental documents, as set forth in the State and local guidelines for implementation of the California Environmental Quality Act (CEQA) have been adhered to; and

WHEREAS, a timely and properly noticed Public Hearing on the subject Tentative Parcel Map was held by the Planning Commission of the City of Atascadero at which hearing evidence, oral and documentary, was admitted on behalf of said Subdivision; and

WHEREAS, the Planning Commission of the City of Atascadero, at a duly noticed Public Hearing held on November 21, 2017, studied and considered Tentative Parcel Map (AT 17-0088) for PLN 2017-1649, after certifying Mitigated Negative Declaration Environmental Document No. 2017-0018, prepared for the project.

NOW, THEREFORE BE IT RESOLVED by the Planning Commission of the City of Atascadero:

SECTION 1. The Planning Commission makes the following findings, determinations and approvals with respect to the Tentative Parcel Map:

A. Findings for Approval of Tentative Parcel Map

2. **FINDING:** The proposed subdivision, together with the provisions for its design and improvement, is consistent with the General Plan and the proposed Zone Text Change Amendment (Government Code§§ 66473.5 and 66474(a) and (b)).

FACT: The proposed subdivision has been designed and includes proposed improvements that are consistent with the City’s adopted General Plan, in addition to the requirements that have been considered for the adoption of the Master Plan of Development.

3. **FINDING:** The site is physically suitable for the type of development (Government Code§ 66474(c)).

FACT: The proposed site is suitable for this type of development as the grades are gently sloping. Based on the existing grades and proposed grading plan, the proposed subdivision contains a number of parcel and ultimately a number of residential air space condominiums and non-residential air space condominiums on Parcels 2 and 3 that are consistent with the density of the underlying Downtown Commercial (DC) zoning district.

4. **FINDING:** The site is physically suitable for the proposed density of



development (Government Code § 66474(d)).

FACT: The site is a redevelopment of formerly developed commercial properties within the City's Downtown Core. The proposed development is consistent with the proposed density within the Downtown Commercial zoning district with an appropriate mix of uses. Therefore, the project is consistent with the surrounding neighborhood and is suitable for the proposed density.

5. **FINDING:** The design of the subdivision or the proposed improvements will not cause substantial environmental damage or substantially and avoidably injure fish or wildlife or their habitat (Government Code § 66474(e)).

FACT: The proposed project has been analyzed, consistent with the California Environmental Quality Act (CEQA), through certified Mitigated Negative Declaration, Environmental Document No. 2017-0018. With the incorporation of mitigation measures, in addition to project conditions, the proposed project's impacts will be mitigated to a threshold of less than significant, therefore the proposed project will not create substantial environmental damage.

6. **FINDING:** The design of the subdivision or the type of improvements will not cause serious health problems (Government Code § 66474(f)).

FACT: The proposed project and the types of improvements includes the subdivision of seven (7) existing parcels into four (4) parcels with two (2) parcels for future condominium purposes which includes a total 40 air space residential condominiums and eight (8) air-space non-residential condominiums. Because the proposed project is consistent with envisioned downtown redevelopment, and the improvements have been reviewed for consistency with the threshold established by the City, through adopted Ordinances, and City policies, the proposed project will not cause serious health problems.

7. **FINDING:** The design of the subdivision will not conflict with easements for access through or use of property within the proposed subdivision (Government Code § 66474(g)).

FACT: The installation of public improvements are necessary, prior to recordation of a Final Map, in order to ensure orderly development of the surrounding area (Government Code § 66411.1(b)(2)).

8. **FINDING:** The proposed project or use satisfies all applicable provisions of the Title 9 of the Atascadero Municipal Code this title.

FACT: Public improvements that are included, as conditioned by the City Engineer, are required and consistent with the City's adopted General Plan, Master Facilities Plan, and Municipal Code. Therefore the proposed improvements are necessary to ensure orderly development of the surrounding area.



SECTION 2. The Planning Commission of the City of Atascadero, in a regular session assembled on November 21, 2017, resolved to approve Tentative Parcel Map (AT 17-0088) for PLN 2017-1649, subject to the following, on file in the Community Development Department and incorporated herein by reference:

1. EXHIBIT A: Tentative Subdivision Map (AT 17-0088)
2. EXHIBIT B: Conditions of Approval
3. EXHIBIT C: Grading Plan
4. EXHIBIT D: Utility Plan

BE IT FURTHER RESOLVED, that a copy of this Resolution be delivered forthwith by the Planning Commission Secretary to the City Council of the City of Atascadero.

On motion by Commissioner _____ and seconded by Commissioner ____ the foregoing resolution is hereby adopted in its entirety by the following roll call vote:

AYES:	0
NOES:	0
ABSTAIN:	0
ABSENT:	0
ADOPTED:	

CITY OF ATASCADERO, CA

Duane Anderson
Planning Commission Chairperson

Attest:

Phil Dunsmore
Planning Commission Secretary



Exhibit B
Conditions of Approval
City of Atascadero



PLN 2017-1649
LA PLAZA DOWNTOWN REDEVELOPMENT
TENTATIVE PARCEL MAP AT 17-0088
6500 EL CAMINO REAL
PARENT APN 030-193-001,003,017,016, 031,030,033

The following conditions of approval apply to the project referenced above. The conditions of approval are grouped under specific headings that relate to the timing of required compliance. Additional language within a condition may further define the timing of the required compliance.

A. General Conditions.

1. The emergency services and facility maintenance costs listed below shall be PLN 100% funded by the project in perpetuity. The service and maintenance costs shall be funded through a community facilities district established by the City at the developer's cost. The funding mechanism must be in place prior to or concurrently with acceptance of the final maps. The funding mechanism shall be approved by the City Attorney, City Engineer and Administrative Services Director prior to acceptance of any final map. The administration of the above mentioned funds shall be by the City. Developer agrees to participate in the community facilities district and to take all steps reasonably required by the City with regard to the establishment of the district and assessment of the property.
 - All Atascadero Police Department service costs to the project.
 - All Atascadero Fire Department service costs to the project.
 - Off-site common City of Atascadero park facilities maintenance service costs related to the project.

2. Prior to recordation of the Parcel Map, the Applicant shall establish a benefit maintenance assessment district, Property Owners' Association, or similar funding mechanism approved by the City, to provide sufficient funds on an annual basis to pay for the operation, maintenance and future replacement of privately owned-in-common subdivision improvements. The engineer of record shall prepare and submit an estimated operating budget and capital improvement replacement analysis for review and approval by the City Engineer, prior to recordation of the Parcel Map. PLN

3. Prior to recording the Final Map, the Applicant shall have the map reviewed PWD by the public utility providers for power, telephone, gas, cable TV, and the Atascadero Mutual Water Company. The Applicant shall obtain a letter from each utility company stating that the easements and rights-of-way shown on the map for public utility purposes are acceptable.

4. Documents that the City of Atascadero requires to be recorded concurrently PWD with the Final Map (e.g.: off-site rights-of-way dedications, easements not



shown on the map, agreements, etc.) shall be listed on the certificate sheet of the map.

- 5. The City of Atascadero may require an additional map sheet for information purposes in accordance with the Subdivision Map Act. PWD
- 6. The El Camino Real pedestrian entry/exit doors serving any building shall not swing into the public right-of-way more than one foot. PWD
- 7. The proposed waterfall structure in front of Building A shall be located entirely within the subject property and shall not encroach into the public right-of-way. PWD
- 8. Public improvements required of this subdivision may be deferred to the time of development of each parcel. The relocation of the existing public sewer main shall be completed prior to issuance of, or, in conjunction with the issuance of building permits for the southerly most building (Tentative Parcel 3). PWD
- 9. Rights-of-way acquisitions and/or abandonments shall be completed in accordance with City regulations, the Streets and Highways Code, the Subdivision Map Act, and to the satisfaction of the City Engineer. Public rights-of-way abandonments completed through the Parcel Map process shall be adequately identified on the certificate sheet of the map as well as on subsequent map sheets. PWD
- 10. All plans shall contain the City of Atascadero "Standard Notes for Improvement Plans" on file in the City Engineer's office. PWD

B. Roadway Improvements

- 11. Construction of the new driveway approach, northwesterly of Building A, requires removal of the existing driveway approach serving the adjacent northwesterly property. Vehicular access to this property shall be combined with the subject property. PWD
- 12. Pedestrian ramps on El Camino Real shall be constructed on each side of the street type entrance that provides vehicular access to the existing gas station property (southerly of the subject property). PWD
- 13. The 20-foot wide vehicular access proposed on the southerly half of Atascadero Mall shall be extended westerly to a point that provides adequate vehicular access to the adjacent southerly property, to the satisfaction of the City Engineer and Community Development Director. PWD
- 14. Improvements within Atascadero Mall shall include pedestrian channelization (including physical barriers such as raised planters, decorative fencing or short walls, planter islands, etc.) necessary to direct pedestrians exiting the tunnel through the open plaza area, to the satisfaction of the City Engineer and Community Development Director. PWD



15. The applicant shall coordinate with the Regional Transit Authority regarding the relocation and modification to the existing bus shelter and bus stop. Final location shall be approved by the Regional Transit Authority and the City Engineer. PWD
16. Roadway signing, striping, and traffic signal modifications shall be in accordance with the California Manual on Uniform Traffic Control Devices (CA-MUTCD) and shall be designed and constructed to the satisfaction of the City Engineer. PWD

C. Wastewater Collection System

17. The public gravity sewer system within the subject property shall be modified in accordance with City Standards and to the satisfaction of the City Engineer. The new sewer main system shall be offered to the public and shall include a 20-foot wide public sewer easement. The wastewater collection system shall be designed and constructed in accordance with City standards and specifications, to the satisfaction of the City Engineer. PWD
18. Gravity sanitary sewer (SS) mains shall terminate in manholes. PWD
19. The on-site sewer laterals shall be privately owned and maintained by individual lot owners. PWD
20. Each lot served by the wastewater collection system shall pay all sewer fees prior to the issuance of a building permit. PWD

D. Stormwater

21. The project shall be designed and constructed in accordance with the post-construction storm water management regulations in accordance with City Standards and the State RWQCB Resolution R3-2013-0032. PWD
22. Common drainage basins (serving multiple lots) shall be designed to desilt, detain and meter storm flows in accordance with City regulations. PWD
23. Storm Sewer shall be of either cast-in-place or precast reinforced concrete pipe, polyvinyl chloride pipe, high density polyethylene pipe or an approved equal. PWD
- Minimum pipe diameter allowable on any storm drain within a roadway or road right-of-way shall be 18" diameter. A lesser size may be used for down drains on fill slopes if approved by the City Engineer.
 - Minimum design velocity in closed conduits shall be 2 f.p.s. when conduit is flowing to capacity and should not exceed 15 f.p.s.
 - Closed conduits shall be designed to convey the 10-year storm flow with gravity flow, the 25-year storm flow with head, and provide a safe overland route for the conveyance for the 100-year storm overflow.



- Manholes or junction boxes, entry to which does not fall in the gutter line, must have standard 24" diameter manhole covers. Those falling in the gutter line may use the standard grated manhole cover and serve also as an inlet manhole.
24. Prior to approval of subdivision improvement plans, the project engineer shall provide calculations and a report confirming compliance with City regulations and the low-impact development requirements. PWD
25. When a Storm Water Operation - Maintenance Plan (OMP) is required for an individual lot or group of lots, the City Engineer can require the OMP to be recorded as an agreement against each property. When recordation of an OMP agreement is required, the agreement shall contain provisions allowing the City to enter the property, following reasonable notice to the property owner or tenant, to conduct an inspection of the storm water system and post-construction storm water management controls. PWD
26. When an OMP is required, the OMP shall include a section addressing annual inspection and reporting to the City by a third party, to the satisfaction of the City Engineer. PWD

E. Easements

27. On-site easements for ingress, egress, pedestrian traffic, drainage, utilities, etc., shall be shown on the Parcel Map, or, created by separate easement documents to be recorded concurrently with the Parcel Map. PWD
28. Driveways serving more than one lot shall require an easement for ingress/egress, public & private utilities, and drainage, to the satisfaction of the City Engineer. PWD
29. Drainage easements shall be dedicated over areas containing drainage improvements that benefit or serve more than one property. The determination as to whether the easement is private or offered to the public will be determined by the City Engineer prior to approval of the subdivision improvement plan and Parcel Map. PWD
30. Wherever an easement is created for commonly owned or operated improvements for the benefit of more than one lot, there shall also be created a maintenance and operations agreement, to the satisfaction of the City Engineer and City Attorney. PWD
31. Easements that are not intended to continue in perpetuity shall not be shown on the Parcel Map and shall be recorded by separate instrument. PWD

F. Utilities

32. New utility distribution systems and services shall be constructed underground, to the satisfaction of the City Engineer. PWD



33. Each parcel shall be served with separate services for water, sewer, gas, PWD power, telephone and cable TV. Utility laterals shall be located and constructed to each lot in accordance with City Standards and Standard Specifications

****END CONDITIONS****



**ATTACHMENT 6: DRAFT PC RESOLUTION 2017-D – ROAD ABANDONMENT
PLN 2017-1649**

DRAFT RESOLUTION PC 2017-D

RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF ATASCADERO, CALIFORNIA, RECOMMENDING THE CITY COUNCIL APPROVE A ROAD ABANDONMENT TO VACATE PORTIONS OF AN EASEMENT FOR PUBLIC USE ALONG EL CAMINO REAL, AND A PORTION OF RIGHT OF WAY FOR ATASCADERO AVENUE PURSUANT TO SECTION 8332 OF THE CALIFORNIA STREETS AND HIGHWAYS CODE

AND RECOMMEND TO THE CITY COUNCIL A PARTIAL VEHICULAR CLOSURE OF ATASCADERO AVENUE FOR THE CONSTRUCTION OF A PUBLIC PEDESTRIAN PLAZA PURSUANT TO SECTION 12920 OF THE CALIFORNIA STREETS AND HIGHWAYS CODE

**PLN 2017-1649
LA PLAZA DOWNTOWN REDEVELOPMENT
Z3 LLC**

WHEREAS, an application has been received from Z3 LLC, Owner / Applicant (8189 San Dimas Lane, Atascadero, CA 93422) to abandon an approximate 0.11 acre portion of a public access easement directly adjacent to the El Camino Real Right-of-Way; abandon a portion of right-of-way, which is approximately 0.24 acre of Atascadero Avenue (also referred to as “Atascadero Mall”), being portions of Lots 18 and 19, Block H-B, Atascadero Colony Subdivision, City of Atascadero, County of San Luis Obispo, State of California and filed for record on October 21, 1914 in Book 4 at Page 32 of Maps; and partially close to vehicular traffic in the remaining portion of right-of-way along Atascadero Avenue (Mall), with the exception of 20-feet for the construction of a pedestrian plaza; and

WHEREAS, the current General Plan Designation and Zoning Designation is right-of-way; and,

WHEREAS, Official City of Atascadero Land Use and Zoning Maps reflect a portion of El Camino Real as right-of-way; however, this portion of right-of-way has been quitclaimed by the City of Atascadero through City Council Resolution 06-95; and,

WHEREAS, the proposed abandoned portion of the right-of-way for Atascadero Avenue (Mall) is shown on portion of Lot 18, Block H-B, Atascadero Colony Subdivision, City of Atascadero, County of San Luis Obispo, State of California and filed for record on



October 21, 1914 in Book 4 at Page 32 of Maps, approximately 0.24 acres, will become a part of the adjacent property currently zoned Downtown Commercial; and,

WHEREAS, the proposed abandoned the public use easement, approximately 0.11 acres created by an offer of dedication per Official Record 88-O.R.-114 along for El Camino Real will become a part of the adjacent property currently zoned Downtown Commercial; and,

WHEREAS, the remaining right of way, approximately 74-feet, as shown in Lot 8 of Block H-B, and a portion of Lot 6 Block E-B, Atascadero Colony Subdivision, City of Atascadero, County of San Luis Obispo, State of California and filed for record on October 21, 1914 in Book 4 at Page 32 of Maps shall be closed to vehicular traffic, with the exception of a 20-foot paved section directly southeast of Lot 6, Block E-B; and,

WHEREAS, the project is in conformance with the Circulation Element of the General Plan and all other applicable General Plan policies; and,

WHEREAS, an Initial Study and proposed Mitigated Negative Declaration, Environmental Document No. 2017-0018 was prepared for the project and made available for public review in accordance with the requirements of the California Environmental Quality Act (CEQA); and

WHEREAS, the laws and regulations relating to the preparation and public notice of environmental documents, as set forth in the State and local guidelines for implementation of the California Environmental Quality Act (CEQA) have been adhered to; and

WHEREAS, the Planning Commission held a duly noticed public hearing to consider the proposed Road Abandonment on November 21, 2017, at 6:00 p.m. and considered testimony, reports from staff, the applicants, and the public; and,

NOW, THEREFORE, the Planning Commission of the City of Atascadero takes the following actions:

SECTION 1. The Planning Commission makes the following findings recommending to the City Council of the City of Atascadero with respect to roadway abandonment pursuant to §8332 and partial vehicular closure pursuant §1920 to of the California Streets and Highways Code:

A. Findings for Roadway Abandonment SHC §8332

1. At the hearing, the legislative body shall hear the evidence offered by persons interested.

FACT. The Planning Commission received oral testimony by the project applicant, project representative, and interested persons prior to making a recommendation for vacation of public easements and roadway abandonment.



2. The street, highway, or public service easement described in the notice of hearing or petition is unnecessary for present or prospective public use

FACT. The Planning Commission, after hearing public testimony by persons interested in the vacation of a public easement along El Camino Real, and the abandonment of a portion of right-of-way in Atascadero Avenue directly adjacent to the proposed project, recommends to the City Council that these portions of right-of-way and public easements are unnecessary for the present or prospective use of the public and therefore should be vacated through recordation of Tentative Parcel Map AT 17-0088.

B. Findings for Partial Vehicular Closure SHC §1920

1. The governing body of a city by resolution or ordinance removes a street from public use, or closes it to vehicular or pedestrian traffic, such resolution or ordinance may set forth such minimum maintenance requirements, including the maintenance of drainage, for the street as the governing body determines are necessary for the public safety.

FACT. The Planning Commission recommends, through approval of a resolution, to the City Council that a portion of Atascadero Avenue be closed to vehicular traffic; however, public right-of-way shall be retained for the use of pedestrians, and bicyclists, and shall continue to be maintained by the City necessary for public safety.

SECTION 3. The Planning Commission recommends the following actions to the City Council of the City of Atascadero with respect to roadway abandonment pursuant to §8332 and partial vehicular closure pursuant §1920 to of the California Streets and Highways Code:

A. Abandoning a portion of the right-of-way for Atascadero Avenue (Mall) shown as a portion of Lot 18, Block H-B, Atascadero Colony Subdivision, City of Atascadero, County of San Luis Obispo, State of California and filed for record on October 21, 1914 in Book 4 at Page 32 of Maps, approximately 0.24 acres, converting this portion of property to a General Plan Land Use Designation of Downtown (D), and a Zoning District of Downtown Commercial (DC);

B. Abandoning a public use easement, approximately 0.11 acres created by an offer of dedication per Official Record 88-O.R.-114 along El Camino Real and reverting this portion of property to a General Plan Land Use Designation of Downtown (D), and a Zoning District of Downtown Commercial (DC);

C. Approximately 74-feet of right-of-way, owned and maintained by the City of Atascadero, as shown in Lot 8 of Block H-B, and a portion of Lot 6 Block E-B, Atascadero Colony Subdivision, City of Atascadero, County of San Luis Obispo, State of California and filed for record on October 21, 1914 in Book 4 at Page 32 of Maps shall be closed to vehicular traffic, with the exception of a 20-foot paved section directly southeast of Lot 6, Block E-B to construct a public pedestrian plaza.



Exhibit A: Location map
RAB 2013-0020



Exhibit B: Road Abandonment diagram

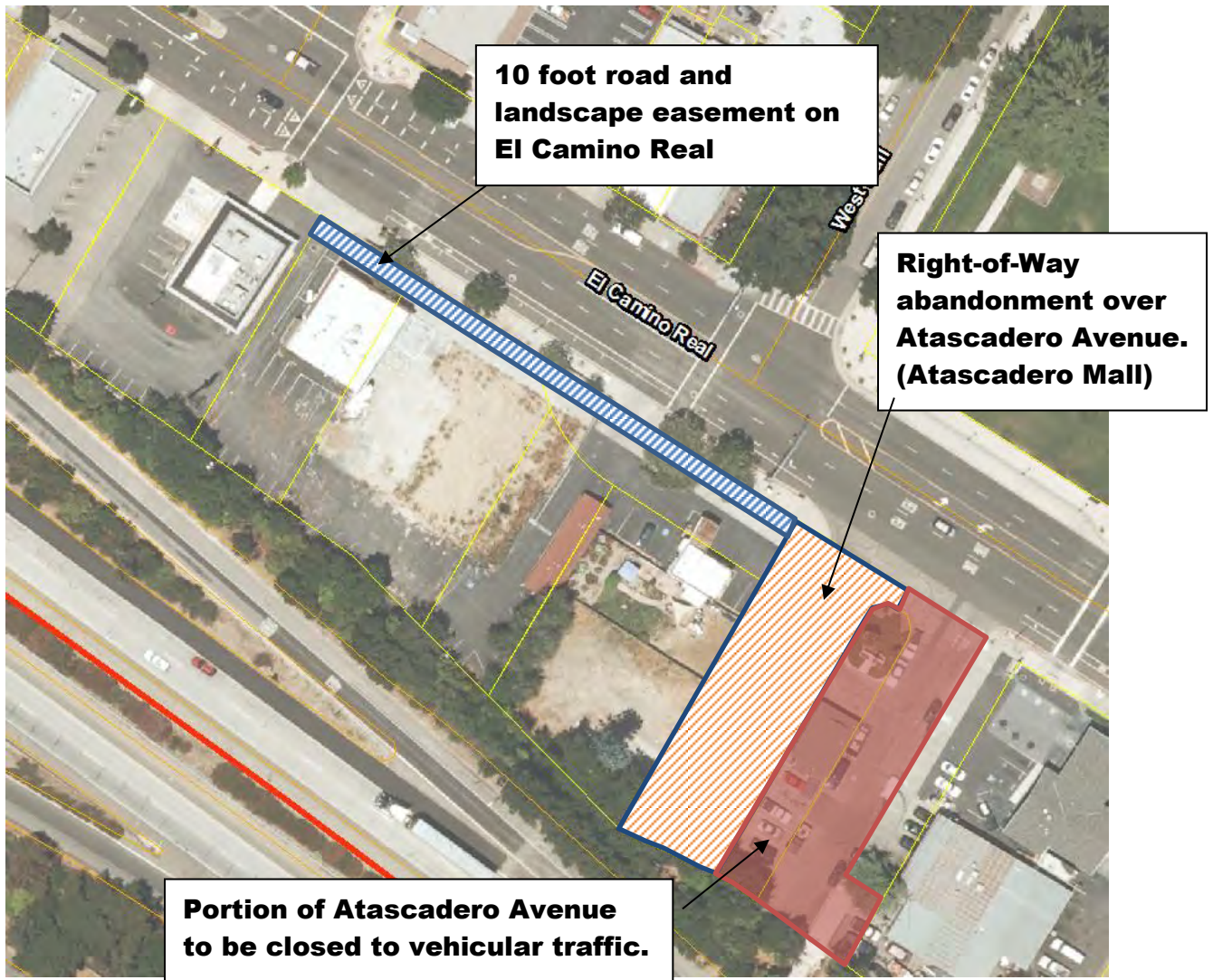


Exhibit C
Conditions of Approval
City of Atascadero



PLN 2017-1649
LA PLAZA DOWNTOWN REDEVELOPMENT
ROADWAY ABANDONMENT / PARTIAL VEHICULAR CLOSURE
6500 EL CAMINO REAL
PARENT APN 030-193-001,003,017,016, 031,030,033

The following conditions of approval apply to the project referenced above. The conditions of approval are grouped under specific headings that relate to the timing of required compliance. Additional language within a condition may further define the timing of the required compliance.

A. The following conditions shall be satisfied PRIOR to Abandonment of Right-of-Way, or at the time specified in the condition.

1. Rights-of-way acquisitions and/or abandonments shall be completed in accordance with City regulations, the Streets and Highways Code, the Subdivision Map Act, and to the satisfaction of the City Engineer. PWD
2. Easements that are not intended to continue in perpetuity shall not be shown on the Parcel Map and shall be recorded by separate instrument. PWD
3. Right-of-Way abandonment and public use easements shall be extinguished upon recordation of Tentative Parcel Map AT 17-0088. PWD
4. Remaining Right-of-Way shall remain a City Maintained street for maintenance purposes. PWD

****END CONDITIONS****





Atascadero Planning Commission

Staff Report – Community Development Department

PLN 2017-1641 Annex Subdivision – Master Plan of Development

RECOMMENDATION(S):

Design Review Committee recommends:

1. Adopt PC Resolution 2017-A, approving a Master Plan of Development for PLN 2017-1641 for a six (6) lot residential subdivision, consistent with the Del Rio Road Commercial Area Specific Plan, subject to Conditions of Approval based on findings;

2. Adopt PC Resolution 2017-B approving Vesting Tentative Subdivision Map for Tract 3104, six (6) lot subdivision with a remainder parcel, subject to Conditions of Approval and based on findings.

Project Info In-Brief:

PROJECT ADDRESS:	1827 & 1843 El Camino Real	Atascadero, CA	APN	049-102-032 (portion), 049-102-020 (portion)
PROJECT PLANNER	Alfredo R. Castillo, AICP Associate Planner	470-3436	acastillo@atascadero.org	
APPLICANT	MP Annex, LLC, 284 Higuera Street, San Luis Obispo, CA 93401			
PROPERTY OWNER	MP Annex, LLC, 284 Higuera Street, San Luis Obispo, CA 93401			
GENERAL PLAN DESIGNATION:	ZONING DISTRICT:	SITE AREA	EXISTING USE	PROPOSED USE
Single Family Residential (SRF-X)	Residential Single-Family (RSF-X) / SP-2 Overlay	1.67 acres	Vacant	Six (6) single-family homes with one (1) remainder lot.



ADDITIONAL INFORMATION MAY BE OBTAINED BY CONTACTING THE ATASCADERO
 COMMUNITY DEVELOPMENT DEPARTMENT AT

<http://www.atascadero.org>
 6500 PALMA AVENUE | ATASCADERO, CA 93422 | (805) 461-5000

ENVIRONMENTAL DETERMINATION
<input checked="" type="checkbox"/> Consistent with previously certified Environmental Impact Report SCH: 20100051034 – Final EIR Del Rio Road Commercial Area Specific Plan <input type="checkbox"/> Negative / Mitigated Negative Declaration No. _____ <input type="checkbox"/> Categorical Exemption CEQA – Guidelines Section 153 _____ <input type="checkbox"/> Statutory Exemption §§ 21000, et seq & _____ <input type="checkbox"/> No Project – Ministerial Project

DISCUSSION:

Existing Surrounding Uses / Parcel Configurations:

Existing Zoning		Existing Aerial / Surrounding	
			
North:	South:	East:	West:
Existing Single-Family Residential / RSF-X	Existing Single-Family Residential / RSF-X (PD-17 Overlay)	Existing Single-Family Residential / Residential Suburban (RS)	Vacant Commercial Retail (CR/Specific Plan-2)

Background:

The Del Rio Road Commercial Area Specific Plan was approved in June 2012. The Specific Plan contained two distinct projects: the Walmart portion and the Annex portion. The current owner, M.P. Annex, LLC purchased the Annex portion of the property and has continued to work on single-family residential development plans on their portion of the Specific Plan and has submitted a proposal for six (6) new residences, known as “The Annex Residential” on the eastern end of the Del Rio Road Commercial Area Specific Plan (DRCASP).



Summary:

The applicant proposes the following consistent with the approved DRCASP:

- Six (6) 2-story single-family residences with 2-car garages;
- Maximum building height of 29.5-feet;
- A pedestrian connection to allow for future residents, as well as, existing residents, to access future commercial uses near the Del Rio Road / El Camino Real intersection.
- A subdivision of seven (7) total lots with one (1) lot designated as a remainder for future subdivision purposes.

The subject site is located along South Marisol Way, which intersects Obispo Road from Del Rio Road. This proposed project is located within the Del Rio Road Commercial Area Specific Plan (SP-2). The Specific Plan outlines that development standards shall follow those set forth in the Section 9-3.662 (Planned Development Overlay Zone No. 17), as well as, the following conditions, as specified in City Council Resolution 2012-046, Master Plan of Development approval, for the Specific Plan:

Condition 15 – City Council Resolution 2012-046

<p>15. The Specific Plan includes a Planned Development-17 (PD-17) overlay to allow lot sizes smaller than one-half acre, with a maximum of 4 units per gross acre for The Annex Project's 1.7 acre residential site:</p> <ul style="list-style-type: none"> a. A future subdivision Tract Map and Conditional Use Permit (Master Plan of Development), will be required for and a 6 lot Tract Map development of the subject parcel beyond the one 1.7 acre single-family lot. b. Project access will be from Marisol Way. c. Marisol Way will end as a cul-de-sac in the residential project with a paved emergency and pedestrian connection provided to the vacant parcel to the north. d. The subdivision will be annexed into the City's community facilities district. e. The subdivision will be required to form a road maintenance mechanism. f. The subdivision will provide affordable housing consistent the City's inclusionary housing policy. g. Project architecture and landscaping will be approved as part of the conditional use permit. 	<p>Future Tentative Map / conditional use permit</p>	<p>PS</p>
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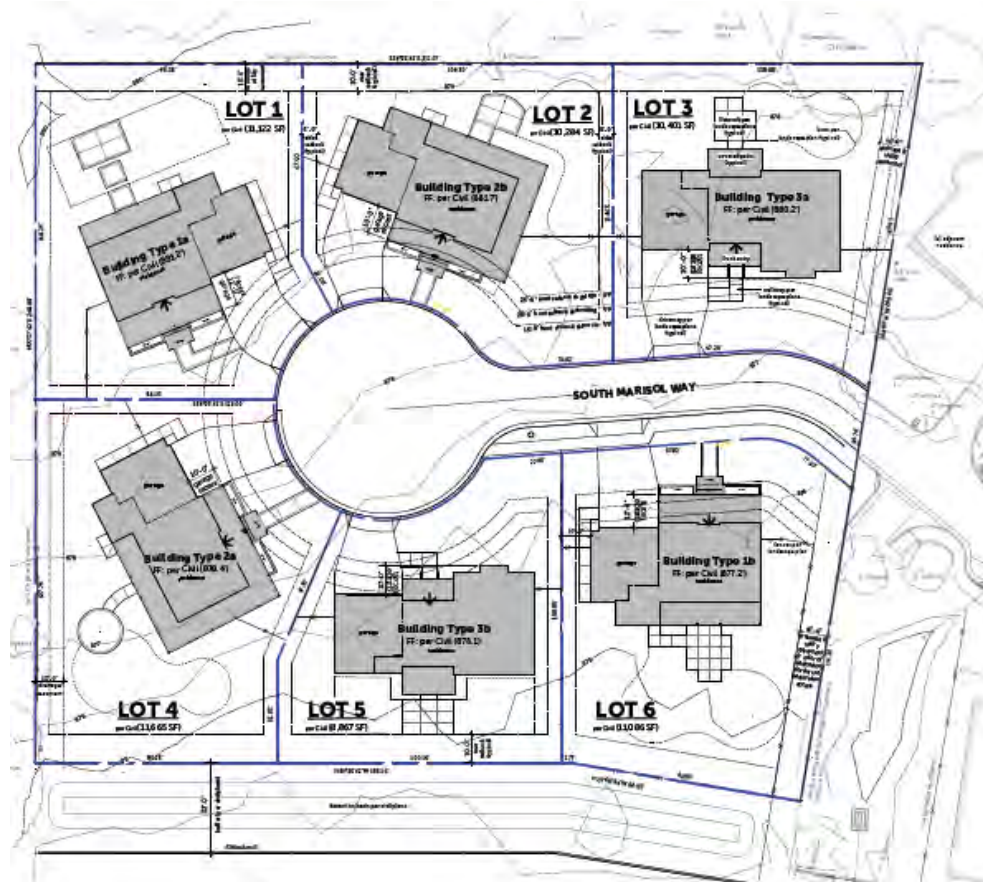


Analysis:
Subdivision Design

The proposed six lot development would be accessed via South Mirasol Way. This project will extend South Mirasol Way to provide access to all 6 residences. Each residence contains a separate driveway for parking and a two-car garage. On-street parking is not available, but each driveway has ample space to accommodate additional vehicles/guests. The extended street would match the centerline and street geometrics of the existing South Mirasol Way, therefore Staff and the applicant design team carried the previous street design through for neighborhood continuity.

The applicant has met the required minimum parking standards and would not require a parking waiver. Prior to application submittal, Staff reviewed ways to connect this subdivision with future subdivisions to the north. Based on internal discussions with Planning and Public Works, the roadway geometrics would not meet minimum City and State standards to accomplish a connection from Del Rio Road. The Specific Plan approval provided a condition for this project to have a cul-de-sac. The applicant has met this specified condition. Lastly, the applicant has no control over the property to the north and there are no current plans to subdivide or develop that adjacent property to facilitate a connection.

Proposed Subdivision



The preliminary subdivision design, as shown in the approved Master Plan of Development for the Specific Plan, showed the proposed project with a pedestrian path and “emergency access” path between proposed lots 1 and 4. Atascadero Fire & Emergency Services reviewed the condition and determined that the emergency access was not needed and the future subdivision to the north may be designed in such a way to facilitate sufficient emergency access. The pedestrian path, which creates an active transportation link from the proposed subdivision, and the existing South Mirasol Way neighborhood, has been relocated to the eastern edge of Lot 6 to instead provide a connection to a future commercial area. The Community Development Director has determined that this is in substantial conformance with Condition #15 in City Council Resolution 2012-046.

Architectural Residential Unit Design

The Specific Plan, which is the document that governs development within the Specific Plan Area, is silent on requirements for architectural design of the single-family units. The document relies on the Design Review process to drive architectural design of the proposed units. The DRC reviewed the proposed architecture and endorsed the unit design as proposed.

The architectural elevations of the residential units utilize “California Farmhouse” and “California Colonial” architectural themes, which are modernized version of “farmhouse” and “colonial” architecture. Three elevation styles are proposed for single-family units. The proposed architecture fully represents the intent of a “high quality” architectural design intended for Planned Developments and envisioned by the Specific Plan. An earth toned color and white color scheme is proposed. The DRC at its August meeting reviewed the proposed elevations and recommend to the Planning Commission approval. The elevation drawings are included as exhibits to Resolution 2017-A and the Applicant Design Package is available for review online.

Example of Proposed Elevation



Landscaping Plan

Consistent with Master Plan of Development standards, the applicant has included a landscaping plan. The landscaping plan has been reviewed against the Atascadero Municipal Code (AMC) to ensure water efficient landscaping. The applicant has proposed high quality fencing, in addition, to drought tolerant landscaping throughout the project.

Consistency with the Del Rio Road Commercial Area Specific Plan

The proposed project has been reviewed for consistency with the DRCASP. The proposed project meets the setback standards and other regulations set forth in AMC Section 9-3.662, standards for Planned Development No. 17. Meeting the regulations sets forth in this section, as well as, ensuring that the project is consistent with additional required conditions including, but not limited to:

- Annexation into the City's Community Facilities District (CFD);
- Provision to pay into the City's inclusionary housing fund;
- Payment of fair share costs for intersection improvements to Del Rio Road / El Camino Real intersection;
- Payment of fair share mitigation fees for Highway 101 / Del Rio Road Interchange fees.

Conclusion:

The proposed project is consistent with the Del Rio Road Commercial Area Specific Plan, and the City's Municipal Code. The applicant is ready to move forward with the development of the project for six (6) single-family residences. The proposed project contributes additional housing stock at a time that the State, and San Luis Obispo County is in need of additional housing units. The proposed design scheme of the project is of high quality and is recommended for approval by the Design Review Committee. This project, coupled with the Annex Hotel, is a beginning for the development of the Specific Plan. The DRC and City staff recommend approval of the proposed project.

ENVIRONMENTAL DETERMINATION:

This project is a necessarily included element of the projects considered in Final EIR No. 20100051034 – Final EIR Del Rio Road Commercial Area Specific Plan certified by the City Council on June 26, 2012, and as amended by the City council July 2017, which adequately addressed the effects of the proposed project. No substantial changes have been made in the project, no substantial changes in the circumstances under which the project is being undertaken and no new information of substantial importance to the project which was not known or could not have been

FINDINGS:

To approve the Master Plan of Development and Vesting Tentative Subdivision Map, the Planning Commission must make the following findings. These findings and the facts to support these findings are included in attached draft Resolutions 2017-A and 2017-B.



Conditional Use Permit (AMC Section 9-2.110(b).(3).(iv)

1. The proposed project or use is consistent with the General Plan:
2. The proposed project or use satisfies all applicable provisions of this title:
3. The establishment, and subsequent operation or conduct of the use will not, because of the circumstances and conditions applied in the particular case, be detrimental to the health, safety or welfare of the general public or persons residing or working in the neighborhood of the use, or be detrimental or injurious to property or improvements in the vicinity of the use:
4. That the proposed project or use will not be inconsistent with the character of the immediate neighborhood or contrary to its orderly development:
5. That the proposed use or project will not generate a volume of traffic beyond the safe capacity of all roads providing access to the project, either existing or to be improved in conjunction with the project, or beyond the normal traffic volume of the surrounding neighborhood that would result from full development in accordance with the land use element:
6. That the proposed project is in compliance with any pertinent city policy or criteria adopted by ordinance or resolution of the city council.

Vesting Tentative Subdivision Maps

1. The proposed subdivision, together with the provisions for its design and improvement, is consistent with the General Plan and the proposed Specific Plan (Government Code §§ 66473.5 and 66474(a) and (b)):
2. The site is physically suitable for the type of development (Government Code § 66474(c)):
3. The site is physically suitable for the proposed density of development (Government Code § 66474(d)):
4. The design of the subdivision or the proposed improvements will not cause substantial environmental damage or substantially and avoidably injure fish or wildlife or their habitat. (Government Code § 66474(e)):
5. The design of the subdivision or the type of improvements will not cause serious health problems. (Government Code § 66474(f)):



6. The design of the subdivision will not conflict with easements for access through or use of property within the proposed subdivision. (Government Code § 66474(g)):
7. The installation of public improvements are necessary prior to recordation of a Final Map in order to insure orderly development of the surrounding area (Government Code § 66411.1(b)(2)):

ALTERNATIVES:

1. The Planning Commission may include modifications to the project and/or conditions of approval for the project. Any proposed modifications including conditions of approval, should be clearly re-stated in any vote on any of the attached resolutions.
2. The Planning Commission may determine that more information is needed on some aspect of the project and may refer the item back to the applicant and staff to develop the additional information. The Commission should clearly state the type of information that is required. A motion, and approval of that motion, is required to continue the item to a future date.
3. The Planning Commission may deny the project. The Commission must specify what findings cannot be made, and provide a brief oral statement, based on the Staff Report, oral testimony, site visit, correspondence, or any other rationale introduced and deliberated by the Planning Commission.

ATTACHMENTS:

1. Project Review
2. Draft PC Resolution 2017-A
3. Draft PC Resolution 2017-B



**ATTACHMENT 1: Project Review
PLN 2017-1641**

Basic Project Information			
Project Number:	PLN 2017-1641		
Planner:	Alfredo R. Castillo, ACIP		
Project Address:	1827 & 1843 El Camino Real		
APN:	049-102-032 (portion) / 020 (portion)		
City: Atascadero	County: San Luis Obispo		
Site Area:	1.67 acres		
General Plan Designation:	Single Family Residential (SFR-X)		
Zoning District:	Residential Single Family (RSF-X) / SP-2 Overlay		
Project Description:	Proposed subdivision of a portions of two (2) legal lots into six (6) consistent with the Del Rio Road Commercial Area Specific Plan (DRCASP) overlay zone.		
Existing & Surrounding Information			
Existing Uses:	Vacant		
Use Classification:	Single Family Dwelling	Allowed <input checked="" type="checkbox"/>	Conditional <input type="checkbox"/>
Surrounding Uses / Zoning District:	North:	Single Family Residential / RSF-X	
	South:	Single Family Residential / RSF-X (PD-17 Overlay)	
	East:	Single Family Residential / Residential Suburban (RS)	
	West:	Vacant / Commercial Retail (CR/SP-2)	
Colony house(s) on property?	Yes <input type="checkbox"/>	No <input checked="" type="checkbox"/>	Notes:
Any existing structures 50 years or older?	Yes <input type="checkbox"/>	No <input checked="" type="checkbox"/>	Notes:



Does the site contain any jurisdictional waters? (blue line creeks, wetlands, etc.)	<input type="checkbox"/> Atascadero Creek <input type="checkbox"/> Graves Creek <input type="checkbox"/> Paloma Creek <input type="checkbox"/> Boulder Creek <input type="checkbox"/> Other _____ <input checked="" type="checkbox"/> N/A			
Zoning Ordinance / Municipal Code Standards:				
Does the proposed project exceed the maximum density allowed in the existing/proposed zoning district?	Yes <input type="checkbox"/>	No <input checked="" type="checkbox"/>	Calculate density: 3.5 units per acre	
What is the total non-residential square foot (sf) and Floor Area Ratio? <input checked="" type="checkbox"/> N/A	Total Square Foot _____ FAR _____			
Does the proposed project meet setback standards? (AMC 9-4) <input type="checkbox"/> N/A	Yes <input checked="" type="checkbox"/>	No <input type="checkbox"/>	If no explain: The Specific Plan sets setback standards per PD-17. Standards include 15-feet for porch, 20-foot front setback living and 25 for garage.	
Does the proposed project meet maximum height standards? (AMC 9-4) <input type="checkbox"/> N/A	Yes <input checked="" type="checkbox"/>	No <input type="checkbox"/>	If no explain:	
If the proposed project requires fencing, does it meet standards? (AMC 9-4) <input type="checkbox"/> N/A	Yes <input checked="" type="checkbox"/>	No <input type="checkbox"/>	If no explain:	
If the proposed project requires landscaping, does it meet standards? (AMC 9-4 / AMC 8-10) <input type="checkbox"/> N/A	Yes <input checked="" type="checkbox"/>	No <input type="checkbox"/>	If no explain:	
If the proposed project includes a parking requirement, does it meet standards? (AMC 9-4) <input type="checkbox"/> N/A	Yes <input checked="" type="checkbox"/>	No <input type="checkbox"/>	Parking Required: 2 per lot Parking Provided: 2 per lot	If no explain:
If the proposed project includes lighting, does it meet standards? (AMC 9-4) <input type="checkbox"/> N/A	Yes <input checked="" type="checkbox"/>	No <input type="checkbox"/>	If no explain:	



<p>Does the proposed project meet established standards for uses listed in AMC 9-6, if applicable?</p> <p><input checked="" type="checkbox"/> N/A</p>	Yes <input type="checkbox"/>	No <input type="checkbox"/>	If no explain:
<p>Does the proposed project need any other exceptions to the City Zoning Ordinance?</p>	Yes <input type="checkbox"/>	No <input checked="" type="checkbox"/>	If yes explain:

Environmental Information			
<p>Is the proposed project under the screening criteria for Project Air Quality Analysis by SLOAPCD?</p>	Yes <input checked="" type="checkbox"/>	No <input type="checkbox"/>	Notes:
<p>Based on aerial photography of the site, will the project have an effect on any riparian or sensitive natural community identified in local or regional plans, policies, or regulations or by the California Department of Fish and Game or US Fish and Wildlife Service?</p>	Yes <input type="checkbox"/>	No <input checked="" type="checkbox"/>	Notes:
<p>Is the proposed project located on or near a known historical or cultural resource (Use GIS internal mapping)?</p>	Yes <input type="checkbox"/>	No <input checked="" type="checkbox"/>	Notes:
<p>Does the site contain any evidence of past landslides, unstable soils or serpentine rock?</p>	Yes <input type="checkbox"/>	No <input checked="" type="checkbox"/>	Notes:
<p>Does the proposed project include more than 50 cubic yards of grading?</p>	Yes <input checked="" type="checkbox"/> (requires grading plan)	No <input type="checkbox"/>	Notes:
<p>Does the proposed project including grading on slopes greater than 10 percent?</p>	Yes <input type="checkbox"/>	No <input checked="" type="checkbox"/>	Notes:



<p>Does the new project include more than 2,500 square feet of new or replacement impervious surface? (required for RWQCB Post Stormwater Construction Regulations)</p>	<p>Yes <input checked="" type="checkbox"/></p>	<p>No <input type="checkbox"/></p>	<p>Total amount of impervious surface _____</p>
<p>Does the proposed project remove any native trees? (AMC 9-11)</p>	<p>Yes <input type="checkbox"/></p>	<p>No <input checked="" type="checkbox"/></p>	<p>Number of Trees proposed to be removed: _____ Total DBH proposed to be remove: _____</p>
<p>Is the project located on a site included on a list of hazardous materials sites compiled pursuant to Government Code Section 65962.5 (Cortese List)?</p>	<p>Yes <input type="checkbox"/></p>	<p>No <input checked="" type="checkbox"/></p>	<p>If Yes explain:</p>
Environmental Information			
<p>Does the proposed project alter the existing drainage pattern of the site or alter a designated waters of the US?</p>	<p>Yes <input type="checkbox"/></p>	<p>No <input checked="" type="checkbox"/></p>	<p>If Yes explain:</p>
<p>Does the proposed project increase noise levels in excess of City Standards when the use is complete?</p>	<p>Yes <input type="checkbox"/></p>	<p>No <input checked="" type="checkbox"/></p>	<p>If Yes explain:</p>
<p>Does the proposed project increase temporary noise levels that cannot be mitigated by the City's existing Noise Ordinance?</p>	<p>Yes <input type="checkbox"/></p>	<p>No <input checked="" type="checkbox"/></p>	<p>If Yes explain:</p>
<p>Does the proposed project require construction of new water and/wastewater treatment facilities?</p>	<p>Yes <input type="checkbox"/></p>	<p>No <input checked="" type="checkbox"/></p>	<p>If Yes explain:</p>



Does the proposed project require the construction of new recreational facilities?	Yes <input type="checkbox"/>	No <input checked="" type="checkbox"/>	If Yes explain:	
Does the proposed project decrease the established traffic Level of Service below Level "C" as contained in the General Plan? (Use ITE Trip Generation for review)	Yes <input type="checkbox"/>	No <input checked="" type="checkbox"/>	Number of daily trips generated: _____	If Yes explain: Traffic counts per DRCASP
			PM Peak: _____	
City Council Policy				
Is the project applicable to any of the following City Council policies?	Inclusionary Housing <input checked="" type="checkbox"/> Mixed-Use Processing <input type="checkbox"/> Park / Creek Reservation <input type="checkbox"/> Planned Development <input type="checkbox"/> Prime Commercial Sites <input type="checkbox"/>			



ATTACHMENT2: Draft PC Resolution 2017-A – Master Plan of Development
PLN 2017-1641

DRAFT RESOLUTION B

RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF
ATASCADERO, CALIFORNIA, APPROVING A MASTER PLAN OF
DEVELOPMENT

ANNEX SUBDIVISION
WHELEN / MP ANNEX, LLC
(PLN 2017-1647)

WHEREAS, an application has been received from Whelen Consulting (PO Box 5021, San Luis Obispo, CA 93401) applicant, and owner, Annex MP, LLC (284 Higuera Street, San Luis Obispo, CA 93401), to consider Planning Application PLN 2017-1647, for a project consisting of a Master Plan of Development, and vesting tentative subdivision map on a 1.67 acre site located on 1827 & 1847 El Camino Real, Atascadero, CA 93422 (Portions of APN 102-032-049, and 020); and

WHEREAS, the site's current General Plan Land Use Designation is Single Family Residential (SFR-X); and

WHEREAS, the site's current Zoning District is Residential single Family (RSF-X) with a Specific Plan Overlay Zone (SP-2); and

WHEREAS, The adopted Del Rio Commercial Area Specific Plan (DRCAS) requires approval of a Master Plan of Development in the form of a Conditional Use Permit (CUP) to promote orderly and harmonious development and to enhance the opportunity to best utilize special site characteristics; and

WHEREAS, the laws and regulations relating to the preparation and public notice of environmental documents, as set forth in the State and local guidelines for implementation of the California Environmental Quality Act (CEQA) have been adhered to; and

WHEREAS, a Final Environmental Impact Report, SCH No. 20100051034, Del Rio Road Commercial Area Specific Plan, was certified on June 26, 2012 by the City Council of the City of Atascadero, in accordance with the requirements of the California Environmental Quality Act (CEQA) and the approve project is in substantial conformance with the analyzed; and

WHEREAS, the Planning Commission of the City of Atascadero, at a Public Hearing held on November 21, 2017, studied and considered PLN 2017-1641 to establish a Master Plan of Development, and



NOW, THEREFORE BE IT RESOLVED by the Planning Commission of the City of Atascadero:

SECTION 1. The Planning Commission makes the following findings, determinations and approvals with respect to the proposed Master Plan of Development:

A. Findings for Approval of Master Plan of Development

1. **FINDING:** The proposed project or use is consistent with the General Plan.

FACT: The proposed project is consistent with the intention of Residential Single-Family Development, as envisioned by the Del Rio Commercial Area Specific Plan. The proposed project meets the underlying zoning ordinance's prescribed minimum density requirements for 4 units per acre.

1. **FINDING:** The proposed project or use satisfies all applicable provisions of the Title 9 of the Atascadero Municipal Code this title.

FACT: The proposed project meets all the development standards outlined in Planned Development Overlay Zone No.17, consistent with the Specific Plan Overlay Zone No.2, the Del Rio Road Commercial Area Specific Plan.

2. **FINDING:** The establishment, and subsequent operation or conduct of the use will not, because of the circumstances and conditions applied in the particular case, be detrimental to the health, safety or welfare of the general public or persons residing or working in the neighborhood of the use, or be detrimental or injurious to property or improvements in the vicinity of the use.

FACT: The proposed project will not be detrimental to the health, safety or welfare of the general public or persons residing or working in the vicinity as the proposed project includes residential uses that are consistent with the City's underlying zoning (RSF-X), as well as consistent with the goals and policies of the City's General Plan and Housing Element. Residential uses will not involve hazardous waste handling.

3. **FINDING:** The proposed project or use will not be inconsistent with the character of the immediate neighborhood or contrary to its orderly development.

FACT: The proposed project is consistent with the immediate neighborhood as it shares similar roadway geometrics, and additionally is approved with similar PD-17 overly zone standards.

4. **FINDING:** The proposed use or project will not generate a volume of traffic



beyond the safe capacity of all roads providing access to the project, either existing or to be improved in conjunction with the project, or beyond the normal traffic volume of the surrounding neighborhood, that would result from full development in accordance with the land use element.

FACT: The proposed project will generate 60 average new daily trips consistent with the certified Del Rio Road Commercial Area Specific Plan FEIR and will continue to operate at an acceptable level of service with the proposed project, as well as what is considered build-out for the General Plan. The proposed project will be required to pay its fair share of intersection improvements to the Del Rio Road / El Camino Real intersection and the Del Rio Road / US Highway 101 interchange, consistent with City Council Resolution 2012-046.

5. **FINDING:** The proposed project is in compliance with any pertinent City policy or criteria adopted by Ordinance or Resolution of the City Council.

FACT: The proposed project will be conditioned to meet the City Council's adopted Inclusionary Housing Policy and provide affordable housing units for individuals or families with incomes ranging from very-low, low, and moderate income. The proposed project has also been conditioned to annex into the City's Community Facilities District to make the proposed project fiscally neutral. Finally, the project has been reviewed for consistency with the City Council's adopted policy on Planned Developments. The proposed project meets the thresholds for high quality architectural design, landscaping, common open spaces, and affordable housing. Therefore, the project, as proposed, is in compliance with pertinent City policies established by the City Council.

SECTION 2. Approval of Master Plan of Development. The Planning Commission of the City of Atascadero, in a regular session assembled on November 21, 2017, resolved to approve to the Master Plan of Development subject to the following, on file in the Community Development Office and incorporated herein by reference:

1. EXHIBIT A: Conditions of approval / Mitigation Monitoring Program
2. EXHIBIT B: Master Plan of Development / Site Plan
3. EXHIBIT C: Landscape Plan
4. EXHIBIT D: Elevations / Floor Plan
5. EXHIBIT E: Colors and Materials Board
6. EXHIBIT F: Grading and Drainage Plan
7. EXHIBIT G: Utility Plan



On motion by Commissioner _____ and seconded by Commissioner _____ the foregoing resolution is hereby adopted in its entirety by the following roll call vote:

AYES: ()

NOES: ()

ABSTAIN: ()

ABSENT:

ADOPTED:

CITY OF ATASCADERO, CA

Duane Anderson
Planning Commission Chairperson

Attest:

Phil Dunsmore
Planning Commission Secretary



Exhibit A
Conditions of Approval
City of Atascadero



PLN 2017-1647
ANNEX SUBDIVISION
1827 & 1843 EL CAMINO REAL
PARENT APN 049-102-032, 020 (PORTIONS)

The following conditions of approval apply to the project referenced above. The conditions of approval are grouped under specific headings that relate to the timing of required compliance. Additional language within a condition may further define the timing of the required compliance.

A. The following conditions shall be satisfied PRIOR TO THE RECORDATION OF A FINAL MAP, or at the time specified in the condition.

1. All Conditions, no matter what timing is specified in the above header, shall be completed that are attached as Conditions of Approval in PC Resolution 2017-B to final Vesting Tentative Subdivision Map for Tract 3104. PLN
2. All pertinent conditions, with City Council Resolution 2012-046 & City Council Resolution 2012-050, with the exception of modification to Condition 15 in city Council Resolution 2012-046 eliminating the need for an emergency fire access. PLN

B. The following conditions shall be satisfied prior to the issuance of the first of any DEMOLITION PERMIT, BUILDING PERMITS, SUBDIVISION IMPROVEMENTS, or at the time specified in the condition.

3. Vesting Tentative Subdivision Map for all lots proposed in Tract 3104 was deemed complete on **August 29, 2017**, for the purposes of vested development rights and fees consistent with the Subdivision Map Act of the State of California. PLN
4. A landscape and irrigation plan shall be approved prior to the issuance of building permits and included as part of site improvement plan consistent with EXHIBIT C and must include the following: PLN
 - All exterior meters, air conditioning units and mechanical equipment shall be screened with landscape material and/or architecturally compatible features.
5. All project fencing shall be installed consistent with EXHIBIT C and details must be submitted and approved as a part of landscaping plan. PLN
6. Fire hydrant locations shall be located within the development to the satisfaction of the City Fire Marshall, if required. FD
7. Properties and/or areas that are managed or owned by the HOA shall be metered separately. PWD

C. The following conditions shall IMPLEMENTED DURING THE CONSTRUCTION PHASE OF THE PROJECT.

8. All site work, grading, and site improvements shall be in substantial conformance with the Master Plan of Development as shown in any of the enclosed exhibits. PLN



D. The following conditions shall be met prior to the RELEASE OF UTILITIES, FINAL INSPECTION, OR ISSUANCE OF A CERTIFICATE OF OCCUPANCY, whichever occurs first.

9. All landscaping is to be installed at each individual lot prior to final inspection. PLN

E. The following conditions shall be complied with AT ALL TIMES that the use permitted by this planning application occupies the premise, and shall be applied to the project in perpetuity until such time that the use is extinguished.

10. The entitlement described at the location per this resolution is determined to be vested with the property upon issuance of a building permit. PLN

11. Approval of this entitlement shall be final and effective consistent with Atascadero Municipal Code (AMC) section 9-1.111 seq. et. al. PLN

12. Project construction must be in accordance with provided Exhibit(s), adopted with this Resolution. Changes to architecture, landscaping design, and non-substantive subdivision design may be approved by the Design Review Committee (DRC). PLN

13. In accordance with the Atascadero Municipal Code section 9-8.105, any violation of any of the conditions of approval is unlawful and may be cause for revocation of this entitlement and subject the applicant and/or future property owners to the penalties set forth in the Atascadero Municipal Code, as well as any other available legal remedies. PLN

14. The applicant shall agree to indemnify and defend at his/her sole expense any action brought against the City, its present or former agents, officers, or employees because of the issuance of this approval, or in any way relating to the implementation thereof, or in the alternative, to relinquish such approval. The applicant shall reimburse the City, its agents, officers, or employees, for any court costs and attorney's fees which the City, its agents, officers or employees may be required by a court to pay as a result of such action. The City may, at its sole discretion, participate at its own expense in the defense of any such action, but such participation shall not relieve applicant of his/her obligations under this condition. CM

15. Should the described use be abandoned or extinguished, the property may be used and / or developed with any use allowed by the underlying zoning district. PLN

****END CONDITIONS****



EXHIBIT B: MASTER PLAN OF DEVELOPMENT / SITE PLAN

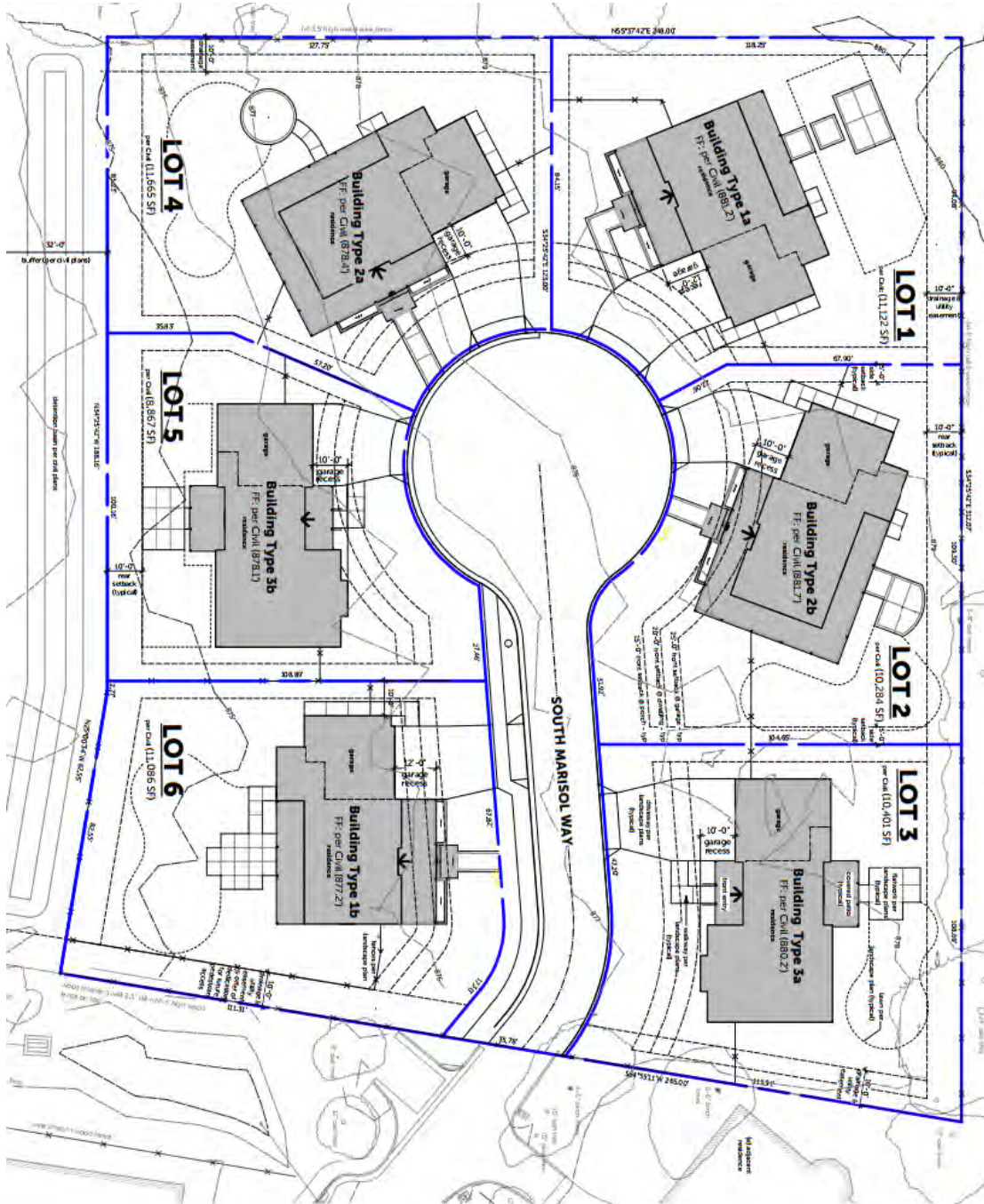


EXHIBIT C- LANDSCAPE PLAN WALLS / FENCES

Water Conservation Notes

The following water conservation techniques shall be employed in this Project:

- Water conserving plants, labeled as "Low" in the Water Use Manual and of Landscape Species (WUCOLS) 10 or lower for California Climate Zones 16-24 shall be utilized in 75% of the total plant count.
- Irrigation systems shall be designed into specific floodwaters.
- Soil moisture sensors shall be installed in specific water holding capacity of soil.
- Automatic irrigation system shall allow "smart controller" and provide multi-tiered scheduling feature to adjust water levels in real time.

Statement of Water Conserving Irrigation Design

The following principles of irrigation design/standard for this project are intended specifically as conserving water and improving the efficiency of the irrigation system:

- All irrigation shall be drip or dripless systems. No overhead design.
- Irrigation systems shall be designed according to water needs and weather.
- Installation of irrigation system shall include "smart controller" with water scheduling feature.
- Installation of irrigation system shall include "smart controller" with water scheduling feature.
- Installation of irrigation system shall include "smart controller" with water scheduling feature.

To help maximize the irrigation efficiency as mandated by the design, irrigation system shall be tested and maintained on a monthly basis by the water service staff.

DETENTION BASIN PLANTING CONCEPT
SCALE: 1/4" = 1'-0"

TEN (100) FOOT WIDE PEDESTRIAN EASEMENT CONCEPT
SCALE: 1/4" = 1'-0"

Drought-tolerant Front Yard Landscape (No Lawn)

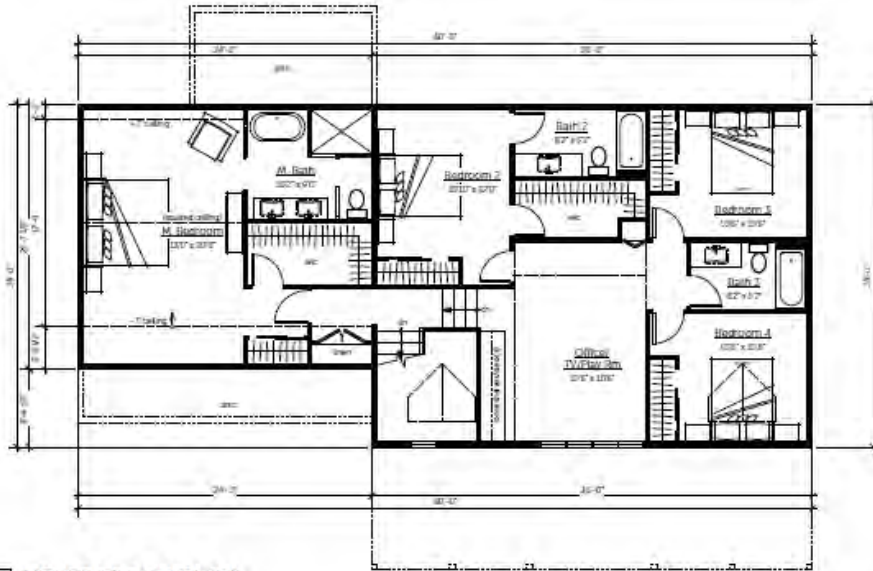
Six (6') Foot Tall, Decorative, Earth-toned Split Face Block Masonry Wall (Along rear property line, Lots 4, 5 and 6)

5' Woven Wire with Wood Posts Front Yard Fence (Step Down to 30" in front yard setback)

5' Cedar Rear and Side Yard Fence

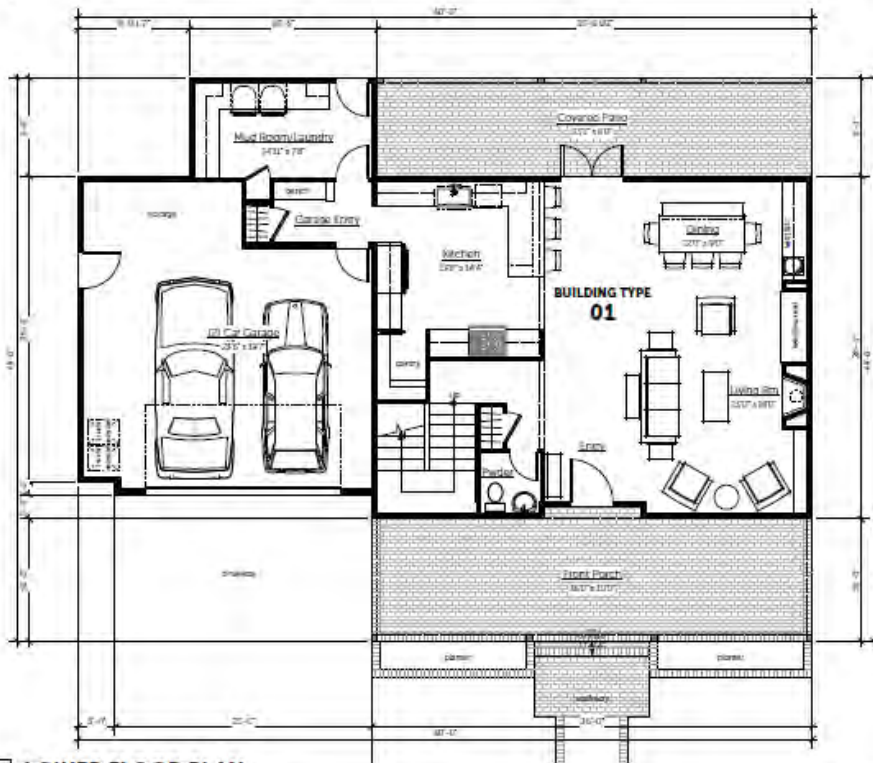


EXHIBIT D: ELEVATIONS / FLOOR PLAN



2 UPPER FLOOR PLAN

Scale: 3/16" = 1'-0"

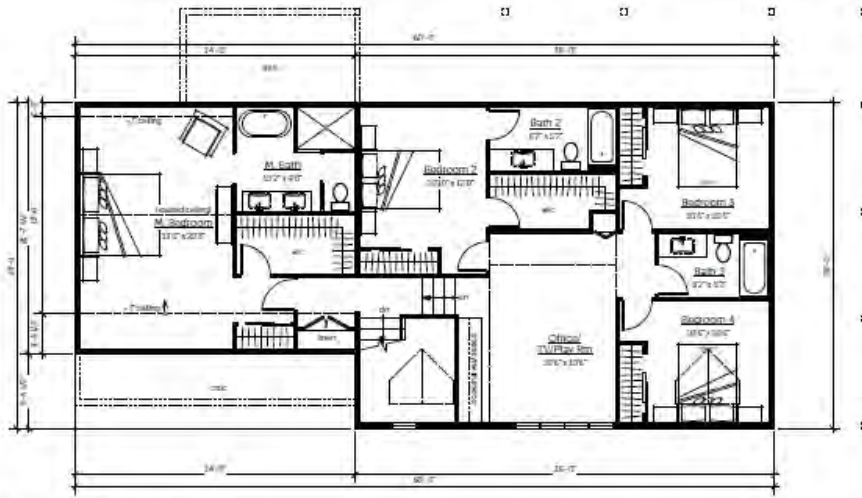


1 LOWER FLOOR PLAN

Scale: 3/16" = 1'-0"

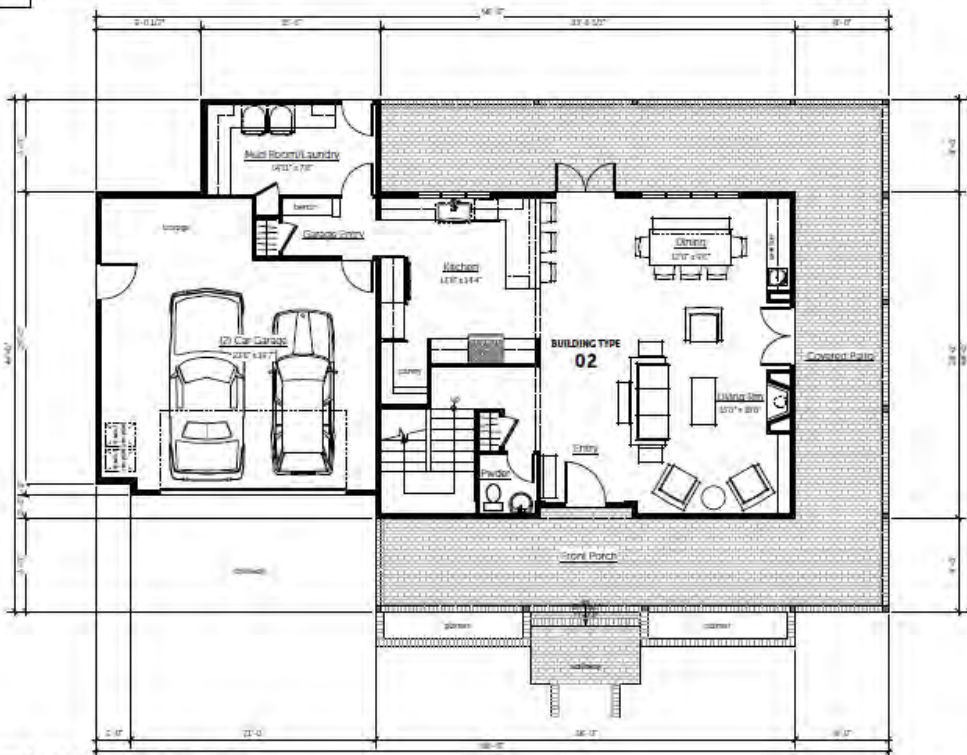
BUILDING AREAS		BUILDING TYPE	
Lower Floor	4,189 sq ft	Living Room	1,189 sq ft
Upper Floor	4,288 sq ft	Bedroom	1,875 sq ft
Total Conditioned	8,477 sq ft	Kitchen	1,064 sq ft
Garage	1,764 sq ft	Covered Front Porch	343 sq ft
Covered Front Porch	343 sq ft	Less Lower Floor Area	4,288 sq ft
Less Lower Floor Area	4,288 sq ft	Total Building Footprint	10,241 sq ft
Total Building Footprint	10,241 sq ft	Notes: #1 in green type, #2 in red type	





2 UPPER FLOOR PLAN

Scale: 3/16" = 1'-0"

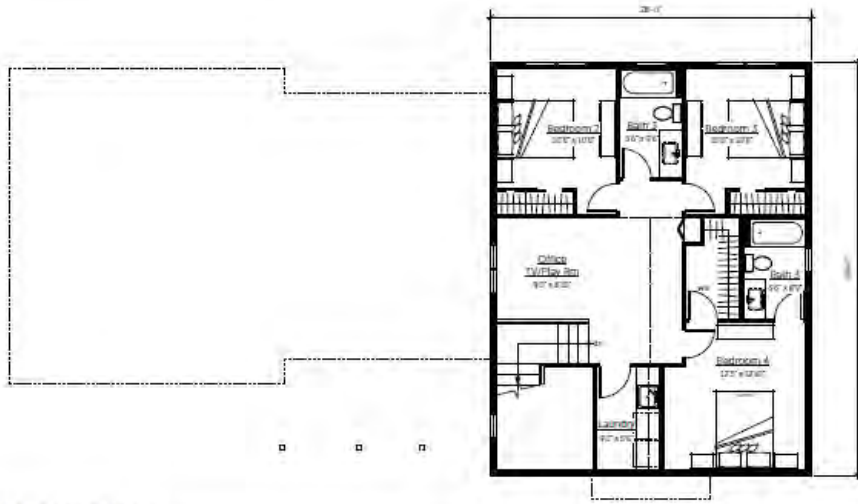


1 LOWER FLOOR PLAN

Scale: 3/16" = 1'-0"

BUILDING AREAS BUILDING TYPE 2		
Lower Floor	1,114 sq ft	
Upper Floor	2,836 sq ft	
Total Constructed	3,950 sq ft	
Garage	274 sq ft	
Covered Porch/Deck	227 sq ft	
Other Covered Floor Area	1,162 sq ft	*shown in blue
Total Building Footprint	5,620 sq ft	100% covered patio





2 UPPER FLOOR PLAN Scale: 3/16" = 1'-0"



1 LOWER FLOOR PLAN Scale: 3/16" = 1'-0"

BUILDING AREAS BUILDING TYPE 1	
Lower Floor	1,942 sq ft
Upper Floor	1,388 sq ft
Total Constructed	3,330 sq ft
Garage	514 sq ft
Covered Porch/Screen	351 sq ft
Gross Lower Floor Area	2,836 sq ft
Total Building Footprint	3,435 sq ft



front elevation

Scale: 3/16" = 1'-0"

California Farmhouse
precedent image

side (left) elevation

Scale: 3/16" = 1'-0"

side (right) elevation

Scale: 3/16" = 1'-0"

back elevation

Scale: 3/16" = 1'-0"

color and materials

	Roof
	Composition Shingle
	Color: Weathered Wood
	Siding
	Fiber Composite "V" Groove Horizontal Siding
	Color: Light Oak
	Wooden Decking
	Color: Light Oak
	Trim
	Cedar Wood
	Color: Weathered Wood
	Painted Decking
	Color: Dark Charcoal
	Accent Siding (if chimney, if present, when occur)
	River Rock
	Color: Red Iron Colors





front elevation

SCALE: 3/16" = 1'-0"



California Colonial

precedent image



side (left) elevation

SCALE: 3/16" = 1'-0"



side (right) elevation

SCALE: 3/16" = 1'-0"



back elevation

SCALE: 3/16" = 1'-0"

color and materials

	Roof
	Composition Shingles
	Color: Off-White
	Siding
	Color: Off-White
	Color: Off-White
	Trim
	Color: Off-White
	Accents
	Color: Dark Charcoal
	Accent Details (to chimney, to pavers, where occur)
	Color: Rustic





front elevation

Scale: 3/16" = 1'-0"

side (left) elevation

Scale: 3/16" = 1'-0"

side (right) elevation

Scale: 3/16" = 1'-0"

back elevation

Scale: 3/16" = 1'-0"

California Colonial

precedent image

color and materials

	Roof
	Composition Shingle
	Color: Charcoal
	Siding
	Fiber Composite "V" Groove Horizontal Siding
	Color: White
	Horizontal Siding
	Color: Dark Green
	Trim
	Painted Wood
	Color: White
	Accent Color Entry/porch doors
	Color: Maroon Red
	Accent Siding 10 channels, 2 panels, white accents
	Color: Rustic



front elevation

SCALE: 3/16" = 1'-0"

side (left) elevation

SCALE: 3/16" = 1'-0"

side (right) elevation

SCALE: 3/16" = 1'-0"

back elevation

SCALE: 3/16" = 1'-0"

color and materials

	Roof
	Aspen Shingles
	Color: Weathered Wood
	Siding
	Four Composite "V" Groove Horizontal Siding
	Color: Rich Red
	Mudboard/Board
	Fourgrove
	Color: Dark Coffee
	Trim
	Four Groove
	Color: Newer Gray
	Accent Color (on garage door)
	Vertical Siding
	Color: French Gray
	Accent Siding (in chimney, 8 panels, where occurs)
	Color: Red Iron Cedar

California Farmhouse
precedent image



EXHIBIT E: COLOR AND MATERIALS BOARD

LOT 1

color and materials

Roof
Composition Shingle
Color: Charcoal

Siding
Fiber Composite "V" Groove Horizontal Siding
Color: White

Windows/Doors
Fiberglass
Color: White

Trim
Painted Wood
Color: White

Accent Color (entry/garage doors)
Painted Wood
Color: Dark Coffee

Accent Stone (in chimney, ½ pavers, where occurs)
Brick
Color: Rustic

LOT 2

color and materials

Roof
Composition Shingle
Color: Weathered Wood

Siding
Fiber Composite "V" Groove Horizontal Siding
Color: Hay Green

Windows/Doors
Fiberglass
Color: Light Ochre

Trim
Painted Wood
Color: Pewter Gray

Accent Color (entry/garage doors)
Painted Wood
Color: Dark Coffee

Accent Stone (in chimney, ½ pavers, where occurs)
Brick
Color: Red (tan color)

LOT 3

color and materials

Roof
Composition Shingle
Color: Charcoal

Siding
Fiber Composite "V" Groove Horizontal Siding
Color: White

Windows/Doors
Fiberglass
Color: Redwood

Trim
Painted Wood
Color: White

Accent Color (entry/garage doors)
Painted Wood
Color: Redwood

Accent Stone (in chimney, ½ pavers, where occurs)
Brick
Color: Rustic

LOT 4

color and materials

Roof
Composition Shingle
Color: Charcoal

Siding
Fiber Composite "V" Groove Horizontal Siding
Color: White

Windows/Doors
Fiberglass
Color: Dark Coffee

Trim
Painted Wood
Color: White

Accent Color (entry/garage doors)
Painted Wood
Color: Indian Red

Accent Stone (in chimney, ½ pavers, where occurs)
Brick
Color: Rustic

LOT 5

color and materials

Roof
Composition Shingle
Color: Weathered Wood

Siding
Fiber Composite "V" Groove Horizontal Siding
Color: Indian Red

Windows/Doors
Fiberglass
Color: Dark Coffee

Trim
Painted Wood
Color: Pewter Gray

Accent Color (entry/garage doors)
Vertical siding
Color: French Grey

Accent Stone (in chimney, ½ pavers, where occurs)
Brick
Color: Red (tan color)

LOT 6

color and materials

Roof
Composition Shingle
Color: Weathered Wood

Siding
Fiber Composite "V" Groove Horizontal Siding
Color: French Grey

Windows/Doors
Fiberglass
Color: Pale Cammie

Trim
Painted Wood
Color: Pewter Gray

Accent Color (entry/garage doors)
Painted Wood
Color: Dark Coffee

Accent Stone (in chimney, ½ pavers, where occurs)
Brick
Color: Red (tan color)



EXHIBIT F: UTILITY PLAN
See Project File for actual plan

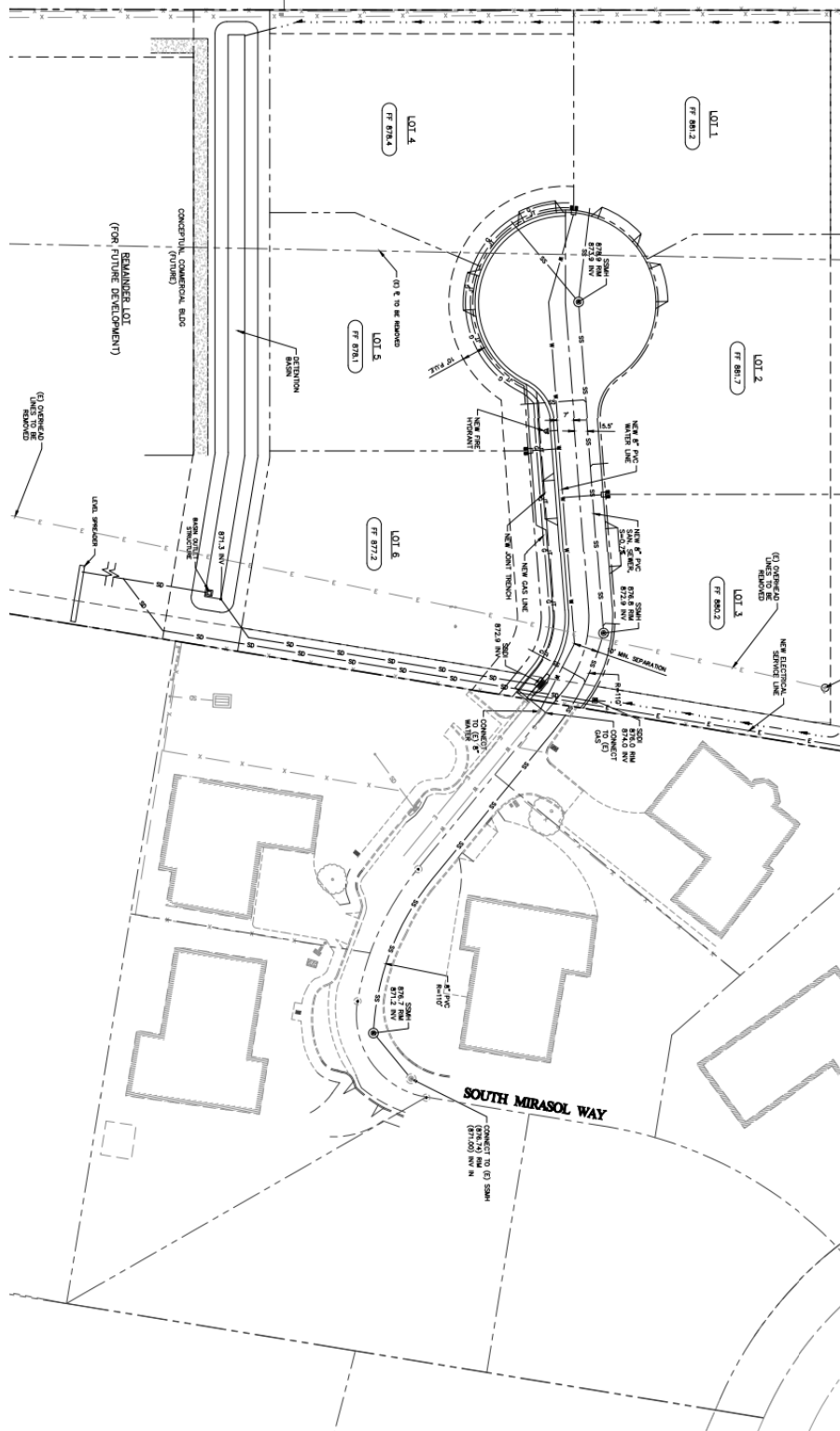
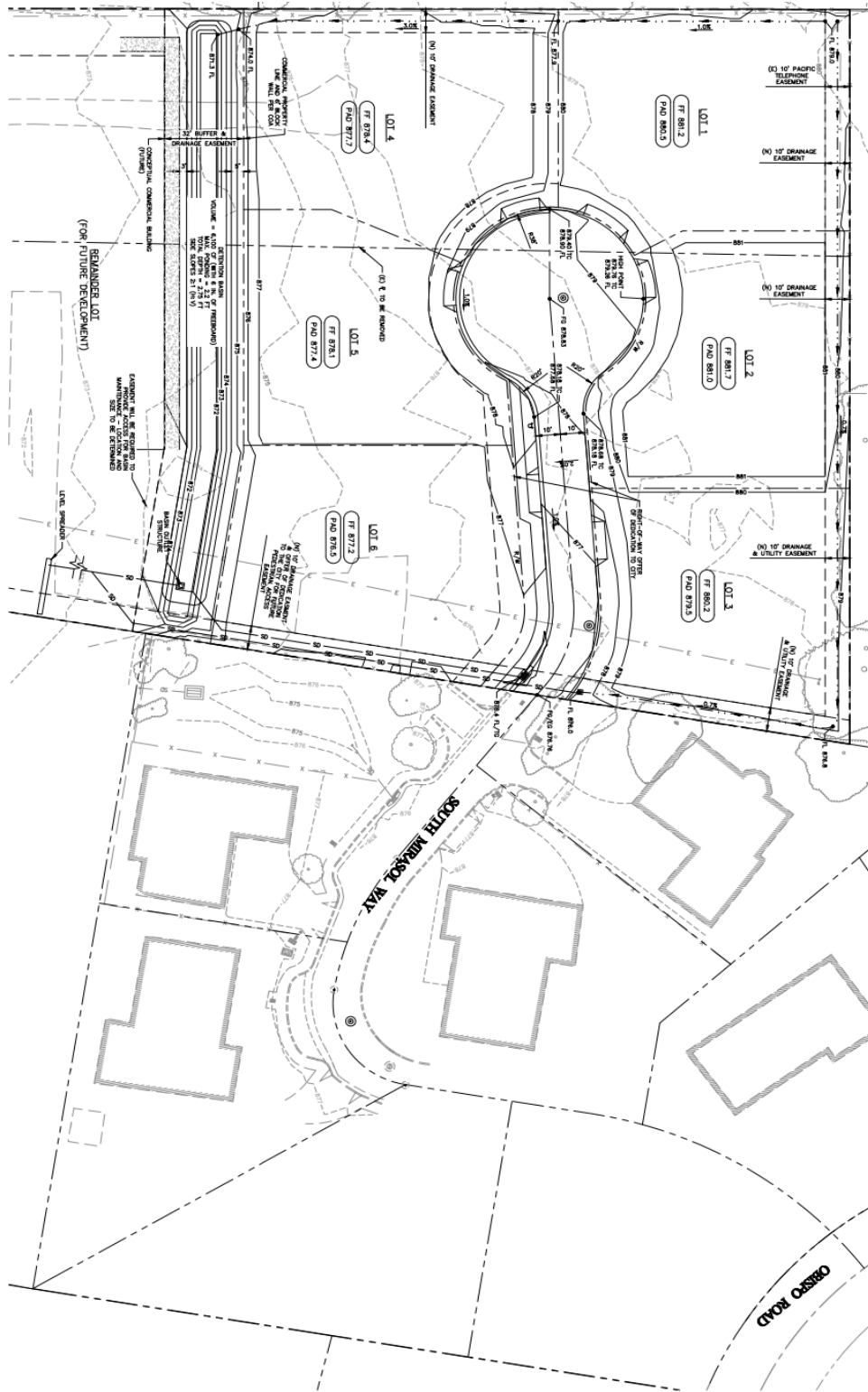


EXHIBIT G – GRADING AND DRAINAGE
See Project File for actual plan



DRAFT PC RESOLUTION 2017-B

**RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF
ATASCADERO, CALIFORNIA, APPROVING A VESTING TENTATIVE
SUBDIVISION MAP FOR TRACT 3104 (6 LOT RESIDENTIAL
SUBDIVISION AND 1 REMAINDER LOT)**

**ANNEX SUBDIVISION
WHELEN / MP ANNEX, LLC
(PLN 2017-1647)**

WHEREAS, an application has been received from Whelen Consulting (PO Box 5021, San Luis Obispo, CA 93401) applicant, and owner, Annex MP, LLC (284 Higuera Street, San Luis Obispo, CA 93401), to consider Planning Application PLN 2017-1647, for a project consisting of a Master Plan of Development, and Testing Tentative Subdivision Map on a 1.67 acre site located on 1827 & 1847 El Camino Real, Atascadero, CA 93422 (Portions of APN 102-032-049, and 020); and

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WHEREAS, The adopted Del Rio Commercial Area Specific Plan (DRCAS) requires approval of a Master Plan of Development in the form of a Conditional Use Permit (CUP) to promote orderly and harmonious development and to enhance the opportunity to best utilize special site characteristics; and

WHEREAS, the laws and regulations relating to the preparation and public notice of environmental documents, as set forth in the State and local guidelines for implementation of the California Environmental Quality Act (CEQA) have been adhered to; and

WHEREAS, a Final Environmental Impact Report, SCH No. 20100051034, Del Rio Road Commercial Area Specific Plan, was certified on June 26, 2012 by the City Council of the City of Atascadero, in accordance with the requirements of the California Environmental Quality Act (CEQA) and the approve project is in substantial conformance with the analyzed; and

WHEREAS, a timely and properly noticed Public Hearing on the subject Vesting Tentative Subdivision Map was held by the Planning Commission of the City of Atascadero at which hearing evidence, oral and documentary, was admitted on behalf of said Subdivision; and

WHEREAS, the Planning Commission of the City of Atascadero, at a duly noticed Public Hearing held on November 21, 2017, studied and considered Vesting Tentative Subdivision Map (Tract 3104) for PLN 2017-1641.



NOW, THEREFORE BE IT RESOLVED by the Planning Commission of the City of Atascadero:

SECTION 1. The Planning Commission makes the following findings, determinations and approvals with respect to the Vesting Tentative Subdivision Map Tract 3104:

A. Findings for Approval of Vesting Tentative Subdivision Map

1. **FINDING:** The proposed subdivision, together with the provisions for its design and improvement, is consistent with the General Plan and the City's Zoning Ordinance (Government Code §§ 66473.5 and 66474(a) and (b)).

FACT: The proposed subdivision has been designed and includes proposed improvements that are consistent with the City's adopted General Plan, in addition to the requirements that have been considered for the adoption of the Master Plan of Development.

2. **FINDING:** The site is physically suitable for the type of development (Government Code § 66474(c)).

FACT: The proposed site is suitable for this type of development as the grades are gently sloping. Based on the existing grades and proposed grading plan, the proposed subdivision is consistent with surrounding neighborhood development.

3. **FINDING:** The site is physically suitable for the proposed density of development (Government Code § 66474(d)).

FACT: The proposed project is consistent with four (4) dwelling units per acre, as specified in the Del Rio Road Commercial Area Specific Plan.

4. **FINDING:** The design of the subdivision or the proposed improvements will not cause substantial environmental damage or substantially and avoidably injure fish or wildlife or their habitat (Government Code § 66474(e)).

FACT: The proposed project has been analyzed in Final Environmental Impact Report, SCH No. 20100051034, Del Rio Road Commercial Area Specific Plan. With the implementation of mitigation measures, in addition to project conditions, the proposed project's impacts satisfy CEQA requirements.

5. **FINDING:** The design of the subdivision or the type of improvements will not cause serious health problems (Government Code § 66474(f)).



FACT: The proposed project has been reviewed for consistency with the threshold established by the City, through adopted Ordinances, and City policies, the proposed project will not cause serious health problems.

6. **FINDING:** The design of the subdivision will not conflict with easements for access through or use of property within the proposed subdivision (Government Code § 66474(g)).

FACT: The installation of public improvements are necessary, prior to recordation of a Final Map, in order to ensure orderly development of the surrounding area (Government Code § 66411.1(b)(2)).

7. **FINDING:** The proposed project or use satisfies all applicable provisions of the Title 9 of the Atascadero Municipal Code this title.

FACT: The proposed project satisfies all applicable provisions of the Atascadero Municipal Code, specifically Planned Development Overlay No.17 as required by the Del Rio Road Commercial Area Specific Plan.

SECTION 2. The Planning Commission of the City of Atascadero, in a regular session assembled on November 21, 2017, resolved to approve Vesting Tentative Subdivision Map (Tract 3104) for PLN 2017-1641, subject to the following, on file in the Community Development Department and incorporated herein by reference:

1. EXHIBIT A: Vesting Tentative Subdivision Map (Tract 3104)
2. EXHIBIT B: Conditions of Approval
3. EXHIBIT C: Grading Plan
4. EXHIBIT D: Utility Plan



On motion by Commissioner _____ and seconded by Commissioner ____ the foregoing resolution is hereby adopted in its entirety by the following roll call vote:

AYES: 0
NOES: 0
ABSTAIN: 0
ABSENT: 0
ADOPTED:

CITY OF ATASCADERO, CA

Duane Anderson
Planning Commission Chairperson

Attest:

Phil Dunsmore
Planning Commission Secretary

t:\- 17 plns\pln 2017-1641 annex subdivision\3.0 staff reports\3.2 pc\11.21.17 pc - annex subdivision.docx



EXHIBIT A - VESTING TENTATIVE Subdivision MAP



Exhibit B
Conditions of Approval



PLN 2017-1647
ANNEX SUBDIVISION
VESTING TENTATIVE SUBDIVISION MAP, TRACT 3104
1827 & 1843 EL CAMINO REAL
PARENT APN 049-102-032, 020 (PORTIONS)

The following conditions of approval apply to the project referenced above. The conditions of approval are grouped under specific headings that relate to the timing of required compliance. Additional language within a condition may further define the timing of the required compliance.

A. General Conditions.

1. All pertinent conditions, with City Council Resolution 2012-050 including the following: PLN
 - Annexation into the City's Community Facilities District prior to recordation of the Final Map;
 - Payment of Affordable Housing In-Lieu Fee at the time of building permit issuance for 1.2 deed restricted moderate housing units;
 - Fair Share payment of US Highway 101 / Del Rio Road interchange fees;
 - Fair Share payment of intersection improvements to Del Rio Road / El Camino Real;
 - All pertinent mitigation measures contained in the Mitigation Monitoring Program for the Del Rio Road Commercial Area Specific Plan.

2. The emergency services and facility maintenance costs listed below shall be 100% funded by the project in perpetuity. The service and maintenance costs shall be funded through a community facilities district established by the City at the developer's cost. The funding mechanism must be in place prior to or concurrently with acceptance of the final maps. The funding mechanism shall be approved by the City Attorney, City Engineer and Administrative Services Director prior to acceptance of any final map. The administration of the above mentioned funds shall be by the City. Developer agrees to participate in the community facilities district and to take all steps reasonably required by the City with regard to the establishment of the district and assessment of the property. PLN
 - All Atascadero Police Department service costs to the project.
 - All Atascadero Fire Department service costs to the project.
 - Off-site common City of Atascadero park facilities maintenance service costs related to the project.

3. Prior to recordation of the Tract Map, the Applicant shall establish a benefit maintenance assessment district, Property Owners' Association, or similar funding mechanism approved by the City, to provide sufficient funds on an PLN



annual basis to pay for the operation, maintenance and future replacement of privately owned-in-common subdivision improvements. The engineer of record shall prepare and submit an estimated operating budget and capital improvement replacement analysis for review and approval by the City Engineer, prior to recordation of the Parcel Map.

4. Prior to recording the Final Map, the Applicant shall have the map reviewed by the public utility providers for power, telephone, gas, cable TV, and the Atascadero Mutual Water Company. The Applicant shall obtain a letter from each utility company stating that the easements and rights-of-way shown on the map for public utility purposes are acceptable. PWD
5. Documents that the City of Atascadero requires to be recorded concurrently with the Final Map (e.g.: off-site rights-of-way dedications, easements not shown on the map, agreements, etc.) shall be listed on the certificate sheet of the map. PWD
6. The City of Atascadero may require an additional map sheet for information purposes in accordance with the Subdivision Map Act. PWD
7. Public improvement plans (PIPs) shall be prepared by a licensed civil engineer. PIPs shall be prepared on 24"x36" plan sheets and in accordance with Section 2 of City Standard Specifications. PWD
8. The Applicant is responsible for all rights-of-way acquisitions and associated costs. PWD
9. Roadway signing and striping shall be in accordance with the California Manual on Uniform Traffic Control Devices (CA-MUTCD) and shall be located to provide adequate warning of the traffic calming improvements associated with speed reduction. PWD
10. Improvements involving wetlands, waterways, watercourses, sensitive habitat, etc., may require the review and approval of other agencies (e.g. – Army Corp of Engineers, State Fish & Wildlife, State Water Resources, etc.). If additional permits are required from other agencies, said permits shall be issued prior to the approval of the subdivision improvement plans PWD
11. All plans shall contain the City of Atascadero "Standard Notes for Improvement Plans" on file in the City Engineer's office. PWD

B. Off-Site Road Improvements

12. The Subdivider shall overlay or reconstruct that portion of the existing South Mirasol Way affected by the extension of the sewer main, to the satisfaction of the City Engineer PWD



C. On-Site Roadways

13. The horizontal and vertical design of roads shall be in compliance with the City of Atascadero Engineering Standards and Standard Specifications, to the satisfaction of the City Engineer. The City Engineer reserves the right to make modifications to all submitted road designs, when in the opinion of the City Engineer, the public's health and safety is benefitted. PWD

14. The design of structural pavement sections shall be based on a Traffic Index (TI) = 5.5 and a 50-year design life. PWD

New roads with pavement placed prior to the construction of buildings will be subjected to additional construction traffic and wear associated with the on-site construction not included in the design life of the pavement section. Therefore to off-set this, the AC thickness shall be increased from that which is derived from Caltrans method by either:

- 1" if the pavement is placed prior to building construction (not phased).
- 1.5" if the pavement construction is phased (i.e. – a portion of the ultimate pavement thickness is deferred and a final pavement cap placed prior to final inspection). Final pavement cap shall not be less than 1.5".

15. Street centerline monuments shall be provided at intersections and at the beginning and end of curves along the street centerline PWD

16. Roadway signing, striping, and traffic signal modifications shall be in accordance with the California Manual on Uniform Traffic Control Devices (CA-MUTCD) and shall be designed and constructed to the satisfaction of the City Engineer. PWD

17. Prior to recordation of the Final Map, the Applicant shall establish a benefit maintenance assessment district, or similar funding mechanism approved by the City, to provide sufficient funds on an annual basis to pay for the operation, maintenance and future replacement of the internal road system serving Tract 3104 including but not limited to: PWD

- Pavement, pavement seals, aggregate base, road frontage improvements
- Striping, signage, street furniture
- Drainage facilities, detention basins, retention basins, bio-swales, & storm water treatment/control measures
- Maintenance of slopes containing the road prism
- Prior to recordation of the Final Map, the Applicant's engineer shall prepare and submit an estimated operating budget and capital replacement analysis for review and approval by the City Engineer.

18. Pavement and base sections shall be designed and constructed in accordance with the City of Atascadero Engineering Standards and PWD



Standard Specifications. When said standards and specifications are not clear, lack necessary details, or are silent, the minimum standard shall be based upon the current edition of the San Luis Obispo County Public Improvement Standards or Caltrans Standard Drawings and Standard Specifications, as determined by the City Engineer.

D. Grading

19. Where cut/fill slopes are visible from a roadway, slopes shall be contour graded to eliminate angular daylight/catch points and shall blend into the adjacent topography to the extent feasible, to the satisfaction of the City Engineer. PWD
20. All finish graded areas shall be landscaped or revegetated using a native seed mixture and shall demonstrate at least 70% ground coverage before final inspection. PWD

E. Stormwater

21. A Storm Water Pollution Prevention Plan (SWPPP) is required prior to any ground disturbing activities. The WDID number provided upon acceptance of the SWPPP into the State's SMARTS system registration shall be noted on the Title Sheet of the Public Improvement Plans. PWD
22. Common drainage basins (serving multiple lots) shall be designed to desilt, detain and meter storm flows in accordance with City regulations. PWD
23. Storm Sewer shall be of either cast-in-place or precast reinforced concrete pipe, polyvinyl chloride pipe, high density polyethylene pipe or an approved equal. PWD
- Minimum pipe diameter allowable on any storm drain within a roadway or road right-of-way shall be 18" diameter. A lesser size may be used for down drains on fill slopes if approved by the City Engineer.
 - Minimum design velocity in closed conduits shall be 2 f.p.s. when conduit is flowing to capacity and should not exceed 15 f.p.s.
 - Closed conduits shall be designed to convey the 10-year storm flow with gravity flow, the 25-year storm flow with head, and provide a safe overland route for the conveyance for the 100-year storm overflow.
 - Storm Sewer Manholes shall be located at junction points, changes in gradient, and changes in conduit size to the satisfaction of the City Engineer. On curved pipes with radii of 200' to 400', manholes shall be placed at the BC or EC of the curve and on 300' maximum intervals along the curve. On curves with radii exceeding 400', manholes shall be placed at the BC or EC of the curve and on 400' maximum intervals along the curve for pipes 24" and less in diameter and 500' maximum intervals along the curve for pipes greater than 24" in diameter. Curves with radii less than 200' will be handled on an individual basis and approved by the City Engineer.



- Spacing of manholes or inlets, of such size as to be enterable for maintenance, shall not exceed 500' for drains 24" and smaller diameter and 600' for pipes greater than 24" in diameter, except under special conditions as approved by the City Engineer. The spacing of manholes shall be nearly equal wherever possible.
- Manholes or junction boxes, entry to which does not fall in the gutter line, must have standard 24" diameter manhole covers. Those falling in the gutter line may use the standard grated manhole cover and serve also as an inlet manhole.
- Storm Drain Inlets shall be spaced so that gutter flow does not exceed a depth of 6" at the face of the curb for a 10-year storm and so that a 25-year storm flow will not cause damage and can be contained within the right-of-way.

24. Prior to approval of subdivision improvement plans, the project engineer shall provide calculations and a report confirming compliance with City regulations and the low-impact development requirements contained in the previously approved EIR and Master Plan of Development for the Walmart/Annex development. PWD
25. When a Storm Water Operation - Maintenance Plan (OMP) is required for an individual lot or group of lots, the City Engineer can require the OMP to be recorded as an agreement against each property. When recordation of an OMP agreement is required, the agreement shall contain provisions allowing the City to enter the property, following reasonable notice to the property owner or tenant, to conduct an inspection of the storm water system and post-construction storm water management controls. PWD
26. When an OMP is required, the OMP shall include a section addressing annual inspection and reporting to the City by a third party, to the satisfaction of the City Engineer. PWD
27. Where storm water is concentrated as a result of new improvements, the drainage shall be conveyed in a non-erosive, controlled condition to an adequate point of discharge, to the satisfaction of the City Engineer. Where concentrated drainage from new improvements cannot be avoided and crosses more than one property, appropriate easements may be required. PWD
28. All stormwater management improvements to be owned or managed by the HOA shall be identified in an Operation and Maintenance Plan/Agreement (OMP) and shall be recorded concurrently with the Final Map. The OMP shall include a financial plan addressing annual and long-term maintenance as well as replacement. Specific requirements for stormwater management may be required to be identified on an additional Final Map information sheet. PWD

F. Flood Control Basins

29. Flood control basins are utilized in the City of Atascadero, as determined PWD



appropriate depending upon site conditions: Retention basins, Detention basins, and Subsurface Infiltration Basins. In all cases, the Project Engineer shall provide evidence that the basin will completely drain within seven (7) days to the satisfaction of the City Engineer.

Retention Basin. Any drainage basin which is used as a terminal disposal facility shall be classified as a retention basin.

- a. Basin Capacity. The basin capacity is to be based on the theoretical runoff from a 50-year storm, 10-hour intensity for 10-hour duration. No reduction in required capacity shall be given for soil percolation rates.
- b. Percolation Test Required. A minimum of 3 percolation tests per basin shall be submitted to the City Engineer for review and approval prior to approval of the plans. The project engineer shall submit calculations and a report demonstrating the basin will drain within seven-days of a single storm event as noted above. Deep soil borings may be required in areas where there is concern of shallow depth to groundwater or bedrock. Percolation tests shall be performed at depths below the basin bottom.

Detention Basin. Any drainage basin which has a downstream outlet designed to meter the outflow shall be classified as a detention basin. Basin capacity shall be based on receiving the runoff from a 50-year storm with the watershed in its fully-developed condition, and releasing the flow equivalent to the runoff from a 2-year storm with the project site in its pre-development condition. The outlet shall release water in a non-erosive manner.

Subsurface Infiltration Basins. Subsurface basins may be used for either retention or detention of site runoff, where their application is suitable for project conditions. Subsurface basins shall be limited to locations where the depth to seasonally high groundwater is greater than 10-feet below the deepest portion of the basin.

Drain Rock. Drain rock shall be clean, crushed granite (or clean, angular rock of similar approved hardness) with rock size ranging from 1-1/2-inch to 3/4-inch. Rock gradation shall conform to the Specification of ASTM C-33 #4.

Operational Requirements.

- i. Water quality of inflow (both sediment and chemical loading) may require pretreatment or separation
- ii. Maintenance plan, including provisions for vehicular access and confined-space entry safety requirements, where applicable
- iii. A safe overflow path shall be identified on the plan and may require easements



Easement Requirements. All drainage basins accepting runoff from roads, streets or other common ownership areas shall be located in an easement offered for dedication to the public. Reversionary clauses shall not be permitted. If a fence is required it shall be located not more than 4-inches inside the drainage easement line, except where setbacks are required as part of the land use permit or by the Land Use Ordinance.

Overflow Path Required. The design of all drainage basins shall identify the designated route for overflow. The Project Engineer shall design the overflow path so that the flow in a 100- year storm is non-erosive and will not damage downstream improvements, including other basins. Easements may be required for concentrated flows across multiple properties.

G. Water Distribution System

30. The Applicant shall extend the water distribution system to the satisfaction of the Atascadero Mutual Water Company (AMWC) and City Engineer. PWD
31. The water system shall include easements outside of the road rights-of-way for water system facilities as required by the AMWC and to the satisfaction of the City Engineer PWD
32. Each lot shall be served with a separate water lateral and meter in accordance with the AMWC requirements. PWD
33. Where the water distribution system requires an above ground reduced pressure unit, pressure booster station or other significant above ground facility, said facility shall be located in an easement contiguous to the road right-of-way and shall include visual screening, to the satisfaction of the AMWC, Community Development Director, and City Engineer. PWD
34. Fire hydrant locations shall be to the satisfaction of the City Fire Marshall. PWD
35. Properties and/or areas that are managed or owned by the HOA shall be metered separately to the satisfaction of the AMWC. PWD

H. Wastewater Collection System

36. The gravity sewer system serving Tract 3104 and extending off-site shall be offered to the public including easements necessary for the operation, maintenance and replacement of the system. The wastewater collection system shall be designed and constructed in accordance with City standards and specifications, to the satisfaction of the City Engineer. PWD
37. Prior to recordation of the Final Map, the Applicant shall establish a benefit maintenance assessment district, or similar funding mechanism approved by the City, to provide sufficient funds on an annual basis to pay for the operation, maintenance and future replacement of the wastewater collection system serving Tract 3104 (both on-site and the off-site extension). The engineer of record shall prepare and submit an estimated operating budget and capitol replacement analysis for review and approval by the City Engineer, prior to recordation of the Final Map. PWD



38. Gravity sanitary sewer (SS) mains shall terminate in manholes. PWD
39. Gravity SS mains shall be a minimum of eight (8) inches in diameter. PWD
40. Each lot served by the wastewater collection system shall pay all sewer fees prior to the issuance of a building permit. PWD
41. The on-site sewer laterals shall be privately owned and maintained by individual lot owners. PWD

I. Easements

42. A 6-foot wide Public Utility Easement (PUE) shall be dedicated contiguous to the road rights-of-way. PWD
43. Road slope easements shall be dedicated where the road prism cut/fill slopes extend beyond the right-of-way. The easement shall extend not less than five feet (horizontally) beyond any daylight or catch line of the graded slope or other required road facility (such as a brow ditch, retaining wall, drainage swale, etc.), to the satisfaction of the City Engineer. PWD
44. Driveways serving more than one lot may require an easement for ingress/egress, public & private utilities, and drainage, to the satisfaction of the City Engineer. PWD
45. Drainage easements: PWD
- Easements shall be dedicated over areas containing drainage improvements that benefit or serve more than one property. The determination as to whether the easement is private or offered to the public will be determined by the City Engineer prior to approval of the subdivision improvement plan and Final Map.
 - Concentrated cross-lot drainage shall be avoided where possible; when required, concentrated drainage shall be conveyed via appropriate easements, to the satisfaction of the City Engineer.
 - Development on any lot that blocks or changes a natural drainage course may be required to provide an easement for the benefit of upstream tributary properties to an adequate point of discharge, to the satisfaction of the City Engineer.
46. Wherever an easement is created for commonly owned or operated improvements for the benefit of more than one lot, there shall also be created a maintenance and operations agreement, to the satisfaction of the City Engineer and City Attorney. PWD
47. Easements that are not intended to continue in perpetuity shall not be shown on the Final Map and shall be recorded by separate instrument.

J. Utilities

48. New utility distribution systems and services shall be constructed underground, to the satisfaction of the City Engineer. PWD



49. Each lot shall be served with separate services for water, sewer, gas, PWD power, telephone and cable TV. Utility laterals shall be located and constructed to each lot in accordance with City Standards and Standard Specifications

****END CONDITIONS****



EXHIBIT D - UTILITY PLAN



Planning Commission Exhibit 1 - La Plaza Design Package



LA PLAZA MIXED-USE

PROJECT DESCRIPTION

THE PROJECT CONSISTS OF A 3 STORY MIXED USE BUILDING, INCLUDING RETAIL ON THE GROUND FLOOR AND OFFICES/RESIDENTIAL UNITS ABOVE. EL CAMINO STREET LINED WITH PEDESTRIAN FRIENDLY INVITING STOREFRONT DESIGN.

VICINITY MAP



PROJECT DIRECTORY

OWNER: MICHAEL ZAPPAS
8189 SAN DIMAS LN
ATASCADERO, CA 93422

ARCHITECT: RRM DESIGN GROUP
3765 S.HIGUERA STREET, SUITE 102
SAN LUIS OBISPO, CA 93401
CONTACT: PAT BLOTE
PHONE: (805) 543-1794
EMAIL: PLBLOTE@RRMDESIGN.COM

PROJECT ADDRESS: 6500 EL CAMINO REAL
ATASCADERO, CA 93422

APN NUMBERS: 030-193-001; 003; 017; 016; 031; 033

PROJECT STATISTICS

ZONING: DC (DOWNTOWN COMMERCIAL)

PARCEL SIZE: 1.83 ACRES (79,541 SF)

PROPOSED COVERAGE: 30,000 SF (38%)

MIXED OCCUPANCY: MIXED USE

PROPOSED RETAIL: 18,000 SF

PROPOSED OFFICE: 9,700 SF

PROPOSED RESIDENTIAL: (32) 1 BD UNIT
(8) 2 BD UNITS

UNIT TYPE	SQ. FT.
1 BD (PLAN 1A)	650 SF
1 BD (PLAN 1B)	650 SF
1 BD (PLAN 1C)	750 SF
2 BD (PLAN 2B)	900 SF

MAX HEIGHT ALLOWED: 45 FT.
MAX. PROPOSED HEIGHT: 45 FT.

DENSITY CALCULATIONS

UNITS:
(32) 1B UNITS
(8) 2B UNITS

PER MUNICIPAL CODE SECTION 9.3.173 (F), A DENSITY BONUS MAY BE GRANTED, SUBJECT TO APPROVAL BY THE CITY COUNCIL THROUGH A MASTER PLAN OF DEVELOPMENT (CUP), CONSISTENT WITH SECTIONS 9.3.801 THROUGH 9.3.806. IN ADDITION, GENERAL PLAN POLICY 2.1 SUPPORTS APPROVAL OF A 15% DENSITY BONUS THROUGH THE CUP UPON A FINDING OF SUPERIOR DESIGN.

ALLOWED DENSITY: 20 UNITS/ACRE
CALCULATION: 20X1.83= 36.6
15% DENSITY BONUS: 36.6X0.15= 42.09
PROPOSED DENSITY: 40 UNITS

PARKING

PARKING REQUIREMENTS

USE TYPE	SQUARE FEET/# UNITS	PARKING REQ.	SPACES REQ.
COMMERCIAL	20,000 SF	N/A	N/A
1 BD	32 UNITS	1.5 SPACES/UNIT	48 SPACES
2 BD	8 UNITS	2 SPACES/UNIT	16 SPACES
GUEST PARKING	40 UNITS	1 SPACE/5 UNITS	8 SPACES
TOTAL			72 SPACES REQ.

PARKING PROVIDED

57 SURFACE
19 GARAGES
1 ADA GARAGE
4 ADA SURFACE
3 TUCK UNDER SPACES (1 ADA)

TOTAL SPACES PROVIDED: 84 SPACES PROVIDED

SHEET INDEX

- T1 TITLE SHEET
- A1 EXISTING SITE SURVEY
- A2 PROPOSED SITE PLAN
- A3 GROUND FLOOR PLAN
- A4 MID LEVEL AND SECOND FLOOR PLAN
- A5 THIRD FLOOR
- A6 ELEVATIONS AND PERSPECTIVE
- A7 ELEVATIONS AND PERSPECTIVE
- A8 SITE SECTIONS
- A9 SITE ACCESSIBILITY PLAN
- A10 COLORS AND MATERIAL BOARD
- C1 EXISTING CONDITIONS/SITE SEPARATION
- C2 SITE IMPROVEMENTS
- C3 CIVIL GRADING AND DRAINAGE PLAN
- C4 STORM WATER MANAGEMENT
- L1 CONCEPTUAL LANDSCAPE PLAN
- L2 TREE REMOVAL AND REPLACEMENT PLAN
- L3 LANDSCAPE MATERIALS
- TPM TENTATIVE PARCEL MAP

SEPT. 28, 2017
0725-01-RS16



T1

TOPOGRAPHIC SURVEY

BEING A TOPOGRAPHIC SURVEY OF A PORTION OF LOT 18 & 19 OF BLOCK 118 OF ATASCADERO ACCORDING TO THE MAP THEREOF RECORDED IN BOOK 4 OF MAPS PAGE 1 ET SEQ. IN THE CITY OF ATASCADERO, COUNTY OF SAN LUIS OBispo, STATE OF CALIFORNIA.

JANUARY 2017

SURVEYORS STATEMENT:

THIS TOPOGRAPHIC SURVEY WAS PERFORMED BY ME OR UNDER MY DIRECTION.

BENCH MARK:

VERTICAL DATUM FOR THE SURVEY IS NAVD 88 AND WAS ESTABLISHED FROM THE NATIONAL GEODETIC SURVEY CORS STATION DESIGNATION LOSOSOR_CS2004 GRP. PID DN5661 AND USED TO ESTABLISH A TEMPORARY BENCHMARK ON A NAIL AND TAG STAMPED KCE 4994 AS SHOWN ON 37 PARCEL MAPS 42 AND FOUND ON THE SOUTHEAST CORNER OF EL CAMINO REAL AND ATASCADERO WAY HAVING AN ELEVATION OF 552.42.

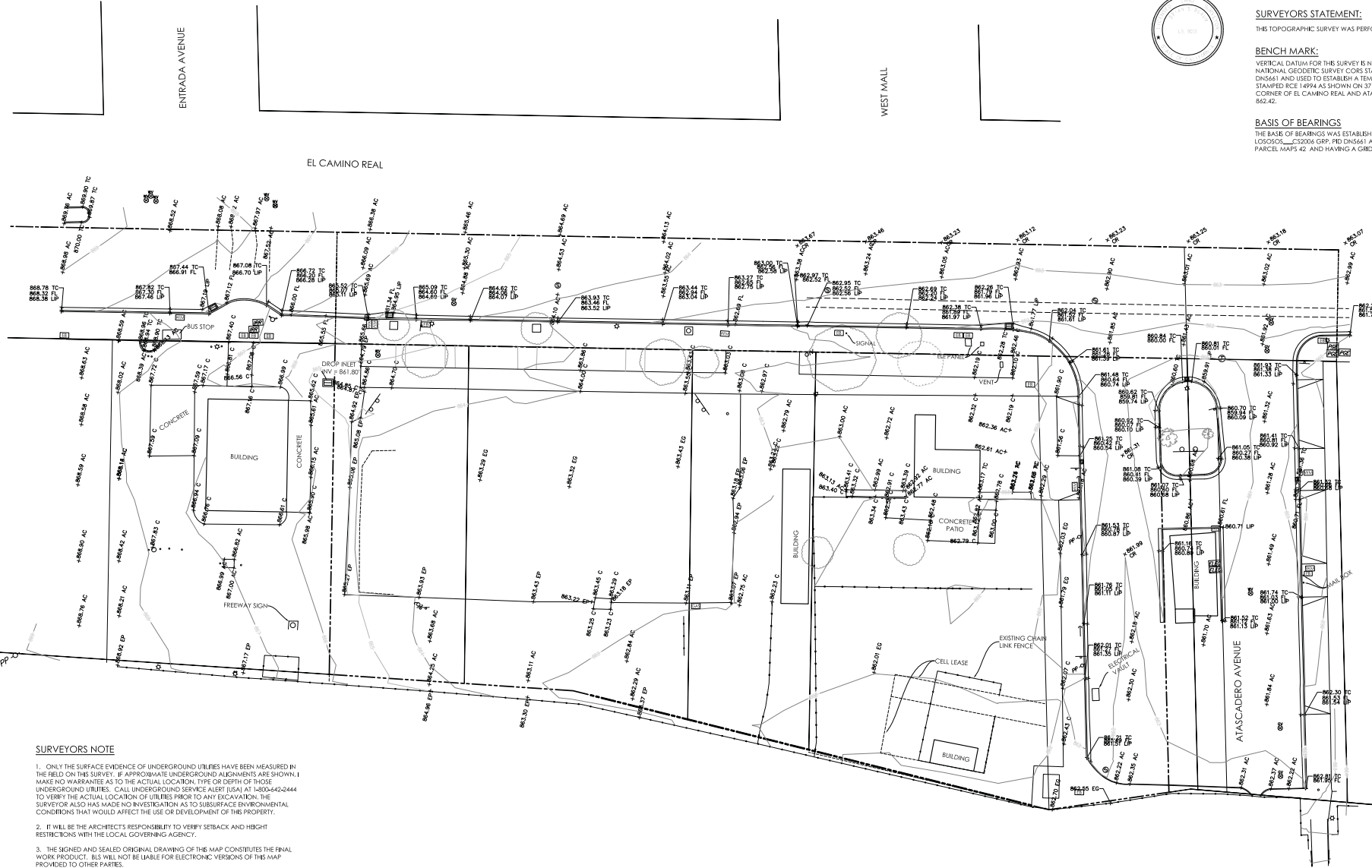
BASIS OF BEARINGS

THE BASIS OF BEARINGS WAS ESTABLISHED BY GPS OBSERVATION BETWEEN CORS STATION LOSOSOR_CS2006 GRP. PID DN5661 AND THE NORTHWEST CORNER OF PARCEL ONE OF 37 PARCEL MAPS 42 AND HAVING A GRID BEARING OF



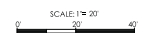
LEGEND

	WATER METER
	PG&E BOX
	AT&T BOX
	FIRE HYDRANT
	TELEPHONE
	PULL BOX OR EVIDENCE THEREOF
	SIGN
	UTILITY POLE
	ELECTRICAL BOX
	WATER VALVE
	BOLLARD
	LIGHT
	GAS VALVE
	GUY
	FIRE HYDRANT
	TELEPHONE MANHOLE
	SEWER MANHOLE



SURVEYORS NOTE

1. ONLY THE SURFACE EVIDENCE OF UNDERGROUND UTILITIES HAVE BEEN MEASURED IN THE FIELD ON THIS SURVEY. IF APPROXIMATE UNDERGROUND ALIGNMENTS ARE SHOWN, I MAKE NO WARRANTIES AS TO THE ACTUAL LOCATION, TYPE OR DEPTH OF THOSE UNDERGROUND UTILITIES. CALL UNDERGROUND SERVICES ALERT (USA) AT 1-800-442-2444 TO VERIFY THE ACTUAL LOCATION OF UTILITIES PRIOR TO ANY EXCAVATION. THE SURVEYOR ALSO HAS MADE NO INVESTIGATION AS TO SUBSURFACE ENVIRONMENTAL CONDITIONS THAT WOULD AFFECT THE USE OR DEVELOPMENT OF THIS PROPERTY.
2. IT WILL BE THE ARCHITECT'S RESPONSIBILITY TO VERIFY SETBACK AND HEIGHT RESTRICTIONS WITH THE LOCAL GOVERNING AGENCY.
3. THE SIGNED AND SEALED ORIGINAL DRAWING OF THIS MAP CONSTITUTES THE FINAL WORK PRODUCT. BLS WILL NOT BE LIABLE FOR ELECTRONIC VERSIONS OF THIS MAP PROVIDED TO OTHER PARTIES.
4. THE BOUNDARY LINES SHOWN HEREON WERE COMPILED FROM RECORD INFORMATION.



LA PLAZA MIXED-USE

EXISTING SITE SURVEY

SCALE: 1" = 20'-0" (24X36 sheet)

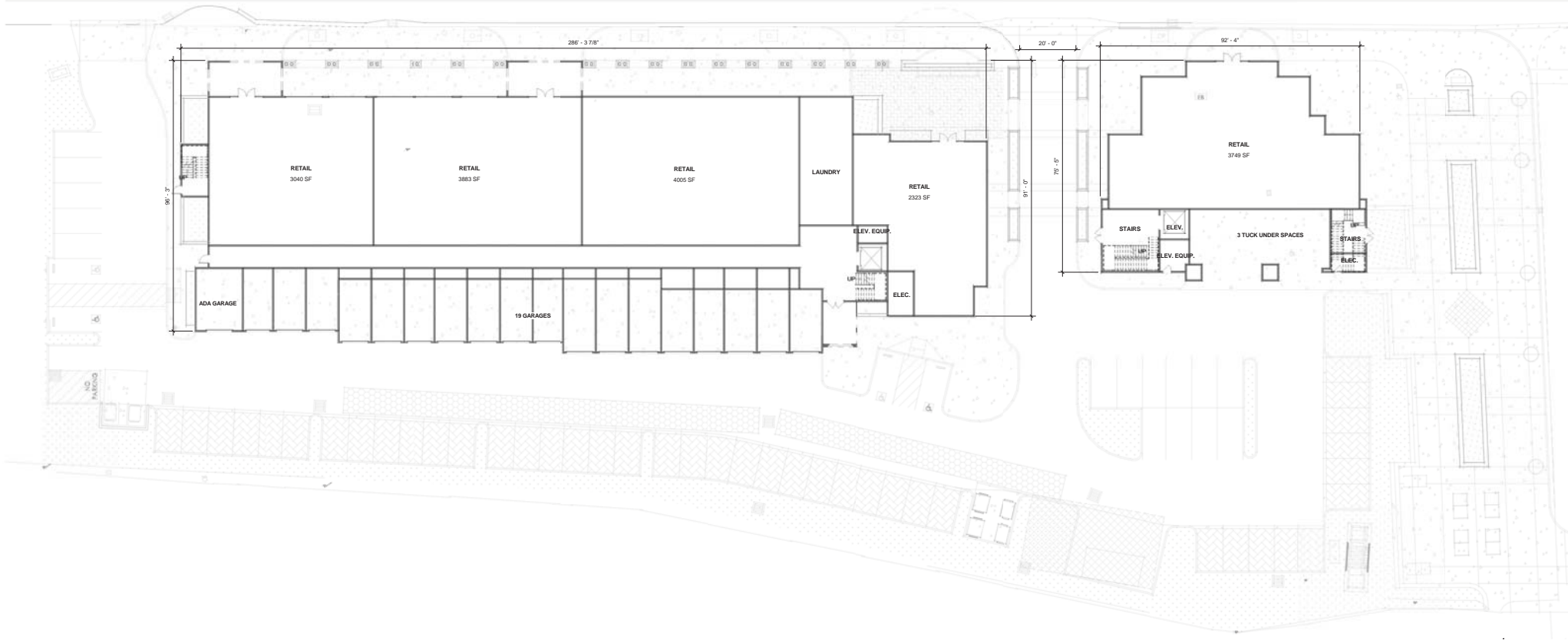


SEPT. 28, 2017
0725-01-RS16



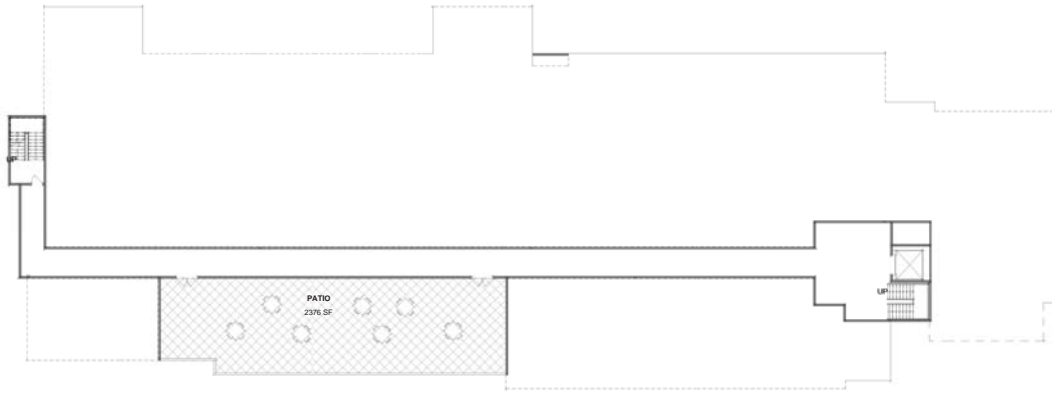
A1

EL CAMINO REAL



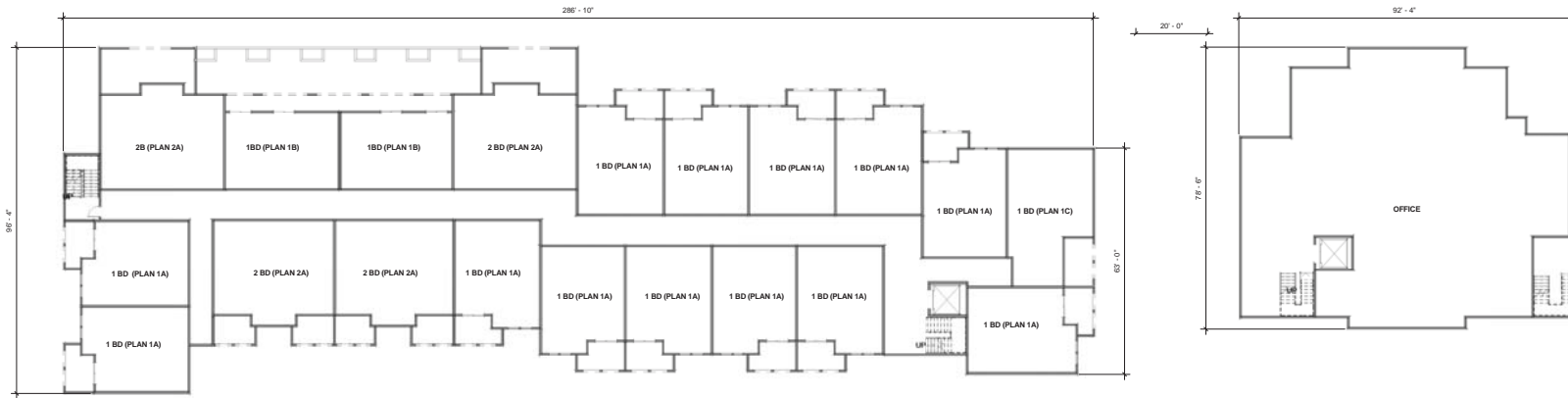
1 GROUND FLOOR PLAN
1/16" = 1'-0" (24 x 36 SHEET)





1 MID LEVEL AND EXTERIOR PATIO FLOOR PLAN

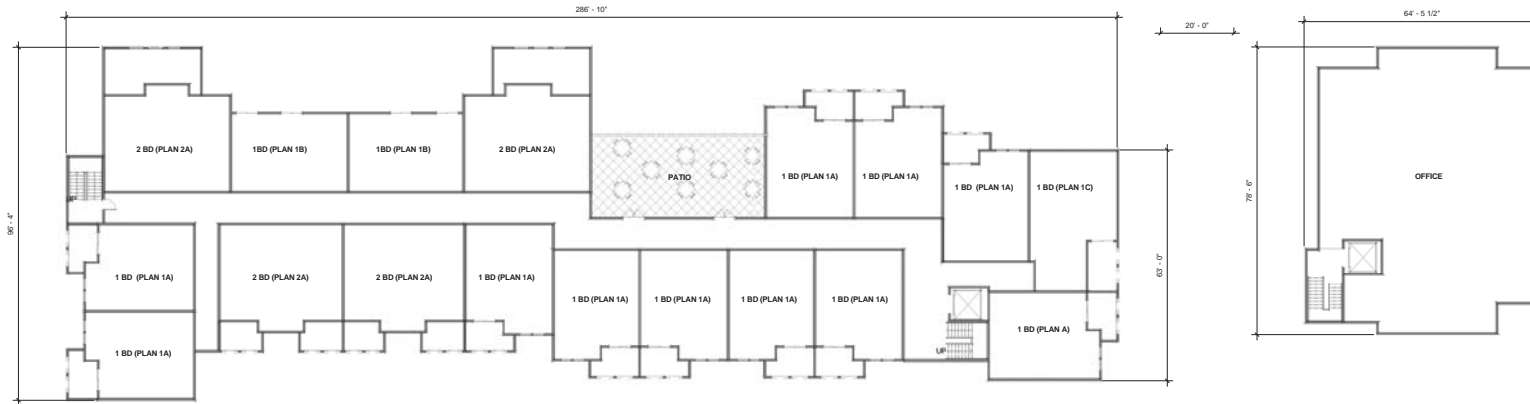
1/16" = 1'-0" (24 x 36 SHEET)



2 SECOND FLOOR PLAN

1/16" = 1'-0" (24 x 36 SHEET)





1 THIRD FLOOR PLAN
 1/16" = 1'-0" (24 x 36 SHEET)





BUILDING B

BUILDING A

① **EAST ELEVATION**
1/16" = 1'-0" (24 x 36 SHEET)



② **BUILDING A PERSPECTIVE**



③ **NORTH ELEVATION BUILDING A**
1/16" = 1'-0" (24 x 36 SHEET)



④ **SOUTH ELEVATION BUILDING A**
1/16" = 1'-0" (24 x 36 SHEET)



BUILDING A

BUILDING B

1 WEST ELEVATION
1/16" = 1'-0" (24 x 36 SHEET)



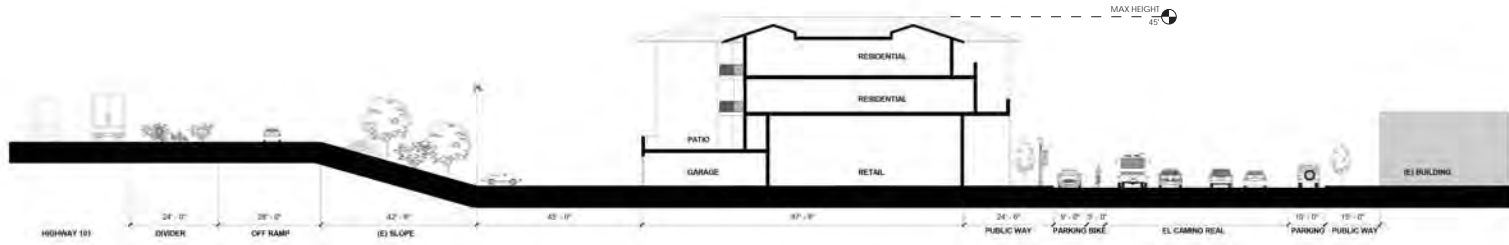
2 BUILDING B PERSPECTIVE



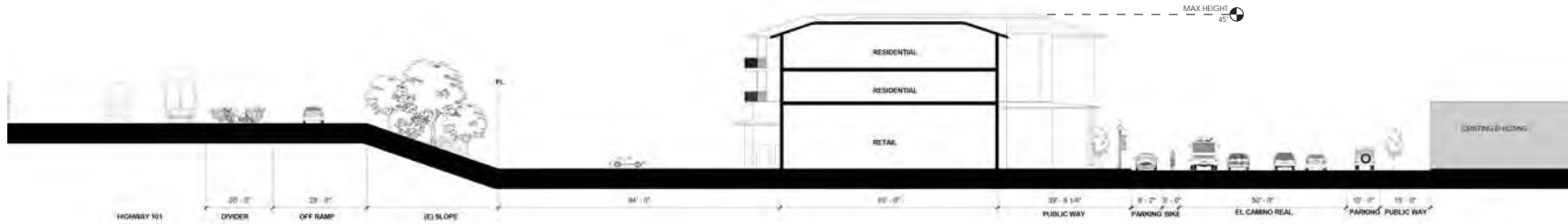
3 NORTH ELEVATION BUILDING B
1/16" = 1'-0" (24 x 36 SHEET)



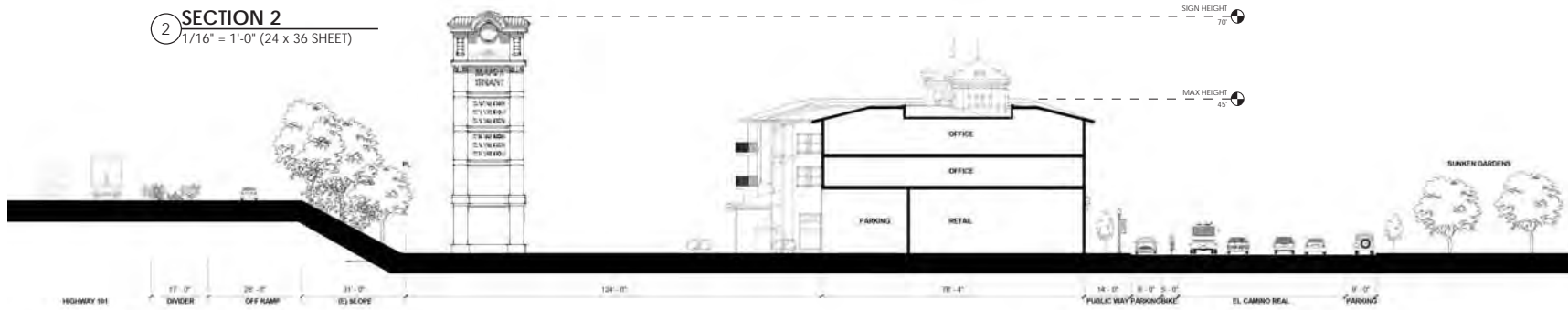
4 SOUTH ELEVATION BUILDING B
1/16" = 1'-0" (24 x 36 SHEET)



1 SECTION 1
1/16" = 1'-0" (24 x 36 SHEET)

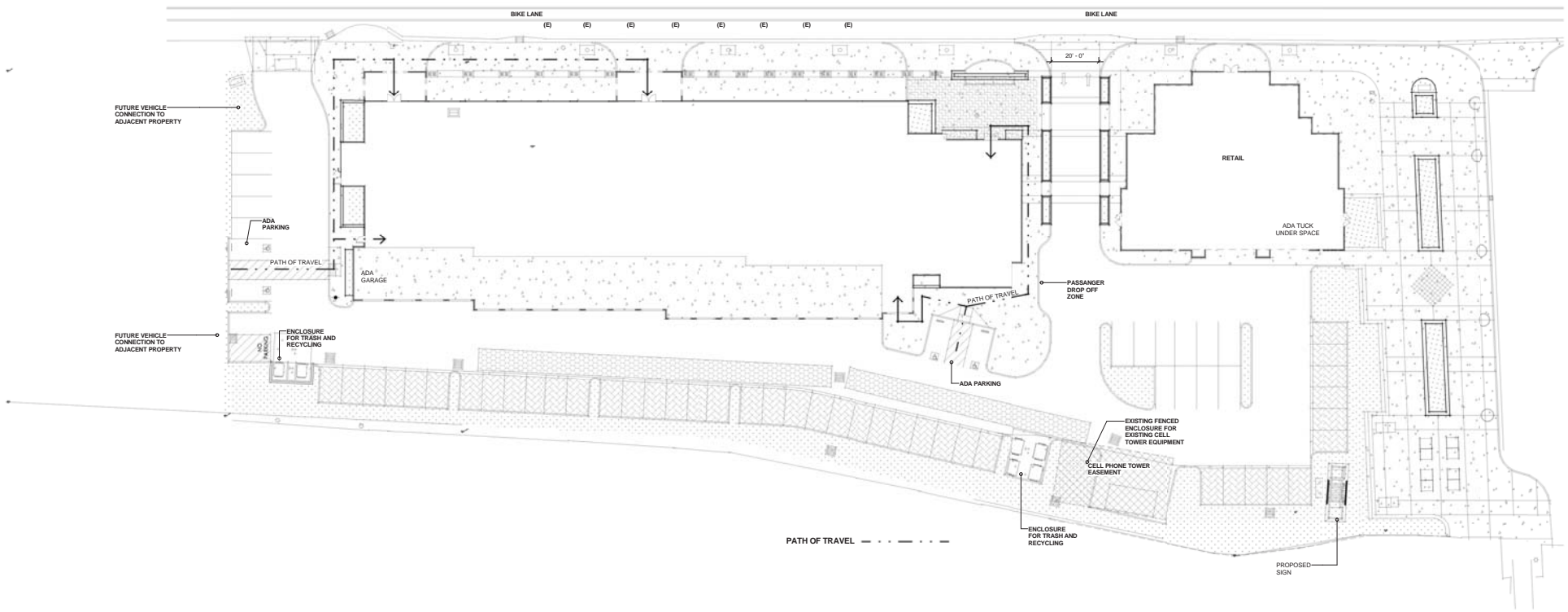


2 SECTION 2
1/16" = 1'-0" (24 x 36 SHEET)



3 SECTION 3
1/16" = 1'-0" (24 x 36 SHEET)





Eagle Roof Tile
Capistrano SMC 8820
Santa Clara Blend



Pacific Clay Thin Brick
Atascadero Dulce
(same color as City Hall)



Precast GFRC Trim + Base
Veristone-J. Ginger in Silverstone



Precast GFRC Concrete Panels
Veristone-J. Ginger
Sandalwood accents on Cornsilk base



Painted Stucco Accent Color
Dunn Edwards DE6376
Looking Glass



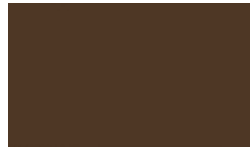
Porcelain Tile Base Accent
American Olean - Method
MT05 Khaki Approach, polished



Painted Stucco Field Color
Dunn Edwards DE6206
Desert Suede



Aluminum Doors and Windows
Medium Bronze



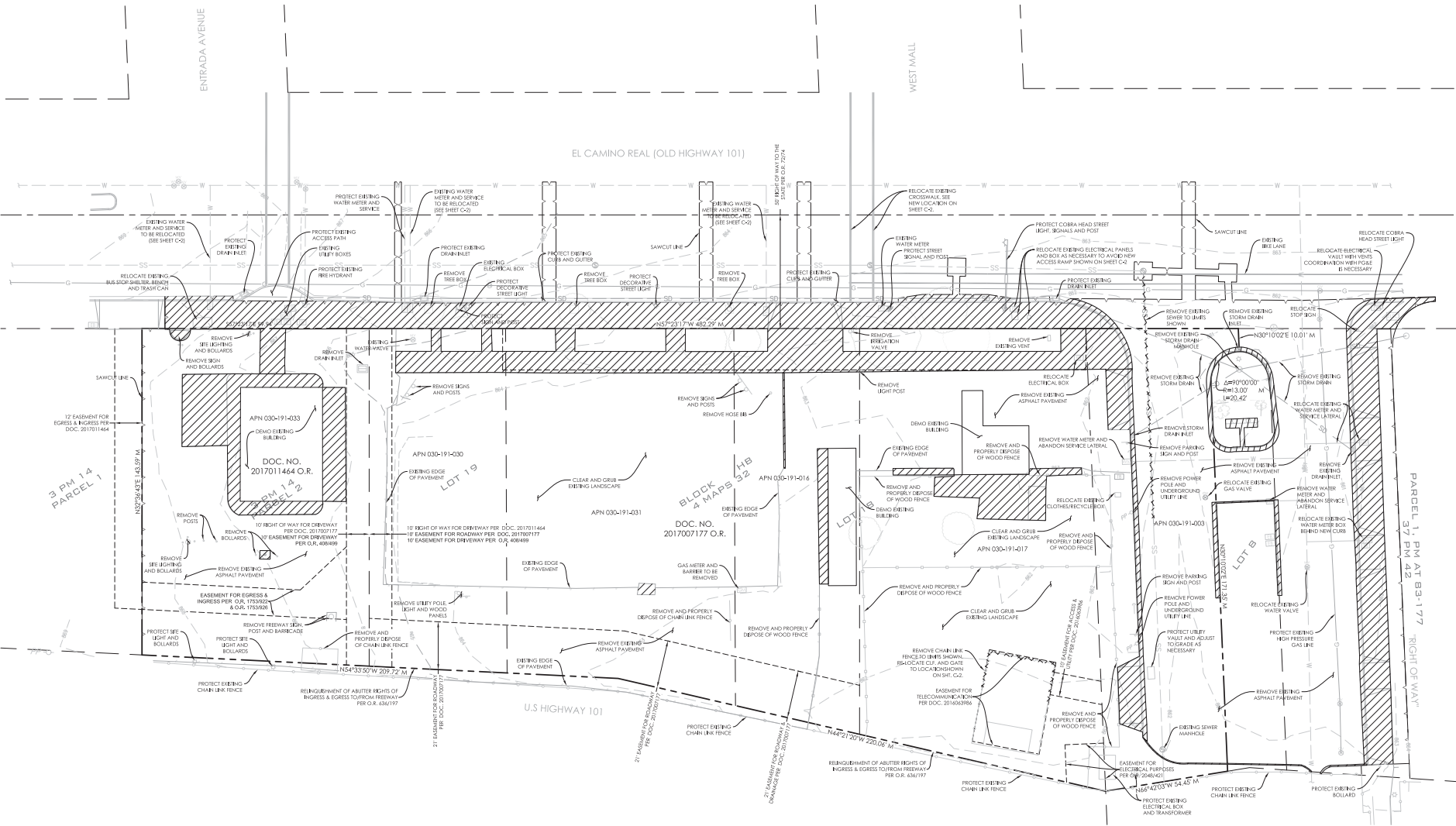
Painted Fascia
Dunn Edwards DE6105
Weathered Leather



Painted Corbels & Rafter Tails
Dunn Edwards DE5173
Deep Coral



Pylon Sign

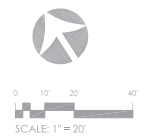
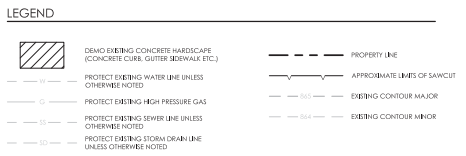


GENERAL NOTES:

1. PROTECT ALL EXISTING INFRASTRUCTURE OUTSIDE OF PROPERTY LINE.
2. ALL UTILITY COMPANIES MUST BE NOTIFIED PRIOR TO THE START OF CONSTRUCTION BY THE CONTRACTOR.
3. ADJUST ALL UTILITY BOXES, VALVES AND MANHOLES TO GRADE UNLESS OTHERWISE NOTED ON PLANS.
4. SEE TREE PROTECTION AND REPLACEMENT PLAN FOR IDENTIFICATION OF REMOVAL AND PROTECTION OF TREES.

CONTRACTOR'S NOTES:

CONTRACTOR SHALL PROTECT EXISTING PUBLIC INFRASTRUCTURE FROM DAMAGE BY HEAVY LOADING AND/OR EQUIPMENT DURING THE COURSE OF CONSTRUCTION. CONTRACTORS SHALL REPAIR, AT OWNERS EXPENSE, ANY AND ALL DAMAGE TO PUBLIC INFRASTRUCTURE INCURRED DURING AND/OR DUE TO CONSTRUCTION TO THE SATISFACTION OF THE RESPECTIVE PERMITTING AGENCY.



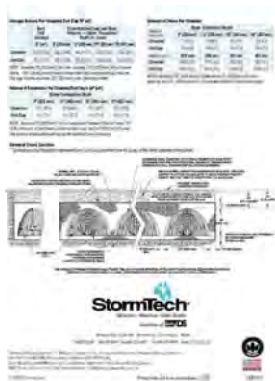
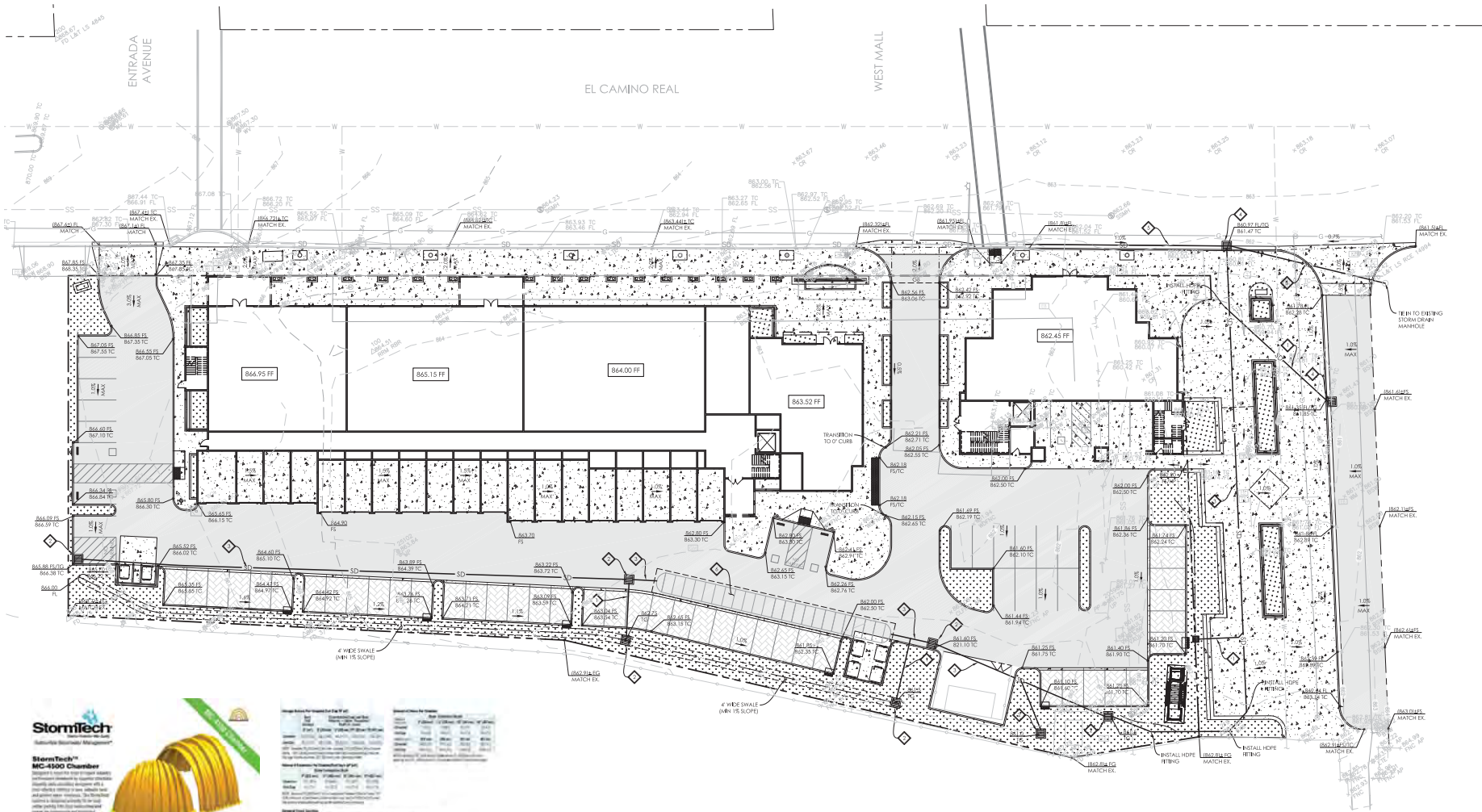
LA PLAZA MIXED-USE

EXISTING CONDITIONS/SITE SEPARATION

SCALE: 1" = 20'-0" (24X36 sheet)

SEPT. 28, 2017
0725-01-RS16





IMPROVEMENT NOTES

- ◆ INSTALL 8" HDPE STORM DRAIN (S = 0.005 M/N)
- ◆ INSTALL 36" X 36" WED STATE CONCRETE DRAIN INLET OR APPROVED EQUAL
- ◆ INSTALL 12" HDPE STORM DRAIN (S = 0.005 M/N)
- ◆ INSTALL STORM DRAIN INLET PER CITY OF ATASCADERO STANDARD DRAWING NO. 302.
- ◆ INSTALL 24" HDPE STORM DRAIN (S = 0.005 M/N)

NOTES:
1. SEE SHEET C-2 FOR SEE UTILITIES AND IMPROVEMENTS

LEGEND

- AC PAVEMENT
- PCC CONCRETE
- LANDSCAPE (SEE LANDSCAPE PLANS)
- PERMEABLE PAVERS (SEE NOTE 15)



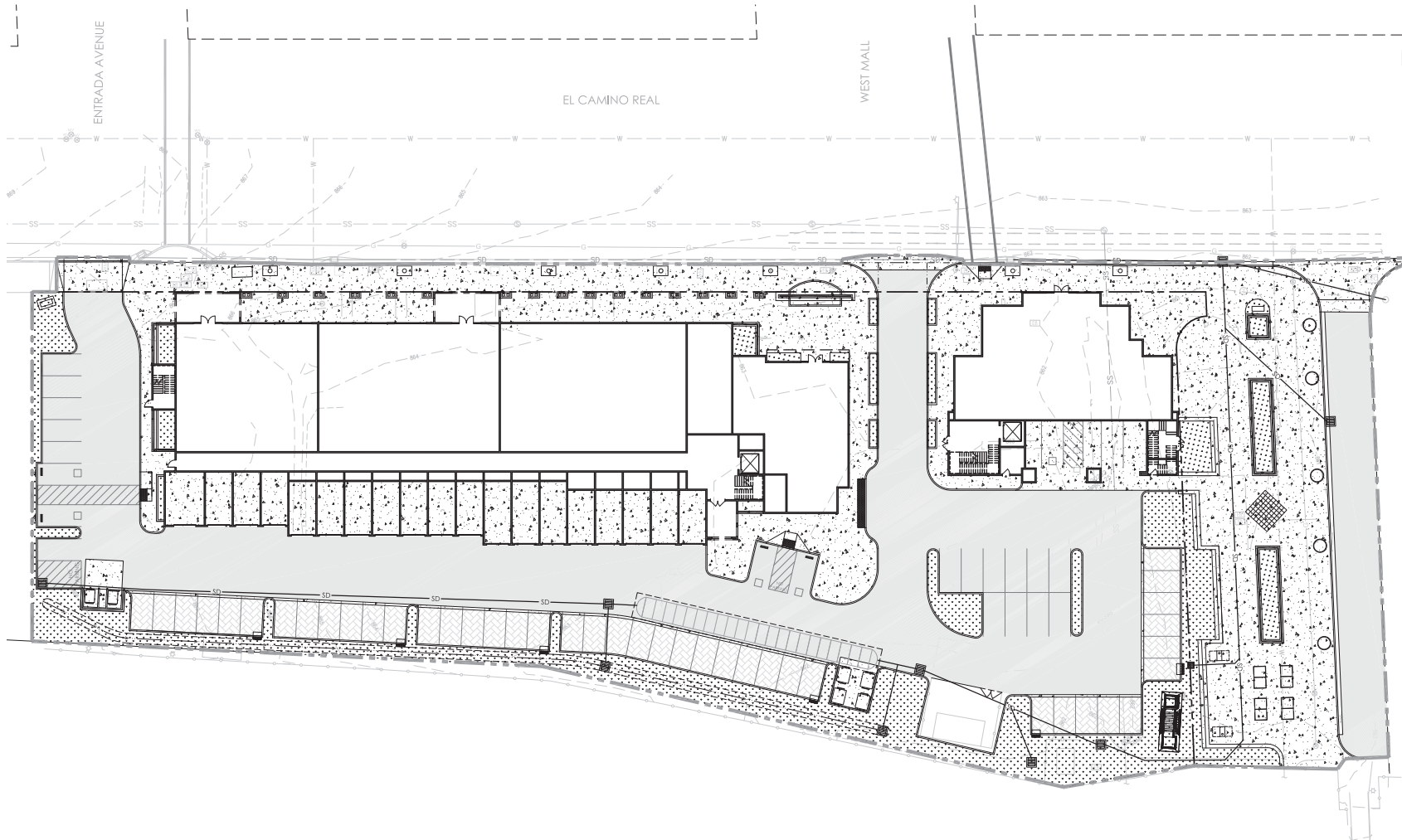
LA PLAZA MIXED-USE

GRADING AND DRAINAGE

SCALE: 1'-0" = 20'-0" (24X36 sheet)
SCALE: 1'-0" = 40'-0" (12X18 sheet)

SEPT. 28, 2017
0725-01-RS16





EXISTING STATISTICS

EXISTING IMPERVIOUS AREA
 SITE AREA: 69,378 SF
 (EXISTING BUILDINGS, EXISTING PAVEMENT)

EXISTING PERVIOUS AREA
 SITE AREA: 32,312 SF

PROPOSED STATISTICS

--- LIMITS OF DISTURBANCE: 101,690 SF

(A) PROPOSED IMPERVIOUS AREA
 CONCRETE/PAVING AREA: 56,068 SF
 BUILDING AREA: 29,212 SF

(B) PROPOSED PERVIOUS AREA
 LANDSCAPE AREA: 10,850 SF
 PAVER AREA: 5,560 SF

PRELIMINARY HYDROLOGY

STORM WATER CONTROL
 TOTAL ONSITE REQUIRED
 CAPTURED AREA : 85,280 SF
 95TH PERCENTILE DEPTH: 1.75"

REQUIRED STORAGE: 12,437 CF

PROJECT IS A REDEVELOPMENT PROJECT OUTSIDE
 AN URBAN SUSTAINABILITY AREA THEREFORE:

REQUIRED STORAGE: 12,437 CF X 0.5 = 6,218 CF

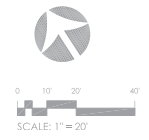
TOTAL ONSITE STORAGE: 6,500 CF
 (STORM TECH CHAMBERS AND PERMEABLE PAVERS)

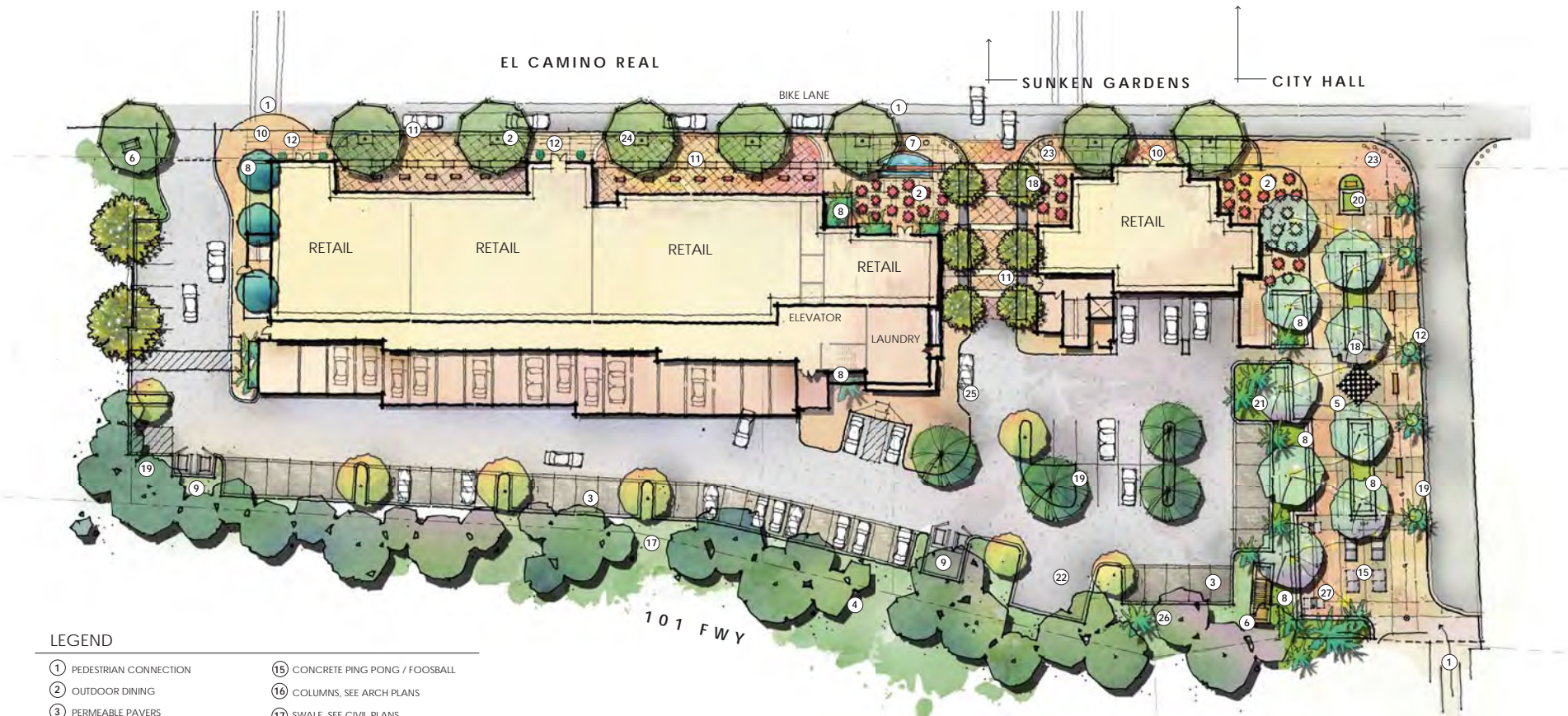
PROPOSED IMPERVIOUS

- PROPOSED AC
- PROPOSED BUILDING
- PROPOSED CONCRETE

PROPOSED PERVIOUS

- PROPOSED PERMEABLE PAVERS
- PROPOSED LANDSCAPE AREA





LEGEND

- ① PEDESTRIAN CONNECTION
- ② OUTDOOR DINING
- ③ PERMEABLE PAVERS
- ④ EXISTING VEGETATIVE BUFFER (CALTRANS)
- ⑤ 12' X 12" INTERACTIVE CHESS BOARD
- ⑥ MONUMENT SIGN / ENTRY FEATURE
- ⑦ WATER WALL AND PLANTER
- ⑧ RAISED PLANTER SEATWALLS
- ⑨ TRASH ENCLOSURES
- ⑩ DECORATIVE CONCRETE PAVING
- ⑪ PAVERS / ACCENT BANDS
- ⑫ CONTAINER GARDENS
- ⑬ STREET TREES (SEE TREE EXHIBIT)
- ⑭ FILTER STRIPS
- ⑮ CONCRETE PING PONG / FOOSBALL
- ⑯ COLUMNS, SEE ARCH PLANS
- ⑰ SWALE, SEE CIVIL PLANS
- ⑱ FLOWERING TREES WITH TWINKLE LIGHTING
- ⑲ RE-LOCATED CURB
- ⑳ E.G. LEWIS STATUE + HISTORICAL MONUMENT
- ㉑ MIXED PLANTING BUFFER AT PARKING AREAS
- ㉒ CELL TOWER EASEMENT
- ㉓ BOLLARDS
- ㉔ STEEL TREE GRATES
- ㉕ DROP-OFF
- ㉖ DROUGHT TOLERANT LANDSCAPING
- ㉗ CORNHOLE COURT

METHOD OF IRRIGATION

THE IRRIGATION SYSTEM WILL UTILIZE THE FOLLOWING METHODS:

DRIP AND BUBBLER IRRIGATION WILL BE USED TO APPLY WATER ACCURATELY TO THE PLANT ROOT ZONES AT A RATE THAT IT CAN INFILTRATE TO IMPROVE IRRIGATION EFFICIENCY.

LOW SPRINKLER HEADS WILL BE USED WHERE NEEDED TO APPLY WATER UNIFORMLY AND SLOWLY.

OVER-SPRAY AND EVAPORATION WILL BE MINIMIZED AND MATCHED PRECIPITATION RATE NOZZLES WILL BE USED WITHIN EACH CONTROL VALVE AND CIRCUIT.

A WEATHER BASED, SELF-ADJUSTING IRRIGATION CONTROL WITH A RAIN SHUT OFF DEVICE WILL BE INSTALLED. THE IRRIGATION SYSTEM WILL BE CONTROLLED BY A TIME CLOCK WITH THE ABILITY TO ADJUST RUNTIMES BY DATA COLLECTED FROM RAIN AND ET SENSORS.

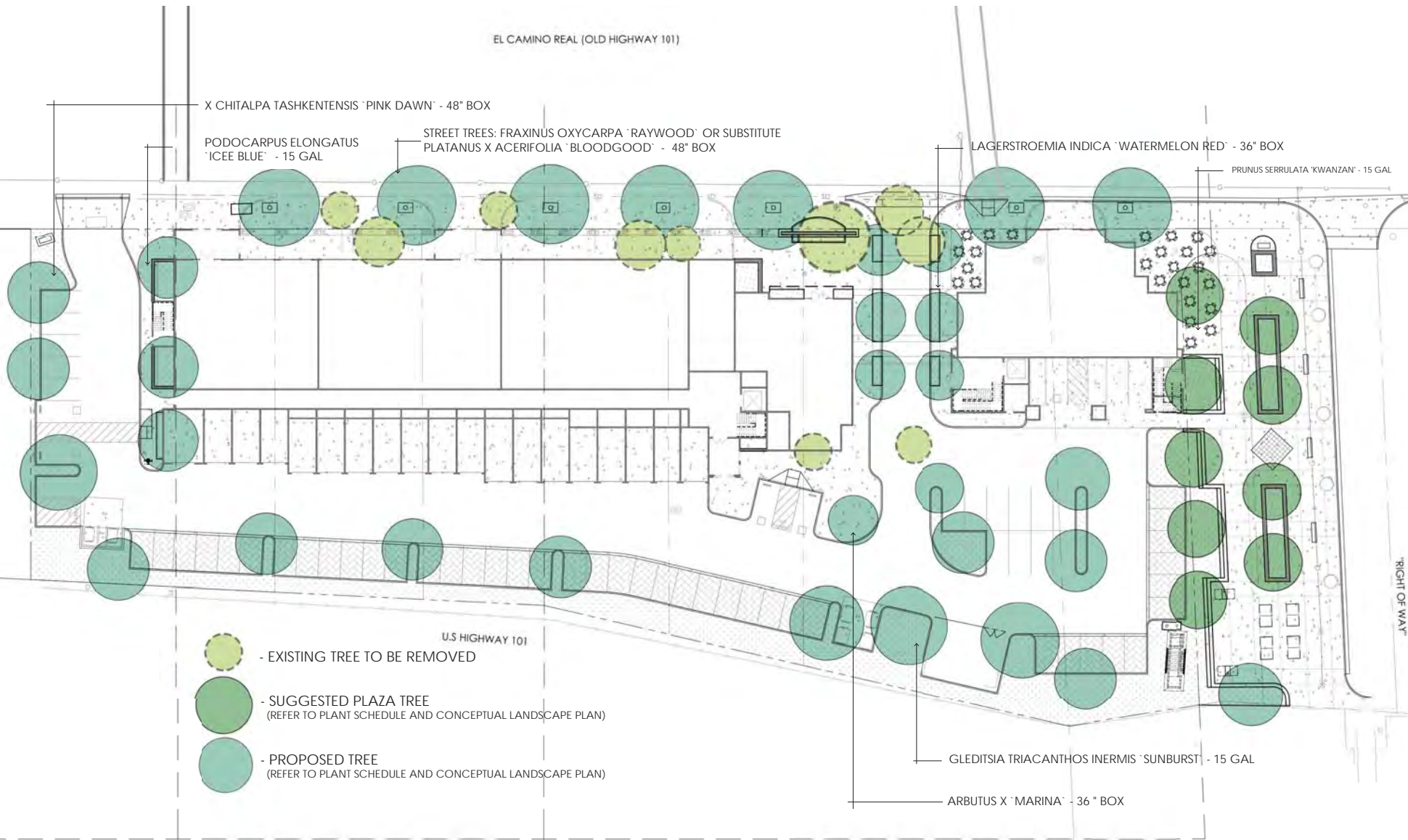
THE WATERING SCHEDULE WILL BE BASED UPON PLANT NEEDS, SOIL TYPE, SLOPE, AND SEASON. IRRIGATION WILL BE SCHEDULED TO AVOID WATERING DURING RAIN AND FREEZE EVENTS.

MWELO WATER CALCULATIONS

MAXIMUM ANNUAL APPLIED WATER ALLOWANCE:
(MAWA) 142,891.70 GALLONS, 191.03 UNITS

ESTIMATED TOTAL WATER USE:
(ETWU) 108,631.99 GALLONS, 145.23 UNITS

PROPOSED PROJECT IS USING 76.02% OF THE MAXIMUM WATER ALLOWANCE PER GOVERNOR'S ORDER NO. B-29-15



CONCEPTUAL PLANT SCHEDULE

	COMMON NAME	CONT
ACCENT TREES		
CERCIS CANADENSIS 'HEARTS OF GOLD'	FOREST PANSY REDBUD	15 GAL
LAGERSTROEMIA INDICA 'WATERMELON RED'	WATERMELON RED CRAPE MYRTLE	15 GAL
LAGERSTROEMIA INDICA 'ZUNI'	LAVENDER CRAPE MYRTLE MULTI-TRUNK	15 GAL
MAGNOLIA X 'STAR WARS'	STAR WARS MAGNOLIA	15 GAL
X CHITALPA TASHKENTENSIS 'PINK DAWN'	PINK DAWN CHITALPA	36"BOX
DECIDUOUS TREES		
PRUNUS SERRULATA 'KWANZAN'	KWANZAN FLOWERING CHERRY	15 GAL
FRAXINUS OXYCARPA 'RAYWOOD' TM	RAYWOOD ASH	36"BOX
GLEDITSIA TRIACANTHOS INERMIS 'SUNBURST'	SUNBURST COMMON HONEYLOCUST	24"BOX
* PLATANUS X ACERIFOLIA 'BLOODGOOD'	LONDON PLANE TREE	24"BOX
EVERGREEN TREES		
ARBUTUS X 'MARINA'	ARBUTUS STANDARD	36"BOX
PODOCARPUS ELONGATUS 'ICEE BLUE' TM	ICEE BLUE YELLOW WOOD	24"BOX
QUERCUS AGRIFOLIA	COAST LIVE OAK MULTI-TRUNK	15 GAL
SHRUBS		
ACANTHUS MOLLIS	BEAR'S BREECH	5 GAL
AEONIUM CANARIENSE	AEONIUM	3 GAL
AGAVE ATTENUATA 'NOVA'	BLUE CLONE	5 GAL
AGAVE DESMETTIANA 'VARIEGATA'	VARIEGATED AGAVE	5 GAL
ALOE POLYPHYLLA	SPIRAL ALOE	5 GAL
ANIGOZANTHOS X 'BUSH PEARL'	DWARF PINK KANGAROO PAW	5 GAL
ARCTOSTAPHYLOS X 'EMERALD CARPET'	EMERALD CARPET MANZANITA	5 GAL
CALAMAGROSTIS X ACUTIFLORA 'KARL FOERSTER'	FEATHER REED GRASS	5 GAL
CARPENTERIA CALIFORNICA	BUSH ANEMONE	5 GAL
CEANOETHUS GRISEUS HORIZONTALIS	CARMEL CREEPER	5 GAL
* CHONDRUPETALUM TECTORUM 'EL CAMPO'	CAPE RUSH	5 GAL
CISTIS PULVERULENTUS 'SUNSET'	ROCKROSE	5 GAL
COTINUS COGGYGRIA 'PURPUREUS'	SMOKE TREE	5 GAL
DIANELLA CAERULEA 'CASSA BLUE'	CASSA BLUE FLAX LILY	1 GAL
ECHVEVERIA X 'PERLE VON NURNBERG'	AFTERGLOW ECHEVERIA	1 GAL
* HAKONECHLOA MACRA 'AUREOLA'	GOLDEN VARIEGATED HAKONECHLOA	1 GAL
* IRIS DOUGLASSIANA 'PACIFIC COAST HYBRIDS'	PCH IRIS	1 GAL
LEONOTIS LEONURUS	LION'S TAIL	5 GAL
* LOMANDRA LONGIFOLIA 'BREEZE'	DWARF MAT RUSH	5 GAL
NEPETA X FAASSENII 'SELECT BLUE'	CATMINT	5 GAL
PENSTEMON HEIEROPHYLLUS	FOOTHILL PENSTEMON	5 GAL
PHORMIUM TENAX 'YELLOW WAVE'	NEW ZEALAND FLAX	5 GAL
PINIUS MUGO PUMILIO	DWARF MUGO PINE	5 GAL
ROSA X 'FLOWER CARPET WHITE'	ROSE	1 GAL
SALVIA CLEVELANDII 'WINIFRED GILLMAN'	CLEVELAND SAGE	5 GAL
SENECIO MANDRALISCAE 'BLUE CHALK STICKS'	SENECIO	1 GAL
SENECIO SERPENS	BLUE CHALKSTICKS	5 GAL
THUJA OCCIDENTALIS 'SHERWOOD MOSS'	SHERWOOD MOSS ARBORVITAE	15 GAL
ZAUSCHNERIA CALIFORNICA	CALIFORNIA FUCHSIA	5 GAL

THE PLANT PALETTE IS DESIGNED TO UTILIZE A MAJORITY OF DROUGHT TOLERANT PLANTS (80%) WITH THE REMAINDER DESIGNATED FOR PLANTS REQUIRING MODERATE WATER (20%). THIS PLANT PALETTE COUPLED WITH THE EFFICIENT IRRIGATION SYSTEM (PROPOSED ON THIS SHEET) WILL MEET OR EXCEED THE STATE'S REQUIREMENTS SET FORTH IN THE MODEL WATER EFFICIENT LANDSCAPE ORDINANCE.

NOTE: * INDICATES SPECIES IS SUITABLE FOR LID INFILTRATION AREAS.

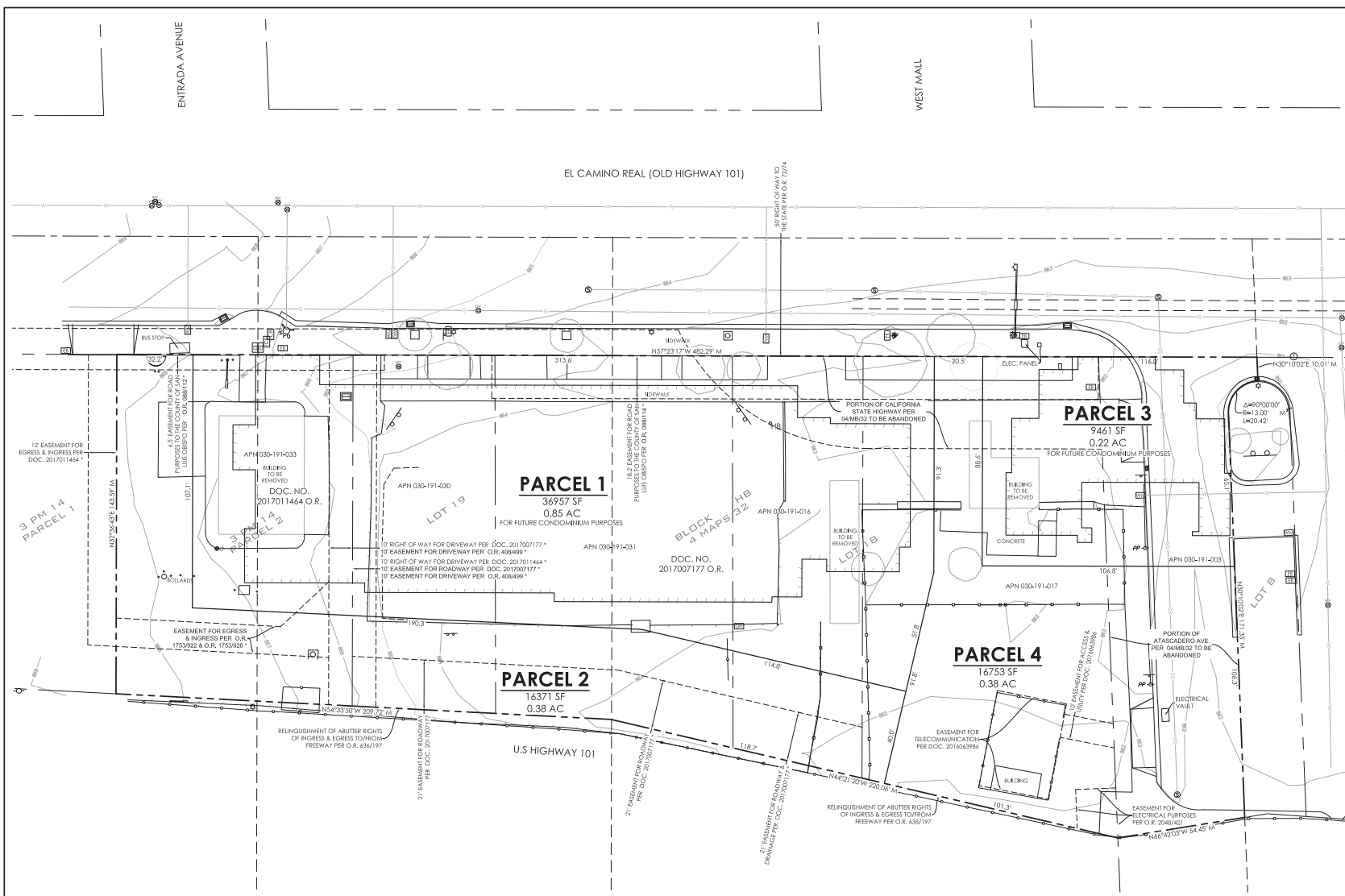
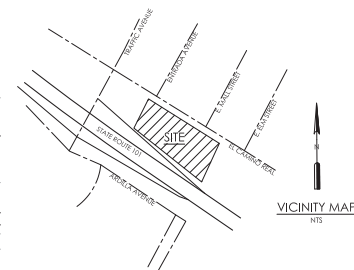


IMAGES FOR CONCEPTUAL PURPOSES ONLY. SEE LANDSCAPE PLAN FOR EDGE CONDITIONS AND LOCATION OF AMENITIES.

TENTATIVE PARCEL MAP PARCEL MAP AT 17-0088

IN THE CITY OF ATASCADERO
BEING A SUBDIVISION OF A PORTION OF LOT 18 AND 19 OF BLOCK 4B OF MAP OF ATASCADERO
AS RECORDED IN BOOK 4 OF MAPS AT PAGE 32 OF ORIGINAL RECORDS, IN THE CITY OF ATASCADERO,
COUNTY OF SAN LUIS OBISPO, STATE OF CALIFORNIA.

SEPTEMBER 15, 2017



PROJECT DESCRIPTION:
A TENTATIVE PARCEL MAP TO CONFIGURE 5 EXISTING LOTS INTO 4

PROPERTY INFORMATION:
APNs: 030-191-003, 016, 017, 018 & 019
ADDRESS: 6320, 6420, 6450, 6490 & 6500 EL CAMINO REAL, ATASCADERO, CA
AREA: 7928 SQ.FT. 1.82 ± ACRES
ZONE: DDC: DOWNTOWN COMMERCIAL

OWNERS:
ZAPPAS FAMILY TRUST- MIKE ZAPPAS
8199 SAN EMILAS LANE
ATASCADERO, CA
CONTACT: MIKE ZAPPAS
EMAIL: MIKEZAPPAS@GMAIL.COM

APPLICANT:
RRM DESIGN GROUP
3765 S. HIGUERA ST., STE. 102
SAN LUIS OBISPO, CA 93401
PH (805) 543-1794
EMAIL: FLBCOTE@RRMDESIGN.COM

REPRESENTATIVE:
SAME AS APPLICANT

LEGEND

	EXISTING PROPERTY LINE
	PROPOSED PROPERTY LINE
	PROPOSED BUILDING
	WATER METER
	PG&E BOX
	AT&T BOX
	FIRE HYDRANT
	GAS
	ELECTRIC BOX
	TELEPHONE
	PULL BOX OR EVIDENCE THEREOF
	SIGN
	POWER POLE
	GAS VALVE
	SEWER MANHOLE
	TELEPHONE MANHOLE
	LIGHT
	WATER VALVE
	GAS VALVE

MAP PREPARED BY:
RRM DESIGN GROUP
3765 S. HIGUERA ST., STE. 102
SAN LUIS OBISPO, CA 93401
PH (805) 543-1794

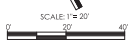
BASIS OF BEARINGS:
THE BASIS OF BEARINGS FOR THIS SURVEY IS THE CALIFORNIA COORDINATE SYSTEM 1983 (CCCS83), ZONE 5, 2010.00 EPOCH, AS BE DETERMINED BY GPS MEASUREMENTS TAKEN ON JANUARY 12, 2017 AND ESTABLISHED FROM CORE STATION 7033 (PP DMS61) DERIVED FROM GEOIDETIC VALUES PUBLISHED BY THE NATIONAL GEODETIC SURVEY.

BENCHMARK:
VERTICAL DATUM FOR THE SURVEY IS NAVD 83 AND WAS ESTABLISHED FROM THE NATIONAL GEODETIC SURVEY CORE STATION DESIGNATION LOS2020_C32006 GRP. PID DMS61 AND USED TO ESTABLISH A TEMPORARY BENCHMARK ON A NAIL AND TAG STAKED REE 1.8794 AS SHOWN ON 37 PARCEL MAPS 42 AND FOUND ON THE SOUTHWEST CORNER OF EL CAMINO REAL AND ATASCADERO WAY HAVING AN ELEVATION OF 862.42.

PARCEL USE:
PARCEL 1 3 COMMERCIAL CONDOMINIUMS AND 40 RESIDENTIAL CONDOMINIUMS
PARCEL 3 3 COMMERCIAL CONDOMINIUMS
PARCELS 2 & 4 ACCESS, PARKING AND UTILITY PURPOSES

PROPOSED EASEMENT NOTE:
PRIVATE RECIPROCAL VEHICULAR ACCESS, PEDESTRIAN ACCESS, PARKING & DRAINAGE EASEMENTS AND PUE COVER ALL OF PARCELS 1 THROUGH 4, INCLUSIVE, EXCLUDING BUILDING LOCATIONS FOR THE USE AND BENEFIT OF THE PRESENT AND FUTURE LOT OWNERS OF PARCELS 1 THROUGH 4.

EXISTING EASEMENT NOTE:
ALL EXISTING EASEMENTS WITH AN " * " ARE TO BE ABANDONED.
THE PRELIMINARY TITLE REPORT NO. 4001-622892A PREPARED BY BEST AMERICAN TITLE COMPANY DOES NOT SET ANY DOCUMENTS STATING A GURCE-ABANDONMENT OF THE CALIFORNIA STATE HIGHWAY AS SHOWN IN BOOK 32 OF MAPS AT PAGE 32 IN THE COUNTY RECORDER'S OFFICE. THE LEGAL DESCRIPTION DESCRIBED IN THE PRELIMINARY TITLE REPORT DOES NOT EXCEPT THAT PORTION LYING IN SAID CALIFORNIA STATE HIGHWAY.



Planning Commission Exhibit 2 - Annex Residential Subdivision Design Package

Tract 3104 for six (6) New Residences
THE ANNEX RESIDENTIAL
 (in the Del Rio Road Commercial Area Specific Plan)
 South Marisol Way, Atascadero, CA 93422



BUILDING CODE DATA

SPRINKLERS	Required	Yes
	Proposed	Yes
CONST. TYPE	V-B	
OCCUPANCY GROUP	R-3	
ALLOWABLE AREA	Unlimited	
MAX HEIGHT	40/10 Stories	
FIRE INFORMATION	City of Atascadero Fire Department	
HAZARD ZONE	LRA - Local Responsibility Area	

CALIFORNIA CODE REFERENCES

1 This Project shall comply w/ all current applicable Codes & Ordinances, including but not limited to:
 2016 California Building Code Volumes 1 & 2 (CBC)
 2016 California Residential Code (CRC)
 2016 California Plumbing Code (CPC)
 2016 California Mechanical Code (CMC)
 2016 California Electrical Code (CEC)
 2016 California Energy Code (CEC) or (CEES)
 2016 California Green Building Standards Code (CGBCS) or (Cal Green)
 2016 California Fire Code (CFC)
 2016 California Reference Standards Code
 City of Atascadero Municipal Code Title 9 Planning & Zoning
 City of Atascadero Municipal Code Title 9 Planning & Zoning

PLANNING DATA

ZONING	SFR-X	Single Family Residential
ZONING DISTRICT	R5F-X	
OVERLAY	R02P	Planned Development Overlay Zone No. 17
SP-2		
SETBACKS/YARDS	Front Setback @ Porch	15'0"
	Front Setback @ Dwelling	20'0"
	Front Setback @ Garage	25'0"
	Side Yard	5'0"
	Side Yard (corner lot)	12'0"
	Rear	10'0"
	*Garage shall be recessed from front of residence by at least 10'	
DENSITY	ALLOWED	161 Units / 141 Units per Acre x 1.67 Acres
	PROPOSED	161 Units
PARKING	REQUIRED	(12) Covered (2) Uncovered
	PROPOSED	(12) Covered (12) Uncovered
		(2) Covered residential unit x (8) units (1) Uncovered guest/unit x (8) units
MAXIMUM HEIGHT	PROPOSED	12'0" Uncovered *Refer to Elevation for Maximum Building Height Illustration

BUILDING AREAS		
BUILDING TYPE 1	Lower Floor	1,169 s.f.
	Upper Floor	1,326 s.f.
	Total Conditioned	2,475 s.f.
	Garage	645 s.f.
	Covered Porch/Patio	1,743 s.f.
	Gross Lower Floor Area	2,386 s.f.
	Total Building Footprint	4,164 s.f.
	*lower ft @ garage	
BUILDING TYPE 2	Lower Floor	1,169 s.f.
	Upper Floor	1,326 s.f.
	Total Conditioned	2,475 s.f.
	Garage	574 s.f.
	Covered Porches/Patios	937 s.f.
	Gross Lower Floor Area	1,743 s.f.
	Total Building Footprint	2,670 s.f.
	*lower ft @ garage *incl covered patio	
BUILDING TYPE 3	Lower Floor	1,492 s.f.
	Upper Floor	1,888 s.f.
	Total Conditioned	2,380 s.f.
	Garage	614 s.f.
	Covered Porch/Patio	535 s.f.
	Gross Lower Floor Area	2,106 s.f.
	Total Building Footprint	2,439 s.f.
	*lower ft @ garage *incl covered patio	

PROJECT DESCRIPTION

This is a proposal for a standard subdivision to create six (6) residential parcels with the following proposed Vesting Tentative Tract Map, per "The Annex" portion of the Del Rio Road Commercial Area Specific Plan, and its Master Plan of Development. This proposal includes a development plan with engineered site, grading and drainage, and utility plans, and a conceptual landscape plan.
 Additionally, this housing plan includes the architecture for six (6) four-bedroom, two-story single-family residences with attached two-car garages. The architectural design aesthetic for the proposed homes incorporates styles of "California Colonial" and "California Farmhouse".
 The proposed on-site construction of South Marisol Way, as well as the required public pedestrian access easement, are offered for dedication to the City. Additionally, a single drainage detention basin for the subdivision is located immediately adjacent to proposed Lots 4, 5 and 6, to be recorded as an easement on the adjacent parcel owned by MP Annex, LLC, and is offered in dedication to the City.
PROJECT SIZE: 1.67 acres - total area for proposed subdivision (incl including detention basin)

PROJECT DATA

PROJECT ADDRESS: South Marisol Way, Atascadero, CA 93422

APN: Portions of 049-102-020 and 049-102-032

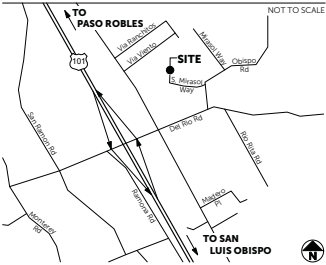
LEGAL DESCRIPTION: Tract 3104 - Vesting Tentative Tract Map

PLANNING APP. NO.: PLN-2017-1641

PROPOSED LOT SIZES: Ranging from 9,000 s.f. to 12,000 s.f.

UTILITIES	Electricity	PG&E
	Natural Gas	Southern California Gas Company
	Water Service	Atascadero Mutual Water Company
	Sanitary Sewer	City of Atascadero
	Trash Service	City of Atascadero

VICINITY MAP



DIRECTORY

Owner: MP Annex, LLC
 284 Higuerita Street
 San Luis Obispo, CA 93401
 (805) 748-9097
 Email: cormadmasonn.com

Project Representative: WHELEN CONSULTING, INC.
 Patti Whelan
 P.O. Box 5021
 San Luis Obispo, CA 93403
 Phone: (805) 503-9747
 Email: patt@whelenconsulting.com

Architect of Record: JADE ARCHITECTURE
 Alison Edwards, C-34194
 5220 Spycamore Road
 Atascadero, CA 93422
 Phone: (805) 254-0799
 Email: alison@jadearchitecture.net

Civil Engineer: CANNON
 Seth Stevens, P.E.
 1050 Southwood Drive
 San Luis Obispo, CA 93401
 Phone: (805) 503-4559
 Email: seth@cannoncorp.us

Surveyor of Record: MBS LAND SURVEYS
 Michael B. Stanton
 3645 Sandoz Street, Unit Q
 San Luis Obispo, CA 93401
 Phone: (805) 594-1960
 Email: mbs@mbslandsurveys.com

Landscape Architect: Sunwest Interiors & Partners
 Jim Burnett
 979 Oscar Street, Suite B6
 San Luis Obispo, CA 93401
 Phone: (805) 235-2231
 Email: jim@sunwestinteriors.net

Geotechnical Engineer: TBD
General Contractor: TBD
Structural Engineer: TBD
Electrical Consultant: TBD
Mechanical & Plumbing: TBD
Fire Sprinkler: TBD

LOT COVERAGE		
LOT 1 (Building Type 1)	Lot Area	11,323 s.f.
	Building Coverage	8,893 s.f.
	Landscape	4,449 s.f.
	REQUIRED	2,386 s.f.
	PROPOSED	1,410 s.f.
	*incl covered patio *incl driveway	
LOT 2 (Building Type 2)	Lot Area	10,288 s.f.
	Building Coverage	5,595 s.f.
	Landscape	4,114 s.f.
	REQUIRED	2,070 s.f.
	PROPOSED	1,185 s.f.
	*incl covered patio *incl driveway	
LOT 3 (Building Type 3)	Lot Area	10,401 s.f.
	Building Coverage	3,640 s.f.
	Landscape	4,160 s.f.
	REQUIRED	2,439 s.f.
	PROPOSED	1,124 s.f.
	*incl covered patio *incl driveway	
LOT 4 (Building Type 1)	Lot Area	11,855 s.f.
	Building Coverage	4,142 s.f.
	Landscape	4,734 s.f.
	REQUIRED	2,670 s.f.
	PROPOSED	4,837 s.f.
	*incl covered patio *incl driveway	
LOT 5 (Building Type 1)	Lot Area	9,067 s.f.
	Building Coverage	1,175 s.f.
	Landscape	3,627 s.f.
	REQUIRED	2,439 s.f.
	PROPOSED	868 s.f.
	*incl covered patio *incl driveway	
LOT 6 (Building Type 1)	Lot Area	11,257 s.f.
	Building Coverage	3,940 s.f.
	Landscape	4,503 s.f.
	REQUIRED	2,386 s.f.
	PROPOSED	1,129 s.f.
	*incl driveway	

FLOOR AREA RATIO	75% of gross first floor (inclusive of Garage)
BUILDING TYPE 01 (Lots 2 & 6)	ALLOWED 1,307 s.f.
	PROPOSED 1,306 s.f.
BUILDING TYPE 02 (Lots 2 & 4)	ALLOWED 1,307 s.f.
	PROPOSED 1,306 s.f.
BUILDING TYPE 03 (Lots 2 & 5)	ALLOWED 1,580 s.f.
	PROPOSED 888 s.f.

SHEET INDEX

T1.0	PROJECT TITLE SHEET
C1	VTTM Tract 3104 TITLE SHEET
C2	SITE and GRADING PLAN
C3	UTILITY PLAN
Landscape	
L-1	CONCEPTUAL LANDSCAPE PLAN
L-2	CONCEPTUAL LANDSCAPE PLAN Architectural
A1.1	OVERALL ARCHITECTURAL SITE PLAN
A2.1	FLOOR PLAN - Building Type 1 (Lots 1 and 6)
A2.2	FLOOR PLAN - Building Type 2 (Lots 2 and 4)
A2.3	FLOOR PLAN - Building Type 3 (Lots 3 and 5)
A5.1	EXTERIOR ELEVATIONS - LOT 1 (Building Type 1)
A5.2	EXTERIOR ELEVATIONS - LOT 2 (Building Type 2)
A5.3	EXTERIOR ELEVATIONS - LOT 3 (Building Type 3)
A5.4	EXTERIOR ELEVATIONS - LOT 4 (Building Type 2)
A5.5	EXTERIOR ELEVATIONS - LOT 5 (Building Type 3)
A5.6	EXTERIOR ELEVATIONS - LOT 6 (Building Type 1)
A6.1	COLOR and MATERIAL BOARD



Alison Edwards
 (805) 254-0799
 (805) 465-1327
 alison@jadearchitecture.net
 2330 Yosemite Ct., #202
 Atascadero, CA 93422

www.jadearchitecture.net



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Tract 3104 for six (6) New Residences
THE ANNEX RESIDENTIAL
 MP Annex, LLC
 South Marisol Way
 Atascadero, CA 93422

PROJECT TITLE SHEET

DATE: June 21, 2017
 JOB NO: 17-007
 DRAWN BY: A. Edwards
 REVISIONS:

SHEET:
T1.0

THE ANNEX VESTING TENTATIVE TRACT MAP TRACT 3104 ATASCADERO, CALIFORNIA

OWNER:

MP ANEX, LLC
284 HIGUERA STREET
SAN LUIS OBISPO, CA 93401
CONTACT: CLYNT PEARCE

OWNER REPRESENTATIVE:

WHELEN CONSULTING, INC.
P.O. BOX 5021
SAN LUIS OBISPO, CA 93403
CONTACT: PATTI WHELEN
PHONE: (805) 503-9747
EMAIL: PATTI@WHELENCONSULTING.COM

ENGINEER/SURVEYOR:

CANNON
1050 SOUTHWOOD DRIVE
SAN LUIS OBISPO, CA 93401
CONTACT: SETH STEVENS
PHONE: (805) 544-7407
EMAIL: SETH@CANNONCSP.US

APN:

049-102-020
049-102-032

PROJECT AREA:

1.66 ACRES (RESIDENTIAL DEVELOPMENT)
2.21 ACRES (REMANDER PARCEL)

BENCH MARK:

THE BENCH MARK FOR THIS PROJECT IS A BRASS DISK STAMPED 7.1400 RESET, SET IN THE SOUTHEASTERLY SIDEWALK OF DEL RIO ROAD OVER CROSSING OF HIGHWAY 101, 14.3' SOUTH OF CENTERLINE OF DEL RIO ROAD OVER CROSSING, 3.4' SOUTH OF FRONT LINE CURB, 1.6' NORTH OF SOUTH EDGE OF SIDEWALK, 12.4' WEST OF SOUTHWEST CORNER OF DEL RIO ROAD OVER CROSSING, 0.9' ABOVE ROADWAY. NGS DESIGNATION AC9284 F 1500.

ELEVATION = 883.7' (NAVD 88)

BASIS OF BEARINGS:

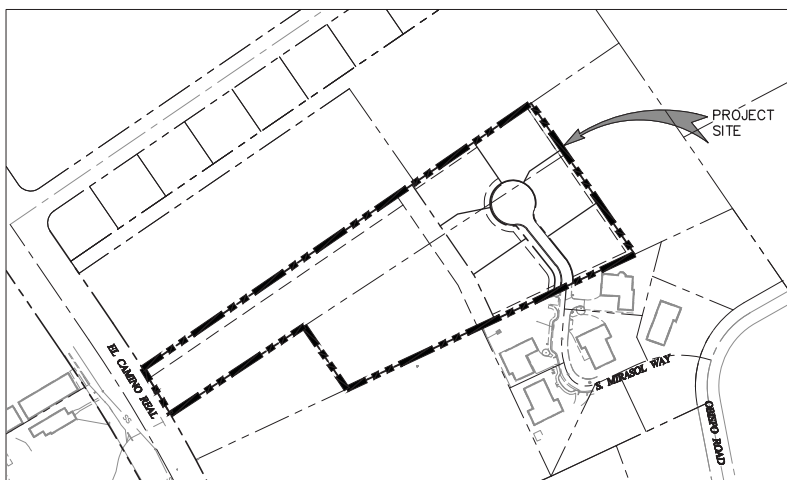
THE BASIS OF BEARINGS FOR THIS PROJECT IS BASED ON FOUND MONUMENTS ALONG THE SOUTHEASTERLY LINE OF LOT. BEARING N 64° 55' 11" E.

ZONING/GENERAL PLAN INFORMATION:

ZONING DISTRICT: RSF-1/PD-17/SP-2
GENERAL PLAN DESIGNATION: SINGLE FAMILY RESIDENTIAL (SFR-1)

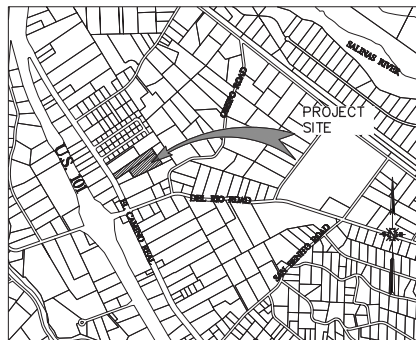
NOTE:

EXISTING TOPOGRAPHIC DATA PREPARED BY OTHERS AND PROVIDED BY OWNER.



SITE MAP

SCALE HORIZ 1"=200'



VICINITY MAP

SCALE HORIZ 1"=1000'

LEGEND

	EXISTING	PROPOSED
TRACT BOUNDARY	---	---
PROPERTY LINE	---	---
RIGHT-OF-WAY	---	---
EASEMENT/SETRACK	---	---
STREET CENTERLINE	---	---
CURB	---	---
CURB & GUTTER	---	---
ROAD STRIPING	---	---
FENCE	X	X
DAKLIGHT LINE	X	X
100YR FLOODPLAIN BOUNDARY	---	---
FLOWLINE	---	---
DRAINAGE SWALE	---	---
RETAINING WALL	---	---
GRADE BREAK	---	---
SLOPE	---	---
CONTOURS	---	---
WATER MAIN	W	W
RECLAIMED WATER LINE	RW	RW
SANITARY SEWER LINE	SS	SS
STORM DRAIN LINE	SD	SD
GAS LINE	G	G
ELECTRIC LINE	E	E
OVERHEAD WRES	OH	OH
FIBER OPTICS	FO	FO
TELEPHONE	T	T
JOINT TRENCH	JT	JT
STORMWATER BIOFILTRATION/RETENTION AREA	---	---
FIRE HYDRANT	CF	CF
STREET LIGHT (PUBLIC)	---	---
STREET LAMP (PRIVATE)	---	---
STORM DRAIN INLET	---	---
CURB INLET	---	---
PROPRIETARY BIOFILTRATION UNIT	---	---
TREES	---	---
POWER POLE	---	---
SOHM	---	---
S3M	---	---
RIPRAP PROTECTION AT SD OUTLET	---	---
ASPHALT CONCRETE	---	---
ASSESSOR'S PARCEL NUMBER	AC	AC
CENTERLINE	CL	CL
DUCTILE IRON	DI	DI
EXISTING GRADE	EG	EG
EXISTING	(E)	(E)
HIGH POINT	HP	HP
INVERT	INV	INV
OPEN SPACE	OS	OS
PUBLIC FACILITY	PF	PF
PROPERTY LINE	PL	PL
PEDESTRIAN ACCESS EASEMENT	PAE	PAE
PUBLIC UTILITY EASEMENT	PUE	PUE
RETAINING WALL	RTW	RTW
RECLAIMED WATER	RW	RW
RIGHT OF WAY	ROW	ROW
SANITARY SEWER	SS	SS
STORM DRAIN	SD	SD
STREET TREE EASEMENT	STE	STE
STORMWATER TREATMENT EASEMENT	SWTE	SWTE
TOP OF GRATE	TG	TG
VERTICAL CURVE	VC	VC
WATERLINE	WL	WL

SHEET INDEX

- C1 TITLE SHEET
- C2 TENTATIVE TRACT MAP
- C3 GRADING AND DRAINAGE PLAN
- C4 UTILITY PLAN

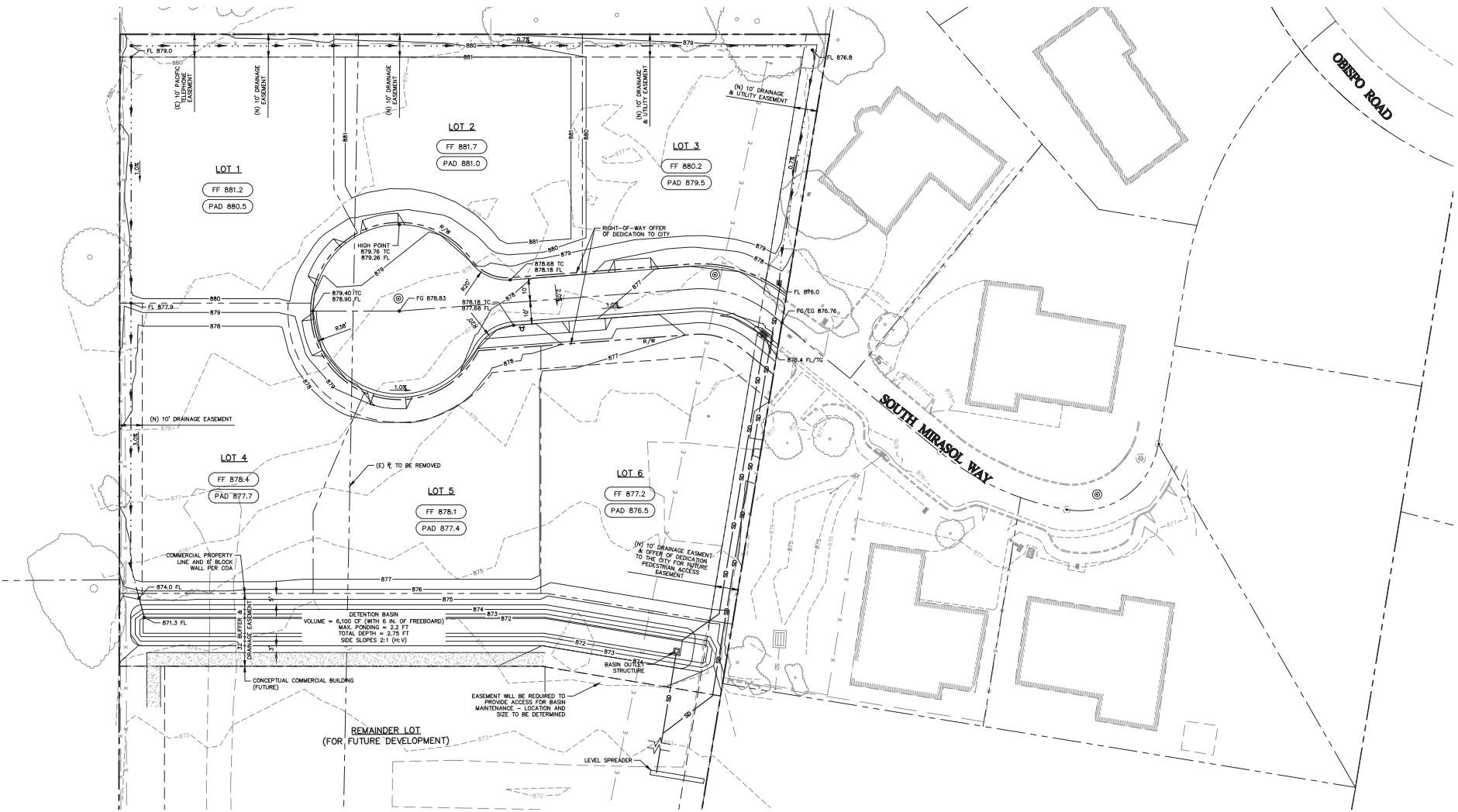
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1050 Southwood Drive
San Luis Obispo, CA 93401
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THE ANNEX TRACT 3104 - VESTING TENTATIVE TRACT MAP TITLE SHEET		
ATASCADERO, CA		
DRAWN BY	DATE	CA JOB NO.
STS	11/13/2017	170103
CHECKED BY	SCALE	SHEET
	N.T.S.	C1 OF 4

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REV. NO.	DATE	REVISED	DESTROY ALL PRINTS BEARING EARLIER DATE.	REV. BY	CHK. BY	APP. BY



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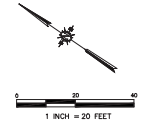
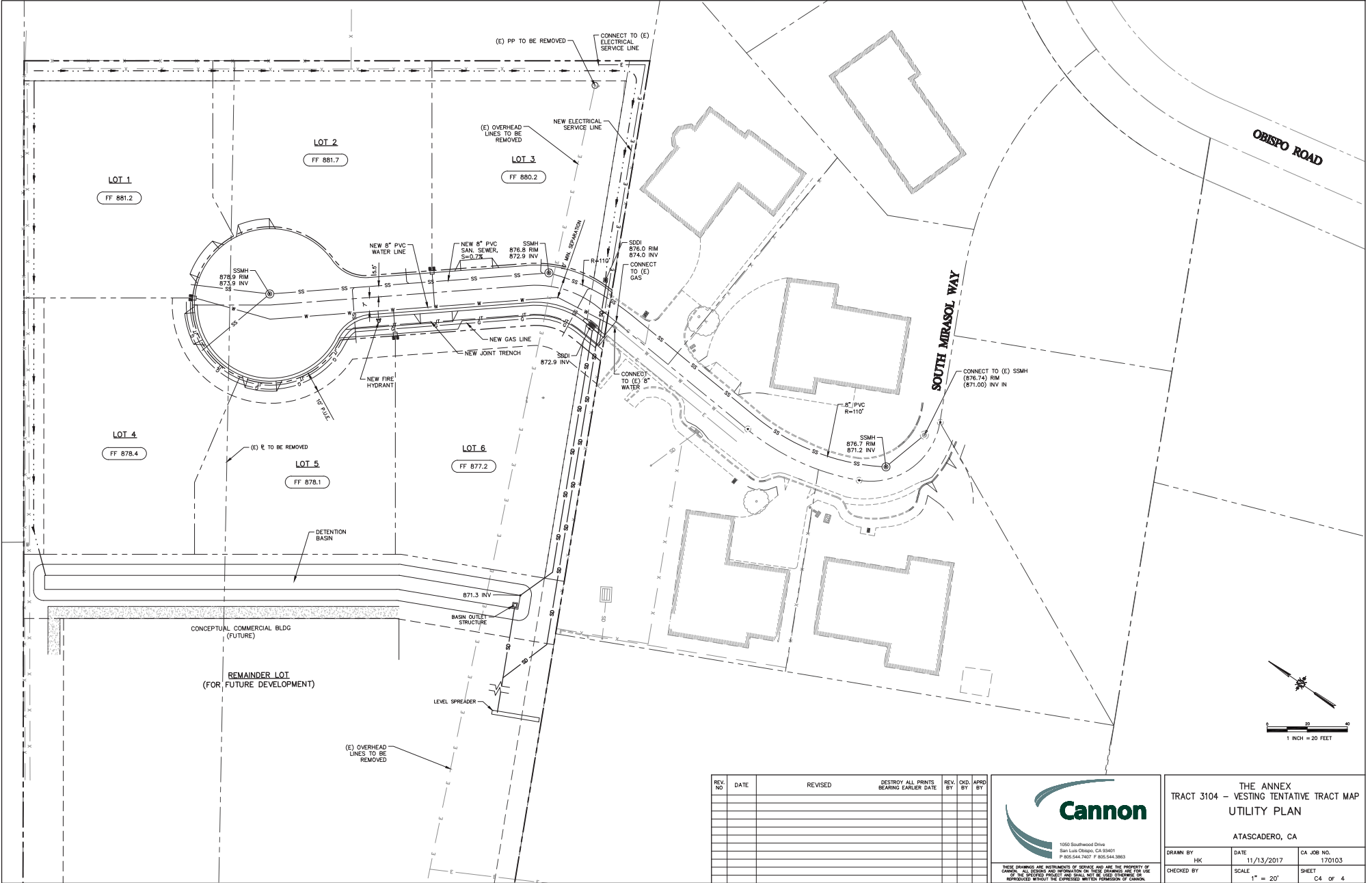
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THE ANNEX
TRACT 3104 - VESTING TENTATIVE TRACT MAP
GRADING AND DRAINAGE PLAN

ATASCADERO, CA

DRAWN BY	DATE	CA JOB NO.
HK	11/13/2017	170103
CHECKED BY	SCALE	SHEET
	1" = 20'	C3 OF 4

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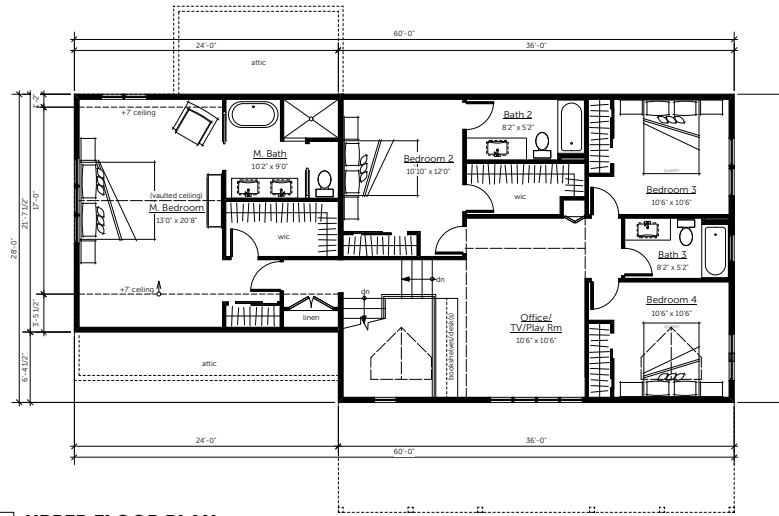
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Cannon
 1050 Southwood Drive
 San Luis Obispo, CA 93401
 P 805.544.7407 F 805.544.3903

THE ANNEX
 TRACT 3104 - VESTING TENTATIVE TRACT MAP
 UTILITY PLAN
 ATASCADERO, CA

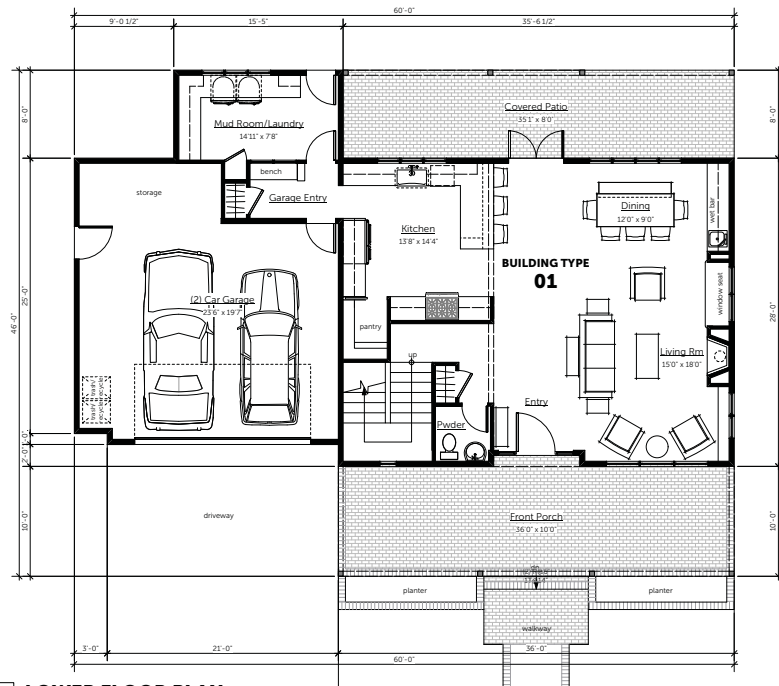
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CHECKED BY		SCALE	1" = 20'	SHEET	C-4 OF 4

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2 UPPER FLOOR PLAN

Scale: 3/16" = 1'-0"



1 LOWER FLOOR PLAN

Scale: 3/16" = 1'-0"

BUILDING AREAS

BUILDING TYPE 1	Area	Area (sq. ft.)
Lower Floor		1,169 s.f.
Upper Floor		1,306 s.f.
Total Conditioned		2,475 s.f.
Garage		574 s.f.
Covered Porch/Patio		643 s.f.
Gross Lower Floor Area		1,743 s.f.
Total Building Footprint		2,386 s.f.

* lower fr. garage
* incl. covered patio



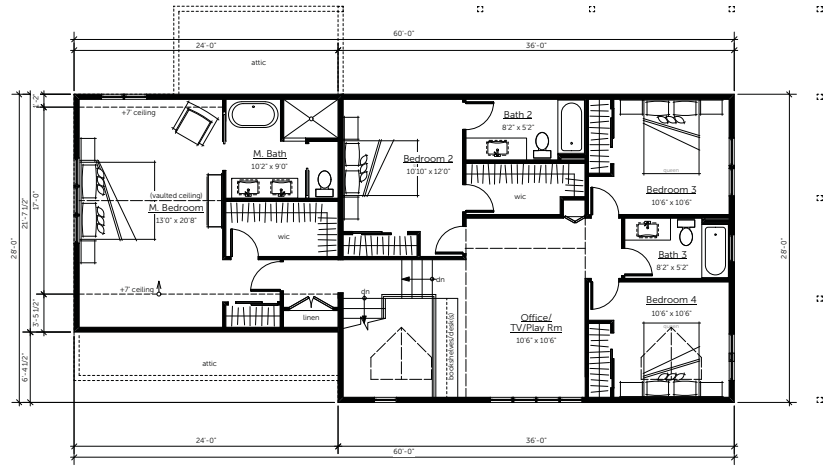
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Tract 3104 for six (6) New Residences
THE ANNEX RESIDENTIAL
 MP Annex, LLC
 South Marisol Way
 Atascadero, CA 93422

FLOOR PLANS
Building Type 01
(Lots 1 & 6)

DATE: June 21, 2017
 JOB NO: 17-007
 DRAWN BY: A. Edwards
 REVISIONS:

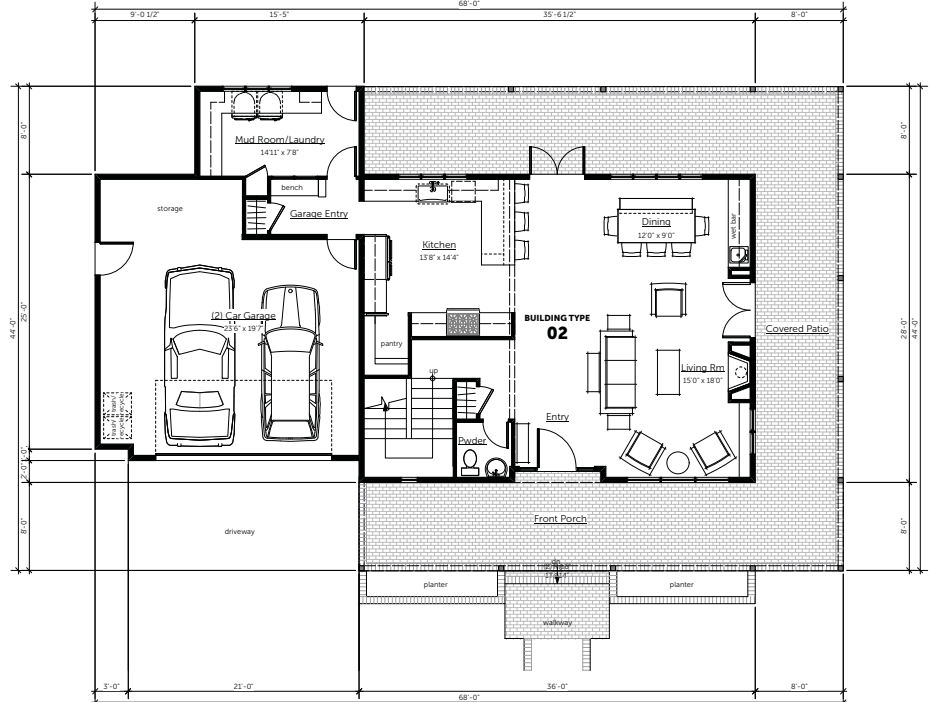
SHEET:
A2.1



2

UPPER FLOOR PLAN

Scale: 3/16" = 1'-0"



1

LOWER FLOOR PLAN

Scale: 3/16" = 1'-0"

BUILDING AREAS

Lower Floor	1,569 s.f.
Upper Floor	1,166 s.f.
Total Conditioned	2,475 s.f.
Garage	574 s.f.
Covered Porches/Patios	927 s.f.
Gross Lower Floor Area	1,745 s.f.
Total Building Footprint	2,670 s.f.

* lower fr & garage
* incl. covered patio



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Tract 3104 for six (6) New Residences
THE ANNEX RESIDENTIAL
 MP Annex, LLC
 South Marisol Way
 Atascadero, CA 93422

FLOOR PLANS
Building Type 02
(Lots 2 & 4)

DATE: June 21, 2017
 JOB NO: 17-007
 DRAWN BY: A. Edwards
 REVISIONS:

SHEET:
A2.2

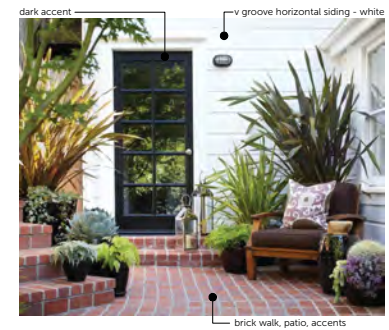


front elevation

Scale: 3/16" = 1'-0"

California Colonial

precedent image



side (left) elevation

Scale: 3/16" = 1'-0"



side (right) elevation







Scale: 3/16" = 1'-0"



back elevation

Scale: 3/16" = 1'-0"

color and materials

	Roof Composition Shingle Color: Charcoal
	Siding Fiber Composite "V" Groove Horizontal Siding Color: White
	Windows/Doors Fiberglass Color: White
	Trim Painted Wood Color: White
	Accent Color (entry/garage doors) Painted Wood Color: Dark Coffee
	Accent Stone (6 chimney, 6 pavers, where occurs) Brick Color: Rustic



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Tract 3104 for six (6) New Residences
THE ANNEX RESIDENTIAL
 MP Annex, LLC
 South Marisol Way
 Atascadero, CA 93422

EXTERIOR ELEVATIONS
LOT 1
Building Type 1

DATE: June 21, 2017
 JOB NO: 17-007
 DRAWN BY: A. Edwards
 REVISIONS:

SHEET:
A5.1



front elevation

Scale: 3/16" = 1'-0"

California Farmhouse

precedent image



side (left) elevation

Scale: 3/16" = 1'-0"



side (right) elevation







Scale: 3/16" = 1'-0"



back elevation

Scale: 3/16" = 1'-0"

color and materials

	Roof Composition Shingle Color: Weathered Wood
	Siding Fiber Composite "V" Groove Horizontal Siding Color: Hay Green
	Windows/Doors Fiberglass Color: Light Ochre
	Trim Painted Wood Color: Pewter Grey
	Accent Color (entry/garage doors) Painted Wood Color: Dark Coffee
	Accent Stone (6 chimney, 6 pavers, where occurs) River Rock Color: tbd (tan colors)

JADE ARCHITECTURE

Alison Edwards
 415.254.0799
 415.465.5237
 alison@jadearchitecture.net
 2330 Yorba Linda Blvd
 Redlands, CA 92323

www.jadearchitecture.net



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Tract 3104 for six (6) New Residences
THE ANNEX RESIDENTIAL
 LOT 2
 Building Type 2
 MP Annex, LLC
 South Marisol Way
 Atascadero, CA 93422

EXTERIOR ELEVATIONS
 LOT 2
 Building Type 2

DATE: June 21, 2017
 JOB NO: 17-007
 DRAWN BY: A. Edwards

REVISIONS:
 SHEET:
A5.2



front elevation

Scale: 3/16" = 1'-0"

California Farmhouse

precedent image



side (left) elevation

Scale: 3/16" = 1'-0"



side (right) elevation







Scale: 3/16" = 1'-0"



back elevation

Scale: 3/16" = 1'-0"

color and materials

	Roof Composition Shingle Color: Charcoal
	Siding Fiber Composite "V" Groove Horizontal Siding Color: White
	Windows/Doors Fiberglass Color: Redwood
	Trim Painted Wood Color: White
	Accent Color (entry/garage doors) Painted Wood Color: Redwood
	Accent Stone (6 chimney, 6 pavers, where occurs) Brick Color: Rustic

JADE ARCHITECTURE

Alison Edwards
 415.254.0799
 415.465.5227
 alison@jadearchitecture.net
 2330 Jermoluk Way
 Redwood City, CA 94062

www.jadearchitecture.net



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Tract 3104 for six (6) New Residences
THE ANNEX RESIDENTIAL
 MP Annex, LLC
 South Marisol Way
 Atascadero, CA 93422

EXTERIOR ELEVATIONS
LOT 3
Building Type 3

DATE: June 21, 2017
 JOB NO: 17-007
 DRAWN BY: A. Edwards
 REVISIONS:

SHEET:
A5.3



front elevation

Scale: 3/16" = 1'-0"

California Colonial

precedent image



dark accent

v-groove horizontal siding - white

brick walk, patio, accents



side (left) elevation

Scale: 3/16" = 1'-0"



side (right) elevation

Scale: 3/16" = 1'-0"



back elevation

Scale: 3/16" = 1'-0"

color and materials

- Roof**
Composition Shingle
Color: Charcoal
- Siding**
Fiber Composite "V" Groove Horizontal Siding
Color: White
- Windows/Doors**
Fiberglass
Color: Dark Coffee
- Trim**
Painted Wood
Color: White
- Accent Color** (entry/garage doors)
Painted Wood
Color: Indian Red
- Accent Stone** (♯ chimney, ♯ pavers, where occurs)
Brick
Color: Rustic



All ideas, designs, arrangements and plans indicated or represented by the drawings, reports, specifications, computer files, field data notes and other documents prepared by JADE ARCHITECTURE are instruments of service owned by and shall remain the property of JADE ARCHITECTURE and were created and developed for use, and in conjunction with, the specific project described herein. None of said instruments of service shall be used by, disclosed to any person, firm, or corporation for any purpose and no copying, reproduction or use thereof is permitted without the consent of JADE ARCHITECTURE. JADE ARCHITECTURE shall retain all common law, statutory and other reserved rights including, without limitation, the copyrights therein.

Tract 3104 for six (6) New Residences
THE ANNEX RESIDENTIAL
 MP Annex, LLC
 South Marisol Way
 Atascadero, CA 93422

EXTERIOR ELEVATIONS
LOT 4
Building Type 2

DATE: June 21, 2017
 JOB NO: 17-007
 DRAWN BY: A. Edwards

REVISIONS:

SHEET:
A5.4



front elevation

Scale: 3/16" = 1'-0"

California Farmhouse

precedent image



side (left) elevation

Scale: 3/16" = 1'-0"



side (right) elevation

Scale: 3/16" = 1'-0"



back elevation

Scale: 3/16" = 1'-0"

color and materials

	Roof Composition Shingle Color: Weathered Wood
	Siding Fiber Composite 1" Groove Horizontal Siding Color: Indian Red
	Windows/Doors Fiberglass Color: Dark Coffee
	Trim Painted Wood Color: Pewter Grey
	Accent Color (entry/garage doors) Vertical siding Color: French Grey
	Accent Stone (chimney, pavers, where occurs) River Rock Color: tbd (tan colors)

JADE ARCHITECTURE

Alison Edwards
 415.214.0799
 415.465.5327
 alison@jadearchitecture.net
 2330 Jermolow Ave
 Redwood City, CA 94062

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Tract 3104 for six (6) New Residences
THE ANNEX RESIDENTIAL
 MP Annex, LLC
 South Marisol Way
 Atascadero, CA 93422

EXTERIOR ELEVATIONS
LOT 5
Building Type 3

DATE: June 21, 2017
 JOB NO: 17-007
 DRAWN BY: A. Edwards
 REVISIONS:

SHEET:
A5.5



front elevation

Scale: 3/16" = 1'-0"

California Farmhouse

precedent image



side (left) elevation

Scale: 3/16" = 1'-0"



side (right) elevation

Scale: 3/16" = 1'-0"



back elevation

Scale: 3/16" = 1'-0"

color and materials

- Roof**
Composition Shingle
Color: Weathered Wood
- Siding**
Fiber Composite "V" Groove Horizontal Siding
Color: French Gray
- Windows/Doors**
Fiberglass
Color: Pale Carmine
- Trim**
Painted Wood
Color: Pewter Grey
- Accent Color** (entry/garage doors)
Painted Wood
Color: Dark Coffee
- Accent Stone** (5 chimney, 6 pavers, where occurs)
River Rock
Color: tbd (tan colors)



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 Atascadero, CA 93422

EXTERIOR ELEVATIONS
LOT 6
Building Type 1

DATE: June 21, 2017
 JOB NO: 17-007
 DRAWN BY: A. Edwards
 REVISIONS:

SHEET:
A5.6



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Tract 3104 for six (6) New Residences
THE ANNEX RESIDENTIAL
 MP Annex, LLC
 South Marisol Way
 Atascadero, CA 93422

COLOR and MATERIAL BOARD

DATE: June 21, 2017
 JOB NO: 17-007
 DRAWN BY: A. Edwards
 REVISIONS:

SHEET:
A6.1

LOT 1

color and materials



Roof
 Composition Shingle
 Color: Charcoal



Siding
 Fiber Composite "V" Groove Horizontal Siding
 Color: White



Windows/Doors
 Fiberglass
 Color: White



Trim
 Painted Wood
 Color: White



Accent Color (entry/garage doors)
 Painted Wood
 Color: Dark Coffee



Accent Stone (5 chimney, 8 pavers, where occurs)
 Brick
 Color: Rustic

LOT 2

color and materials



Roof
 Composition Shingle
 Color: Weathered Wood



Siding
 Fiber Composite "V" Groove Horizontal Siding
 Color: Hay Green



Windows/Doors
 Fiberglass
 Color: Light Ochre



Trim
 Painted Wood
 Color: Pewter Grey



Accent Color (entry/garage doors)
 Painted Wood
 Color: Dark Coffee



Accent Stone (5 chimney, 8 pavers, where occurs)
 River Rock
 Color: tbd (tan colors)

LOT 3

color and materials



Roof
 Composition Shingle
 Color: Charcoal



Siding
 Fiber Composite "V" Groove Horizontal Siding
 Color: White



Windows/Doors
 Fiberglass
 Color: Redwood



Trim
 Painted Wood
 Color: White



Accent Color (entry/garage doors)
 Painted Wood
 Color: Redwood



Accent Stone (5 chimney, 8 pavers, where occurs)
 Brick
 Color: Rustic

LOT 4

color and materials



Roof
 Composition Shingle
 Color: Charcoal



Siding
 Fiber Composite "V" Groove Horizontal Siding
 Color: White



Windows/Doors
 Fiberglass
 Color: Dark Coffee



Trim
 Painted Wood
 Color: White



Accent Color (entry/garage doors)
 Painted Wood
 Color: Indian Red



Accent Stone (5 chimney, 8 pavers, where occurs)
 Brick
 Color: Rustic

LOT 5

color and materials



Roof
 Composition Shingle
 Color: Weathered Wood



Siding
 Fiber Composite "V" Groove Horizontal Siding
 Color: Indian Red



Windows/Doors
 Fiberglass
 Color: Dark Coffee



Trim
 Painted Wood
 Color: Pewter Grey



Accent Color (entry/garage doors)
 Vertical siding
 Color: French Grey



Accent Stone (5 chimney, 8 pavers, where occurs)
 River Rock
 Color: tbd (tan colors)

LOT 6

color and materials



Roof
 Composition Shingle
 Color: Weathered Wood



Siding
 Fiber Composite "V" Groove Horizontal Siding
 Color: French Gray



Windows/Doors
 Fiberglass
 Color: Pale Carmine



Trim
 Painted Wood
 Color: Pewter Grey



Accent Color (entry/garage doors)
 Painted Wood
 Color: Dark Coffee



Accent Stone (5 chimney, 8 pavers, where occurs)
 River Rock
 Color: tbd (tan colors)